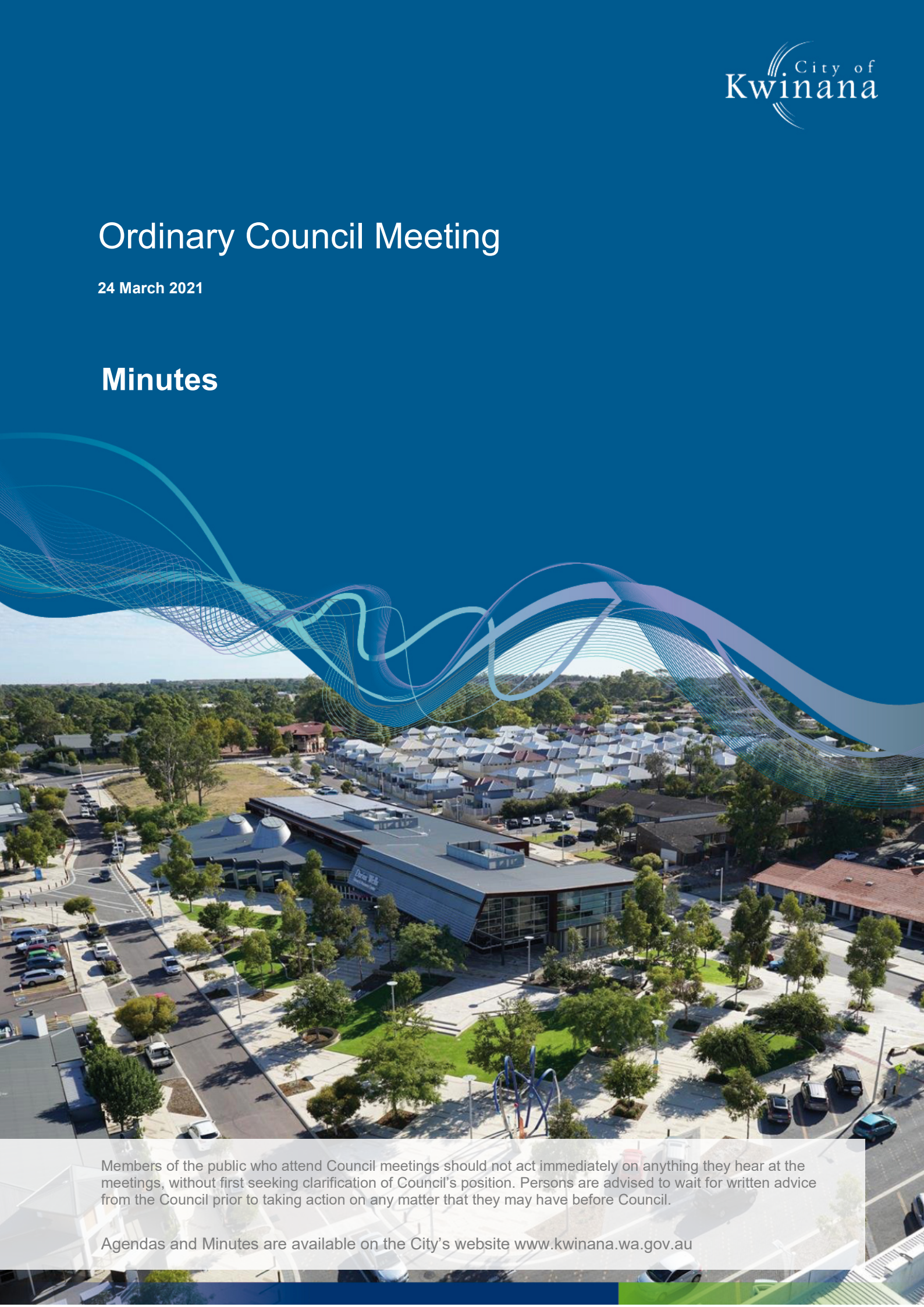


# Ordinary Council Meeting

24 March 2021

## Minutes

An aerial photograph of a city street scene. In the foreground, there is a large, modern building with a glass facade and a flat roof. To the right, there are several residential houses with white roofs. The street is lined with trees and parked cars. A decorative graphic of blue and white wavy lines is overlaid on the top half of the image.

Members of the public who attend Council meetings should not act immediately on anything they hear at the meetings, without first seeking clarification of Council's position. Persons are advised to wait for written advice from the Council prior to taking action on any matter that they may have before Council.

Agendas and Minutes are available on the City's website [www.kwinana.wa.gov.au](http://www.kwinana.wa.gov.au)

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## **Present:**

MAYOR CAROL ADAMS, OAM  
DEPUTY MAYOR PETER FEASEY  
CR W COOPER  
CR M KEARNEY  
CR S LEE  
CR M ROWSE

MR W JACK - Chief Executive Officer  
MRS B POWELL - Director City Engagement  
MRS M COOKE - Director City Development and Sustainability  
MR D ELKINS - Director City Infrastructure / Acting Director City Business  
MR A HOLBROOK - Coordinator Strategic Planning (5:34pm – 5:53pm)  
MS A MCKENZIE - Council Administration Officer

Members of the Press 0  
Members of the Public 0

## **1 Opening and announcement of visitors**

*Presiding Member declared the meeting open at 5:30pm and welcomed all in attendance.*

## **2 Acknowledgement of country**

*Presiding Member read the Acknowledgement of country*

*“It gives me great pleasure to welcome you all here and before commencing the proceedings, I would like to acknowledge that we come together tonight on the traditional land of the Noongar people and we pay our respects to their Elders past and present.”*

## **3 Dedication**

*Deputy Mayor Peter Feasey read the dedication*

*“May we, the Elected Members of the City of Kwinana, have the wisdom to consider all matters before us with due consideration, integrity and respect for the Council Chamber.*

*May the decisions made be in good faith and always in the best interest of the greater Kwinana community that we serve.”*

## **4 Attendance, apologies, Leave(s) of absence (previously approved)**

### **Apologies**

Councillor Dennis Wood

### **Leave(s) of Absence (previously approved):**

Councillor Sherilyn Wood from 16 March 2021 to 17 April 2021 inclusive.

## 5 Public Question Time

Nil

## 6 Receiving of petitions, presentations and deputations:

### 6.1 Petitions:

Nil

### 6.2 Presentations:

Nil

### 6.3 Deputations:

Nil

## 7 Confirmation of minutes

### 7.1 Ordinary Meeting of Council held on 10 March 2021:

#### COUNCIL DECISION

382

MOVED CR M KEARNEY

SECONDED MAYOR C ADAMS

That the Minutes of the Ordinary Meeting of Council held on 10 March 2021 be confirmed as a true and correct record of the meeting.

CARRIED  
6/0

## 8 Declarations of Interest (financial, proximity, impartiality – both real and perceived) by Members and City Officers

Mayor Carol Adams declared an impartiality interest in item 18.2, Accounts for payment for the month ended 28 February 2021 due to her spouse's employer (Kwinana Industries Council) receiving a reimbursement payment.

## 9 Requests for leave of absence

Nil

**10 Items brought forward for the convenience of those in the public gallery**

Nil

**11 Any business left over from previous meeting**

Nil

**12 Recommendations of committees**

Nil

**13 Enbloc reports**

Nil

## 14 Reports - Community

### 14.1 Implementation of Child Safety Officers in Local Government

#### **DECLARATION OF INTEREST:**

There were no declarations of interest declared.

#### **SUMMARY:**

The Department of Local Government, Sport and Cultural Industries has issued a Discussion Paper on the Implementation of Child Safety Officers in Local Governments (Discussion Paper). The Discussion Paper is in response to Recommendation 6.12 of the Royal Commission into Institutional Responses to Child Sexual Abuse (Royal Commission).

For the Department, the Discussion Paper has two key aims:

- to develop a better understanding of the current role of local governments in promoting child safety and how the outcomes of this work are reported internally, to executive and to council; and
- to use this understanding of current work promoting child safety to inform development of an approach to meet recommendation 6.12 of the Royal Commission in implementing the child safety officer role.

A response to the Discussion Paper has been prepared and is attached at Attachment 2.

#### **OFFICER RECOMMENDATION:**

That Council endorse the response to the Department of Local Government, Sport and Cultural Industries Discussion Paper on the Implementation of Child Safety Officers in Local Governments as detailed at Attachment 2, and authorise the Chief Executive Officer of the City of Kwinana to submit the response to the Department by the 2 April 2021.

#### **DISCUSSION:**

The Royal Commission into Institutional Responses to Child Sexual Abuse was established in response to allegations of child sexual abuse in institutional contexts that had been emerging in Australia for many years. The Royal Commission's Final Report made 409 recommendations, with 310 applicable to the Western Australian State Government.

The recommendations of the Royal Commission emphasised that organisations working with children must be able to provide safe environments where the rights, needs and interests of children are met. The Royal Commission recommended a range of mechanisms to support child safe organisations, including 10 Child Safe Standards, which organisations the standards should apply to and the role of an independent oversight body to monitor and enforce the standards to promote child safety across organisations, and, importantly, the role of child safety officers in local government.

#### 14.1 IMPLEMENTATION OF CHILD SAFETY OFFICERS IN LOCAL GOVERNMENT

The Royal Commission also envisioned that the National Office for Child Safety would have a key role in collaborating with the Commonwealth, state and territory governments to support national consistency. It would do this by leading capacity building, continuous improvement of child safe initiatives through resources development, best practice material and evaluation. They also expected the National Office for Child Safety to promote participation and empowerment of children and young people.

Included at Attachment 1 is the Department of Local Government, Sports and Cultural Industries Discussion Paper on the Implementation of Child Safety Officers in Local Governments, in Western Australia, resulting from the recommendations from the Royal Commission.

The Discussion Paper outlines in detail the National Principles for Child Safe Organisations, what is happening in Western Australia to support implementation, and the role of local governments in the process.

Essentially the Royal Commission cited the fundamental role local governments play in assisting and resourcing communities across Australia. It highlighted the important roles local governments play in communities that impact on the safety of children including:

- providing services to children, for example libraries, swimming pools and childcare;
- providing spaces for community activities, for example halls, theatres and sports grounds;
- funding or contracting services;
- facilitating community education or outreach programs;
- regulating planning and development approvals, infrastructure and property services; and
- water and food inspection.

The active role local governments take in community development and community safety, particularly roles that impact on child safety, was recognised as an opportunity to integrate their direct responsibilities to children with their wider role within the community, particularly with respect to supporting smaller organisations within their communities to implement the National Principles and create child safe environments.

Recommendation 6.12 of the Royal Commission recommended that, with support from governments at the national, state and territory levels, local governments should designate child safety officer positions from existing staff profiles to carry out the following functions:

- a) developing child safe messages in local government venues, grounds and facilities;
- b) assisting local institutions to access online child safe resources;
- c) providing child safety information and support to local institutions on an as needs basis; and
- d) supporting local institutions to work collaboratively with key services to ensure child safe approaches are culturally safe, disability aware and appropriate for children from diverse backgrounds.

Child safety officers are intended to promote child safety within the organisation and support smaller community-based organisations providing services to children to create child safe environments. The role would be expected to support local staff and volunteers to build existing capacity around child safety within their organisations by providing information and assistance.



#### 14.1 IMPLEMENTATION OF CHILD SAFETY OFFICERS IN LOCAL GOVERNMENT

The Royal Commission's view was that child safety officers should work closely with the independent state oversight body responsible for monitoring and enforcing the National Principles, as they would be well placed to support smaller organisations to understand how they can be child safe.

The intent of the Royal Commission is for local governments to identify where they already have existing staff who could fulfil a role of promoting child safety within the organisation and supporting smaller local organisations to develop capacity in this area. Local governments could create new positions to facilitate implementation of this role where desired and resourcing allows, but this is not mandated.

Acknowledging the existing investment local governments make to promoting community safety, including child safety, the Royal Commission stated that local governments do not need to provide additional financial investment into implementing a child safety officer role and suggest that existing community safety positions within local governments could be expanded to align existing responsibility to strengthen child safety.

The Discussion Paper has two key aims:

- to develop a better understanding of the current role of local governments in promoting child safety and how the outcomes of this work are reported internally, to executive and to council; and
- to use this understanding of current work promoting child safety to inform development of an approach to meet recommendation 6.12 of the Royal Commission in implementing the child safety officer role.

#### **Comment on the Discussion Paper**

City Officers have considered the recommendations of the Royal Commission as outlined in the Discussion Paper and have determined that while the assumptions made by the Royal Commission are generally sound in that local government does provide a range of services to children, and can be well placed to support capacity building in local communities – across a wide range of areas, it is a generalisation to assume that all local governments are currently resourced to do so and have the capacity to undertake the role of child safety officer as outlined in the Royal Commission Report.

It is noted that all of the services provided by organisations recommended to be required to implement the National Principles are currently regulated by the state government. Caution should therefore be exercised with respect to the suggested role for local governments with this initiative so that it does not become mandated, does not become a cost shifting exercise thereby taking away resources from other already overstretched functional areas, does not impose additional onerous responsibilities onto local government, outside of the scope of existing functions, and that local governments continue to retain their ability to choose their level of involvement in the proposed program.

If the role as outlined is developed, it must be supported by a grants program to assist those local governments that do not have the capacity to undertake the functions because of resourcing or other constraints. Additionally the role should not be expanded to be a direct point of contact for community members or staff seeking advice on child protection matters. Existing staff are not adequately trained or resourced to undertake such a function. Similarly any scope creep with respect to community monitoring and reporting on the implementation of the National Principles, rather than provision of information and assistance only, is not supported.

#### 14.1 IMPLEMENTATION OF CHILD SAFETY OFFICERS IN LOCAL GOVERNMENT

City Officers also noted that information and resources and provision of training to staff/agencies should be centrally developed and distributed by the relevant national and state agencies to ensure consistency of messaging and prevention, again, of direct cost shifting to local governments.

The proposed submission to the Department of Local Government, Sport and Cultural Industries is included at Attachment 2.

##### **LEGAL/POLICY IMPLICATIONS:**

This report responds to the State Government of Western Australia's Department of Local Government, Sport and Cultural Industries Discussion Paper on the Implementation of Child Safety Officers in Local Governments, stemming from Recommendation 6.12 of the Royal Commission into Institutional Responses to Child Sexual Abuse.

##### **FINANCIAL/BUDGET IMPLICATIONS:**

There are no financial implications as a result of this report or its recommendation.

##### **ASSET MANAGEMENT IMPLICATIONS:**

There are no asset management implications as a result of this report or its recommendation.

##### **ENVIRONMENTAL IMPLICATIONS:**

There are no environmental implications as a result of this report or its recommendation.

##### **PUBLIC HEALTH IMPLICATIONS:**

The proposal as outlined in the Discussion Paper has the potential to build capacity amongst community organisations and individuals in the community with respect to child safe organisations and child safe practices, and the rights of children to have their needs and interests met.

The recommendations of this report have the potential to improve the following determinants of health and factors –

- Health Behaviour – Well being;
- Socio-economics – Community Safety.

## 14.1 IMPLEMENTATION OF CHILD SAFETY OFFICERS IN LOCAL GOVERNMENT

**STRATEGIC/SOCIAL IMPLICATIONS:**

This proposal will support the achievement of the following outcomes and objectives detailed in the Strategic Community Plan and Corporate Business Plan.

Plan	Outcome	Objective
Strategic Community Plan	A safe and welcoming place	1.3 Facilitate improved community safety and reduced crime levels
Strategic Community Plan	Strong Community Leaders	1.5 Actively work with the community to build local capacity

**COMMUNITY ENGAGEMENT:**

There are no community engagement implications as a result of this report or recommendation'.

**RISK IMPLICATIONS:**

The risk implications in relation to this proposal are as follows:

Risk Event	A response is not submitted to the Department of Local Government, Sport and Cultural Industries on the Discussion Paper on the Implementation of Child Safety Officers in Local Governments and unintended consequences result.
Risk Theme	Inadequate engagement practices
Risk Effect/Impact	Service Delivery Reputation
Risk Assessment Context	Operational

Consequence	Minor
Likelihood	Unlikely
Rating (before treatment)	Low
Risk Treatment in place	Avoid - remove cause of risk
Response to risk treatment required/in place	Submit a response to the Discussion Paper
Rating (after treatment)	Low

14.1 IMPLEMENTATION OF CHILD SAFETY OFFICERS IN LOCAL GOVERNMENT

**COUNCIL DECISION**

**383**

**MOVED CR W COOPER**

**SECONDED CR S LEE**

**That Council endorse the response to the Department of Local Government, Sport and Cultural Industries Discussion Paper on the Implementation of Child Safety Officers in Local Governments as detailed at Attachment 2, and authorise the Chief Executive Officer of the City of Kwinana to submit the response to the Department by the 2 April 2021.**

**CARRIED**

**6/0**

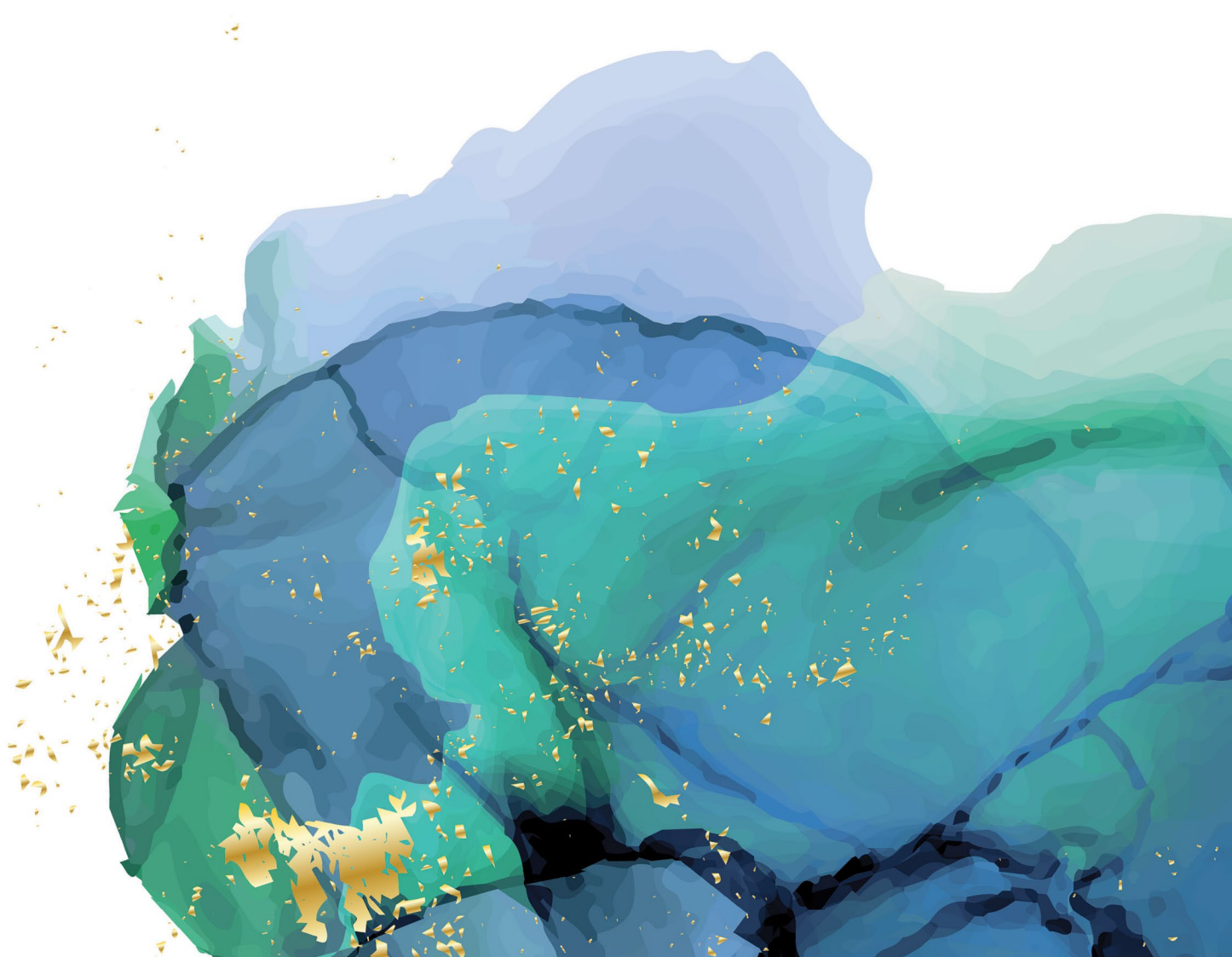


This initiative is part of the WA Government's action to create a Safer WA for Children by implementing the recommendations from the Royal Commission into Institutional Responses to Child Sexual Abuse.

# Discussion paper on the implementation of child safety officers in local governments

## Recommendation 6.12 of the Royal Commission into Institutional Responses to Child Sexual Abuse

December 2020



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## 1. Background

### 1.1 Royal Commission into Institutional Responses to Child Sexual Abuse

The Royal Commission into Institutional Responses to Child Sexual Abuse (the Royal Commission (Royal Commission)) was established in response to allegations of child sexual abuse in institutional contexts that had been emerging in Australia for many years. The Royal Commission's Final Report<sup>1</sup> made 409 recommendations, with 310 applicable to the Western Australian State Government.

The recommendations of the Royal Commission emphasised that organisations working with children must be able to provide safe environments where the rights, needs and interests of children are met. The Royal Commission recommended a range of mechanisms to support child safe organisations, including 10 Child Safe Standards (rec 6.5), which organisations the standards should apply to (rec 6.9) and the role of an independent oversight body to monitor and enforce the standards (rec 6.10 and 6.11) to promote child safety across organisations and the role of child safety officers in local government (rec 6.12).

The Royal Commission also envisioned that the National Office for Child Safety (rec 6.16 and 6.17) would have a key role in collaborating with the Commonwealth, state and territory governments to support national consistency. It would do this by leading capacity building, continuous improvement of child safe initiatives through resources development, best practice material and evaluation. They also expected the National Office for Child Safety to promote participation and empowerment of children and young people.

### 1.2 National Principles for Child Safe Organisations

The Royal Commission defined child safe organisations as those which create cultures, adopt strategies and take actions to prevent harm to children, including child sexual abuse. The Royal Commission proposed 10 Child Safe Standards be adopted to foster child safety and wellbeing in organisations across Australia, as referenced above.

The Council of Australian Governments endorsed the National Principles for Child Safe Organisations (National Principles) in February 2019. The National Principles (Appendix 1) incorporate the 10 Child Safe Standards recommended by the Royal Commission, with a broader scope that goes beyond child sexual abuse to include all forms of abuse or potential harm to children.

### 1.3 What is happening in Western Australia to support implementation?

In Western Australia, the Royal Commission recommendations related to the National Principles are being led by the Department of Communities and the Department of the Premier and Cabinet in partnership with key government agencies and the Commissioner for Children and Young People (CCYP).

The Department of the Premier and Cabinet is leading the development of advice to the State Government on an independent oversight system, which will include the monitoring and enforcing of the National Principles for organisations engaged in child-related work. The Royal Commission was of the view that all organisations should strive to be child safe but

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<sup>1</sup> <https://www.childabuseroyalcommission.gov.au/final-report>

recommended that organisations providing the following services should be required to implement the National Principles:

- accommodation and residential services for children;
- activities or services under the auspices of a religious denomination;
- childcare or childminding services;
- child protection services;
- activities or services where clubs and associations have a significant involvement by children;
- coaching or tuition services for children;
- commercial services for children;
- services for children with a disability;
- education services for children;
- health services for children;
- justice and detention services for children; and
- transport services for children.

The Department of Communities is leading the implementation of the National Principles through a range of administrative and legislative levers such as funding agreements and regulatory frameworks. They are also providing support to government and non-government agencies to implement the National Principles in preparation for independent oversight.

Western Australia currently has a voluntary approach to the implementation of the National Principles focused on capacity building, led by CCYP, while options for legally requiring implementation are developed. CCYP has enabling legislation to raise awareness, provide capacity building and consult with children. In 2019, CCYP revised their child safe resources to align with the National Principles.



## 2. Role of local governments

The Royal Commission cited the fundamental role local governments play in assisting and resourcing communities across Australia, particularly in regional and remote areas where access to resources and services is often more limited than for their urban counterparts.

The Royal Commission highlighted the important roles local governments play in communities that impact on the safety of children including:

- providing services to children, for example libraries, swimming pools and childcare;
- providing spaces for community activities, for example halls, theatres and sports grounds;
- funding or contracting services;
- facilitating community education or outreach programs;
- regulating planning and development approvals, infrastructure and property services; and
- water and food inspection<sup>2</sup>.

The active role local governments take in community development and community safety, particularly roles that impact on child safety, was recognised as an opportunity to integrate their direct responsibilities to children with their wider role within the community. Local governments are recognised as well placed to support smaller organisations within their communities to implement the National Principles and create child safe environments.

Through this consultation process the State Government has two key aims:

- to develop a better understanding of the current role of local governments in promoting child safety and how the outcomes of this work are reported internally, to executive and to council; and
- to use this understanding of current work promoting child safety to inform development of an approach to meet recommendation 6.12 of the Royal Commission in implementing the child safety officer role.

### 2.1 Engagement with the local communities

As part of the response to this consultation process it would be useful for local governments to consider how to engage their local communities in relation to this issue. This active engagement can ensure that the community's expectations and the local government's outcomes and investment in child safety are well understood.

Local governments undertaking the required periodical review of their Integrated Planning and Reporting, may choose to include discussions on child safety as part of the engagement with the local community.

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<sup>2</sup> Final Report: Volume 6, Making institutions child safe, p.300

### 3. Child safety officers

Recommendation 6.12 of the Royal Commission recommended that, with support from governments at the national, state and territory levels, local governments should designate child safety officer positions from existing staff profiles to carry out the following functions:

- a) developing child safe messages in local government venues, grounds and facilities;
- b) assisting local institutions to access online child safe resources;
- c) providing child safety information and support to local institutions on a need's basis; and
- d) supporting local institutions to work collaboratively with key services to ensure child safe approaches are culturally safe, disability aware and appropriate for children from diverse backgrounds.

Child safety officers are intended to promote child safety within the organisation and support smaller community-based organisations providing services to children to create child safe environments. The role would be expected to support local staff and volunteers to build existing capacity around child safety within their organisations by providing information and assistance.

The Royal Commission's view was that a child safety officer proximate to services and local industries would be especially important in regional and remote areas, given these communities are known to routinely miss out on resources and access to services that are available in urban centres. In regional and remote communities, child safety officers could be a conduit for information.

The Royal Commission's view was that child safety officers should work closely with the independent state oversight body responsible for monitoring and enforcing the National Principles, as they would be well placed to support smaller organisations to understand how they can be child safe.

The intent of the Royal Commission is for local governments to identify where they already have existing staff who could fulfil a role of promoting child safety within the organisation and supporting smaller local organisations to develop capacity in this area. Local governments could create new positions to facilitate implementation of this role where desired and resourcing allows. Volume 6<sup>3</sup> and Volume 14<sup>4</sup> of the Royal Commission's Final Report provide this recommendation in detail.

Acknowledging the existing investment local governments make to promoting community safety, including child safety, the Royal Commission stated that local governments do not need to provide additional financial investment into implementing a child safety officer role and suggest that existing community safety positions within local governments could be expanded to align existing responsibility to strengthen child safety.

The following portfolios may have existing roles that could be considered for alignment with child safety responsibilities and it is recognised that significant work is already occurring in

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<sup>3</sup> [https://www.childabuseroyalcommission.gov.au/sites/default/files/final\\_report\\_-\\_volume\\_6\\_making\\_institutions\\_child\\_safe.pdf](https://www.childabuseroyalcommission.gov.au/sites/default/files/final_report_-_volume_6_making_institutions_child_safe.pdf)

<sup>4</sup> [https://www.childabuseroyalcommission.gov.au/sites/default/files/final\\_report\\_-\\_volume\\_14\\_sport\\_recreation\\_arts\\_culture\\_community\\_and\\_hobby\\_groups.pdf](https://www.childabuseroyalcommission.gov.au/sites/default/files/final_report_-_volume_14_sport_recreation_arts_culture_community_and_hobby_groups.pdf)

these areas within some local governments to promote child safety, as recommended by the Royal Commission:

- Community safety;
- Community and club development;
- Governance and risk;
- Communications; and
- Disability Access and Inclusion.

It is also recognised that not all local governments have existing community safety positions or have limited capacity to expand the functions of these roles to include child safety. In fulfilling the functions of the child safety officer role, it is recognised that local government staff will need access to appropriate training. Where local governments have limited resources to create child safety officer positions the Royal Commission suggested that state and territory governments may be able to provide assistance.

### 3.1 Functions of the child safety officer

The four key functions of child safety officers, recommended by the Royal Commission, are outlined below with suggestions as to how each function may look in practice. It is noted that some of these examples may represent work already occurring in many local governments.

#### a) Developing child safe messages in local government venues, grounds and facilities

Developing child safe messages in local government venues, grounds and facilities promotes the knowledge and understanding of child safety by community members. Public messaging promotes the rights of children to feel safe as well as increasing the understanding of child safety by staff, volunteers and community members and acting as a deterrent for those who may intend to cause harm to children.

To implement this function would include:

- Working with key stakeholders including CCYP and the Working with Children Screening Unit to ensure that nationally consistent child safe messages are identified for use in local governments' venues, grounds and facilities.
- Working with internal communication teams to print posters/signs outlining nationally consistent child safe messages for their various venues, grounds and facilities.

#### b) Assisting local institutions to access online child safe resources

Institutions in local communities such as sole traders (i.e. music teachers, tennis coaches), private and community organisations (i.e. arts, cultural, community, sport and recreation groups, clubs and associations) may require assistance to access online child safe resources.

To implement this function would include:

- Facilitating the inclusion of information about child safety on their local government website including links to online child safe resource created by CCYP and the National Office of Child Safety. This would be in line with the current practice of many local governments in providing information and a link to Kidsport<sup>5</sup> on their websites.

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<sup>5</sup> Kidsport is a scheme that enables low income families to participate in community sport through provision of financial assistance towards club fees).

- Signposting local government staff and local organisations to CCYP, the National Office of Child Safety, and other relevant resources on the local government's website.

Other local government resources may also assist in facilitating this function. Community Resource Centres and libraries provide physical access to computers and the internet, and library staff could provide support to access suitable online child safe resources. Community, Club Development and Community Safety Officers may signpost to online resources within newsletters.

#### c) Provide child safety information and support to local institutions on a need's basis

Child safety officers are expected to provide general advice around promoting child safety and the implementation of the National Principles within organisations. For some local governments this may include hosting workshops/seminars with external providers.

It is expected that child safety officers would be supported by relevant agencies, such as CCYP, the National Office of Child Safety, or in the case of child protection concerns, the Western Australia Police Force or Department of Communities in meeting this function.

While it is not the intention of the Royal Commission for local government child safety officers to be a direct point of contact for community members or staff seeking advice on child protection matters, it would be important for anyone in this role to have appropriate knowledge and understanding of child abuse and neglect, as well as local child safeguarding procedures, in order to provide appropriate information, guidance and signposting. It is important for the local government to consider what support mechanisms are in place, to ensure the wellbeing of child safety officers when dealing with these matters and what specific areas of training would be required to build upon existing skills and knowledge of staff.

#### d) Support local institutions to work collaboratively with key services to ensure child safe approaches are culturally safe, disability aware and appropriate for children from diverse backgrounds

Communities within local government areas differ based on social demographics. The needs of supporting children from diverse backgrounds will differ based on the local population.

To implement this function would include:

- Identifying needs within the local community and key services providing support in meeting these needs.
- Working collaboratively with local government staff, responsible for supporting disability inclusion and access and promoting the needs of Aboriginal and culturally diverse children, to provide advice and support to local organisations on implementing child safe approaches that are accessible and inclusive for children with diverse needs.
- Linking local institutions with key services, including disability advocacy services, Aboriginal family support services or professional interpreters.

## 4. Progress to date across jurisdictions

Information relating to implementation of the recommendation for local government child safety officers in each state/territory is outlined below. The National Office for Child Safety intends to work with the Australian Local Government Association to develop a plan for implementation at a national level.

<b>Jurisdiction</b>	<b>Action to date</b>
Western Australia	Accepted in principle. The Department of Communities is leading implementation of child safety officers in partnership with the Department of Local Government, Sport and Cultural Industries. The Department of the Premier and Cabinet is leading the development of advice to the State Government on an independent oversight system.
Victoria	Accepted in principle. Volume 6 of the Final Report identifies that a rural city council in Victoria has appointed two child safety officers to help prevent and respond to concerns of abuse.  With the Victorian Government's support, Vicsport provides a 'helpdesk', which delivers assistance and advice to state sporting associations, regional sport assemblies, regional academies of sport, clubs and associations to assist them with cultural change, policy development, change management and communications to meet obligations in Victoria's Child Safe Standards.
New South Wales	Accepted in principle.
Australian Capital Territory	Accepted in principle. No reported progress.
Northern Territory	Accepted in principle. No reported progress.
Queensland	Listed for further consideration. The Queensland Government notes this recommendation is primarily the responsibility of the local government sector. The Queensland Government notes there are likely to be resource implications associated with implementing this recommendation, particularly for smaller remote, rural and Indigenous local governments, and will collaborate with the local government sector to identify the best way to support local institutions.
South Australia	Noted. This recommendation is seen to be the responsibility of local governments and is outside the scope of the South Australian Government's response to the Final Report
Tasmania	Accepted in principle. The Tasmanian Government agreed to work with the Local Government Association to progress this work.

## 5. Process for consultation with the local government sector

### Phase 1 – Endorsement of consultation process (complete)

September 2020

The Department of Communities and the Department of Local Government, Sport and Cultural Industries developed a proposed consultation process, which was shared, discussed and endorsed with the Local Government Professionals of Western Australia's (LG Pro) Community Development Network on 1 October 2020. The paper was also shared with the leadership group of the Western Australian Local Government Association's (WALGA) Local Government Community Safety Network for comment and feedback.

### Phase 2 – Consultation process (in progress)

December 2020 – April 2021

This discussion paper was developed by the Department of Communities and the Department of Local Government, Sport and Cultural Industries, in consultation with the Department of the Premier and Cabinet, CCYP and WALGA. It will be distributed to individual local governments through WALGA and LG Pro by Wednesday, 2 December 2020, with feedback required from individual local governments and peak bodies by Close of Business on Friday, 2 April 2021.

The State Government will facilitate presentations for local governments on the discussion paper via webinar on Monday, 14 December 2020 and Thursday, 4 February 2021. The webinars will be presented by the Department of Communities and the Department of Local Government, Sport and Cultural Industries with support from WALGA and LG Pro. To register your interest please email [childsafeguarding@dlgsc.wa.gov.au](mailto:childsafeguarding@dlgsc.wa.gov.au) prior to each webinar.

This consultation process aligns with the consultation on the design of the independent oversight system, which includes the monitoring and enforcing of the National Principles. Between November 2020 and February 2021, feedback is being sought from organisations likely to be impacted by the independent oversight and broader community stakeholders on particular elements of the system and how they will work. The local government sector is encouraged to provide feedback through the dedicated consultation webpage <http://www.wa.gov.au/independent-oversight-system-consultation>.

It is recommended that local government officers seek a position in relation to both consultation issues, child safety officers and the independent oversight system, from their respective councils during the consultation period to inform the organisation's feedback. Each local government should then respond individually to the consultation questions with specific feedback relevant to their geographical context.

### Phase 3 – Presentation of policy position and guidance on implementation

April – June 2021

The consultation findings from this discussion paper will inform a draft report by Friday, 7 May 2021. The draft report will outline the State Government's proposed implementation of recommendation 6.12 and provide guidance as to how local governments can implement the role of child safety officers, including what support will be needed.

The draft report will be shared with WALGA, LG Pro, the Department of the Premier and Cabinet and CCYP for comments and feedback before being made publicly available and distributed to the local government sector as a final report by Friday, 4 June 2021.

## 6. Consultation questions

Responses to consultation questions are due to the Department of Communities by Close of Business on Friday, 2 April 2021. Responses can be emailed to [csaroyalcommission@communities.wa.gov.au](mailto:csaroyalcommission@communities.wa.gov.au).

- 1) Please specify which local government you are responding on behalf of.

Click or tap here to enter text.

- 2) What is your role within the organisation?

Click or tap here to enter text.

- 3) Please consider each of the functions of a child safety officer and the three accompanying questions for each.

### ***a) Developing child safe messages in local government venues, grounds and facilities;***

In what ways is this function already being delivered within your local government and by which existing role/s and portfolio/s?

Click or tap here to enter text.

In what ways can this existing work be built upon to implement the function, as recommended by the Royal Commission?

Click or tap here to enter text.

What supports or training might be needed to achieve full implementation of this function?

Click or tap here to enter text.

### ***b) Assisting local institutions to access online child safe resources***

In what ways is this function already being delivered within your local government and by which existing role/s and portfolio/s?

Click or tap here to enter text.

In what ways can this existing work be built upon to implement the function, as recommended by the Royal Commission?

Click or tap here to enter text.

What supports or training might be needed to achieve full implementation of this function?

Click or tap here to enter text.

**c) *Providing child safety information and support to local institutions on a need's basis;***

In what ways is this function already being delivered within your local government and by which existing role/s and portfolio/s?

Click or tap here to enter text.

In what ways can this existing work be built upon to implement the function, as recommended by the Royal Commission?

Click or tap here to enter text.

What supports or training might be needed to achieve full implementation of this function?

Click or tap here to enter text.

**d) *Supporting local institutions to work collaboratively with key services to ensure child safe approaches are culturally safe, disability aware and appropriate for children from diverse backgrounds.***

In what ways is this function already being delivered within your local government and by which existing role/s and portfolio/s?

Click or tap here to enter text.

In what ways can this existing work be built upon to implement the function, as recommended by the Royal Commission?

Click or tap here to enter text.

What supports or training might be needed to achieve full implementation of this function?

Click or tap here to enter text.

- 4) Please specify any additional feedback in relation to the proposed implementation of child safety officers within Western Australia.

Click or tap here to enter text.



## 7. Further information and resources

### Contact information

Please contact any of the State Government representatives below to discuss queries relating to this paper or the consultation process.

Amanda Furnell  
Manager Royal Commission Implementation Team  
Strategy and Partnerships  
Department of Communities  
Phone:  
Email: [csaroyalcommission@communities.gov.wa.au](mailto:csaroyalcommission@communities.gov.wa.au)

Ben Armstrong  
Acting Director Strategic Coordination and Delivery  
Department of Local Government, Sport and Cultural Industries  
Phone: 08 9492 9622  
Email: [childsafeguarding@dlgsc.wa.gov.au](mailto:childsafeguarding@dlgsc.wa.gov.au)

Gordon MacMile  
Acting Executive Director Local Government  
Department of Local Government, Sport and Cultural Industries  
Phone: 08 9492 9752  
Mobile: 0418 968 952  
Email: [childsafeguarding@dlgsc.wa.gov.au](mailto:childsafeguarding@dlgsc.wa.gov.au)

### Resources

Further detail about the National Principles and resources relating to their implementation in organisations can be found on the websites listed below.

The Commissioner for Children and Young People Western Australia's website has a range of resources related to implementation of the National Principles, as well as links to provide information, resources and practical examples on each of the 10 National Principles.  
<https://www.ccyp.wa.gov.au/our-work/child-safe-organisations-wa/>

The National Office for Child Safety provides further resources to help organisations, children and young people, parents and carers learn about the National Principles and how they should be used.  
<https://childsafety.pmc.gov.au/what-we-do/national-principles-child-safe-organisations>

A Working with Children Check is a compulsory screening strategy in Western Australia and one strategy to keep children safe. The website includes a range of resources including creating a child safe organisation through recruitment and staff management.  
<https://workingwithchildren.wa.gov.au/about/safeguarding-children>

## Appendix 1:

### National Principles for Child Safe Organisations

1. Child safety and wellbeing is embedded in organisational leadership, governance and culture.
2. Children and young people are informed about their rights, participate in decisions affecting them and are taken seriously.
3. Families and communities are informed and involved in promoting child safety and wellbeing.
4. Equity is upheld and diverse needs respected in policy and practice.
5. People working with children and young people are suitable and supported to reflect child safety and wellbeing values in practice.
6. Processes to respond to complaints and concerns are child focused.
7. Staff and volunteers are equipped with the knowledge, skills and awareness to keep children and young people safe through ongoing education and training.
8. Physical and online environments promote safety and wellbeing while minimising the opportunity for children and young people to be harmed.
9. Implementation of the national child safe principles is regularly reviewed and improved.
10. Policies and procedures document how the organisation is safe for children and young people.

The Australian Human Rights Commission was engaged by the Australian Government Department of Social Services to lead consultations and development of the National Principles for Child Safe Organisations. The goal is to build cultures in all organisational settings to advance the safety and wellbeing of children and young people.

A full description of the National Principles can be found at:

[National Principles for Child Safe Organisations \(humanrights.gov.au\)](https://www.humanrights.gov.au/national-principles-for-child-safe-organisations)

**Response to the Department of Local Government, Sport and Cultural Industries on the Discussion Paper on the Implementation of Child Safety Officers in Local Governments.**

1. Please specify which local government you are responding on behalf of.

**City of Kwinana**

2. What is your role within the organisation?

**Director of City Engagement**

3. Please consider each of the functions of a child safety officer and the three accompanying questions for each.
  - a. Developing child safe messages in local government venues, grounds and facilities
    - In what ways is this function already being delivered within your local government and by which existing role/s and portfolio/s?
    - In what ways can this existing work be built upon to implement the function, as recommended by the Royal Commission?
    - What supports or training might be needed to achieve full implementation of this function?

**Child safe messages are currently not displayed on grounds, in venues and facilities other than at the aquatic centre. The function could be expanded upon by the existing responsible line managers in conjunction with the Marketing and Communications team with respect to style guide compliance and branding. Installation by the Facilities Management team would be possible. Rather than provide specific training or supports, it would be preferable if appropriate messaging was developed centrally and distributed to all local governments to ensure consistency of messaging, and resources provided to cover the cost of the additional signage and installation.**

- b. Assisting local institutions to access online child safe resources
    - In what ways is this function already being delivered within your local government and by which existing role/s and portfolio/s?
    - In what ways can this existing work be built upon to implement the function, as recommended by the Royal Commission?
    - What supports or training might be needed to achieve full implementation of this function?

**A range of information and resources is currently provided on line to the community including some child safe information and resources. The function could be expanded upon by the existing community development staff in conjunction with the Marketing and Communications team with respect to style guide compliance and branding. Rather than provide specific training or supports, it would be preferable if appropriate resources and messaging were developed centrally and distributed to all local governments to ensure consistency.**

- c. Providing child safety information and support to local institutions on a needs basis

- In what ways is this function already being delivered within your local government and by which existing role/s and portfolio/s?
- In what ways can this existing work be built upon to implement the function, as recommended by the Royal Commission?
- What supports or training might be needed to achieve full implementation of this function?

**The City currently convenes/participates in a number of community networking forums, runs skills development sessions for clubs and associations, runs a community leaders forum and operates a Volunteer Resource Centre. Child safe messaging and distribution can be carried out through the existing avenue, and additionally the City could become a co-ordination point for training requirements.**

- d. Supporting local institutions to work collaboratively with key services to ensure child safe approaches are culturally safe, disability aware and appropriate for children from diverse backgrounds
- In what ways is this function already being delivered within your local government and by which existing role/s and portfolio/s?
  - In what ways can this existing work be built upon to implement the function, as recommended by the Royal Commission?
  - What supports or training might be needed to achieve full implementation of this function?

**As for c. above.**

4. Please specify any additional feedback in relation to the proposed implementation of child safety officers within Western Australia.

**The City has considered the recommendations of the Royal Commission as outlined in the Discussion Paper and has determined that while the assumptions made by the Royal Commission are generally sound in that local government does provide a range of services to children, and can be well placed to support capacity building in local communities – across a wide range of areas, it is a generalisation to assume that all local governments are currently resourced to do so and have the capacity to undertake the role of child safety officer as outlined in the Royal Commission Report.**

**It is noted that all of the services provided by organisations recommended to be required to implement the National Principles are currently regulated by the state government. Caution should therefore be exercised with respect to the suggested role for local governments with this initiative so that it does not become mandated, does not become a cost shifting exercise thereby taking away resources from other already overstretched and important functional areas, does not impose additional onerous responsibilities onto local government outside of the scope of existing functions, and that local governments continue to retain their ability to choose their level of involvement in the proposed program.**

**If the role as outlined is developed, it must be supported by a grants program to assist those local governments that do not have the capacity to undertake the functions because of resourcing or other constraints. Additionally the role should not be expanded to be a direct point of contact for community members or staff seeking advice on child protection matters. Existing staff are**

**not adequately trained or resourced to undertake such a function. Similarly any scope creep with respect to community monitoring and reporting on the implementation of the National Principles, rather than provision of information and assistance only, is not supported.**

**The City has also noted, as outlined above, that information and resources and provision of training to staff/agencies should be centrally developed and distributed by the relevant national and state agencies to ensure consistency of messaging and prevention, again, of direct cost shifting to local governments.**

**Thank you for the opportunity to comment.**

## **15 Reports – Economic**

Nil

## 16 Reports – Natural Environment

### 16.1 City of Kwinana Climate Change Plan 2021-2026

#### DECLARATION OF INTEREST:

There were no declarations of interest declared.

#### SUMMARY:

Science has demonstrated that our climate is already experiencing changes, and these changes will continue to grow. In the future, Kwinana and its surrounding greater Perth region will experience higher average temperatures, increased frequency of hot days and heatwaves, lower rainfall, increased frequency and severity of extreme weather events, increased fire prone conditions and sea levels will continue to rise.

In response to, and in preparation for, climate change, the City has undertaken a planned approach to mitigation and adaptation activities for several years. The City's previous Climate Change Mitigation and Adaptation Plan 2015-2020 was recognised as one of the best examples in Western Australia in WALGA's Climate Change Governance Study 2020, and its implementation resulted in a number of key achievements.

Long term feedback has clearly demonstrated that sustainability, environment and climate change response is of high importance to the Kwinana community. In addition, it is crucial that the City undertake ongoing mitigation and adaptation action to protect public health, assets and the local environment in the face of current and future climate change impacts. In response to these demands, the City of Kwinana Climate Change Plan 2021-2026 (CCP) has been developed, to both replace the City's previous Climate Change Mitigation and Adaptation Plan 2015-2020 and direct future actions in a prioritised, coordinated and best practice manner.

A comprehensive review process was undertaken by City Officers in the development of the CCP. In brief, this included a complete review of the City's previous Plan and outcomes, review of current scientific climate data, benchmarking of goals and activities across all levels of government, review of best practice guidance, key stakeholder consultation and community feedback review. The review process directly informed the priority focus areas, goals, targets and actions of the CCP with respect to both mitigation and adaptation activities

The CCP's overarching mitigation goal is:

- to reduce carbon dioxide equivalent emissions generated by the City's operations by 5% per capita by 2025.

The CCP's overarching adaptation goals are:

- to identify and ensure that the increasing impacts of climate change are considered and addressed through the City's strategic framework and operations, and
- to support the community to better understand, prepare for and adapt to the impacts of climate change.

Emissions reduction was a primary focus of mitigation in the previous Climate Change Mitigation and Adaptation Plan 2015-2020, and while progression of further reductions will continue in the CCP, other aspects have now also been identified as key focus areas supported by a number of actions for implementation; including the following:

### 16.1 CITY OF KWINANA CLIMATE CHANGE PLAN 2021-2026

- Optimising data collection and reporting to enable more accurate benchmarking and the ability to set informed targets and plans towards future carbon neutrality;
- Optimising future energy efficiency opportunities through the improvement of the City's current energy projects framework;
- Improving community education and communications to increase transparency, awareness, enthusiasm and community trust; and
- Increasing activities in the conservation of green spaces, protection of existing trees and tree planting programs.

#### **OFFICER RECOMMENDATION:**

That Council adopt the City of Kwinana Climate Change Plan 2021-2026 as at Attachment A.

#### **DISCUSSION:**

In the development of the CCP, a comprehensive review was undertaken including the following key aspects:

- Review of the City's previous Climate Change Mitigation and Adaptation Plan 2015-2020, including establishing the current status of all actions and the evaluation of outcomes;
- Review of current evidence based scientific on climate change status and forecasts;
- Benchmarking of other Local, State and Commonwealth Government climate change goals, plans and actions;
- Review of best practice climate change planning sources;
- Review of the City's greenhouse gas emissions data, reporting and performance;
- Review of applicable City Policies and Plans;
- Consultation with key stakeholders; and
- Review of community feedback, primarily obtained through the Catalyse community perception surveying program and community workshop feedback surveying.

There were a number of key findings from the review process, including:

- The implementation of the previous Climate Change Mitigation and Adaptation Plan 2015-2020 resulted in a number of key achievements including:
  - A 30.2% reduction in greenhouse gas emissions per capita, far exceeding the Plan's 10% target
  - Installation of 7 solar systems, totaling 183kW, on City facilities, bringing the City's total solar photovoltaic capacity across all facilities to 217kW
  - Adoption of a Green Building Policy
  - Comprehensive energy audits of 4 City facilities
  - Recognition in WALGA's Climate Change Governance Study as one of the best plans in WA Local Government
- The City's electricity supplier, Western Power, significantly reduced their emissions in the generation of energy over the past decade with a shift towards greater renewable energy supply, which contributed largely to the City's emissions reduction;



### 16.1 CITY OF KWINANNA CLIMATE CHANGE PLAN 2021-2026

- Science has demonstrated that our climate has already been changing and these changes will continue to grow. In the future, Kwinana will experience:
  - Higher average temperatures and an increasing frequency of hot days and heatwaves
  - Lower annual rainfall, with less concentration of rain events over the winter period
  - The frequency and severity of extreme weather events will increase
  - Fire weather will continue to worsen due to higher temperatures and lower rainfall
  - Sea levels will continue to rise
- The Commonwealth Government has committed to a target of reducing nationwide greenhouse gas emissions by 26-28% from 2005 levels by 2030;
- The WA State Government has committed to being carbon neutral by 2050;
- A limited number of Local Governments have set carbon neutral targets, of varying timeframes;
- Much of the City's data relating to emissions and potential offsets are not currently centralised or readily available for collation, analysis or reporting by the City's Sustainability Officer;
- There is a high level of community importance and satisfaction in the City's efforts to promote and adopt sustainable practices and respond to climate change. There was an 83% satisfaction rating in the most recent October 2020 Catalyse community perception survey, with a performance index score of 59 being slightly above the industry average of 53, but notably lower than industry high of 71;
- The City has a high proportion of culturally and linguistically diverse residents, as well as a high level of social disadvantage, which increases the vulnerability of the Kwinana community to the impacts of climate change.

The findings of the review directly informed the goals, targets and actions of the CCP. The CCP's development also coincided with the development of the City's Strategic Community Plan and Local Planning Strategy. This enabled collaboration across all three key documents, ensuring consistency of several fundamental aspects. Furthermore, the City has chosen to align with a number of the United Nations Sustainability Goals, which will be identified in the upcoming Strategic Community Plan, and the actions outlined in the CCP will assist in operations and reporting within that framework.

Climate change mitigation refers to actions taken to reduce the direct or indirect generation of greenhouse emissions. The CCP includes an overarching mitigation goal "to reduce carbon dioxide equivalent emissions generated by the City's operations by 5% per capita by 2025". In contrast to the 30.2% reduction achieved over the preceding decade, this new goal appears conservative, however there are limited opportunities to substantially reduce the City's emissions further by 2025. Projects undertaken by the City to date, combined with the emissions reductions achieved by Western Power, has largely exhausted readily available opportunities to notably reduce the City's emissions further. Achieving further reductions now becomes more challenging and less fruitful, hence the modest target in the CCP.

Climate change adaptation refers to any decisions and actions taken either in anticipation of, or in response to, the impacts of climate change. The CCP includes the overarching adaptation goals "to identify and ensure that the increasing impacts of climate change are considered and addressed through the City's strategic framework and operations", and "to support the community to better understand, prepare for and adapt to the impacts of climate change".

### 16.1 CITY OF KWINANA CLIMATE CHANGE PLAN 2021-2026

The CCP contains an Implementation Plan outlining a multitude of actions aimed at achieving the City's mitigation and adaptation goals. There is a range of new and continuing actions, with key action categories including the following:

- Energy generation and efficiency;
  - Development of an Energy Sustainability Plan
  - Formalisation of the City's Revolving Energy Fund
  - Continued participation in the Cities Power Partnership
- Vehicle emissions reduction initiatives;
- Monitoring and review;
  - Optimising data collection, analytics and reporting
  - Enabling an informed pathway to future carbon neutrality to be developed
- Policy development and review;
  - Sea Level Rise Policy
  - Climate Change Policy
  - Green Building Policies
- Development and construction;
  - Sustainability initiatives to be encouraged in all development and construction activities in Kwinana
- Trees and green cover;
  - Development of an Urban Forest Plan
  - Improvements to the tree asset database
  - Increased protection for City managed trees;
- Water sustainability;
  - Waterwise Council re-endorsement
  - Review of the City's Sustainable Water Management Plan;
- Sustainability and environmental education initiatives;
- Community and staff health;
  - Heat stress, illness, vector-borne disease and food safety
- Urban heat.
  - Public shade and cool spaces

The review identified priority areas of key focus over the duration of the CCP's implementation; these include improving the City's data to enable informed planning towards future carbon neutrality, improving the City's energy related projects framework, improving community communication and education, and improving protection of green spaces and trees. The Implementation Plan contains several actions that support, and specifically target, these overarching key focus areas.

#### **LEGAL/POLICY IMPLICATIONS:**

There are no legal/policy implications associated with this report.

#### **FINANCIAL/BUDGET IMPLICATIONS:**

It should be noted that while there are now more actions than in the previous Plan, there are no direct financial implications resulting from the adoption and implementation of the CCP, with all aspects being achievable within existing budgets and long term financial plan projections.

**16.1 CITY OF KWINANA CLIMATE CHANGE PLAN 2021-2026****ASSET MANAGEMENT IMPLICATIONS:**

There are no asset management implications associated with this report.

**ENVIRONMENTAL IMPLICATIONS:**

Adoption and implementation of the Climate Change Plan 2021-2026 will result in environmental benefits through reduced greenhouse gas emissions, increased tree canopy coverage, reduced heat island effect and increased water efficiency.

**STRATEGIC/SOCIAL IMPLICATIONS:**

This proposal will support the achievement of the following outcome/s and objective/s detailed in the Corporate Business Plan and Strategic Community Plan.

Plan	Outcome	Objective
Corporate Business Plan and Strategic Community Plan	An energy efficient City	3.3 Promote the use of renewable energy within the City of Kwinana and reduce energy use where possible
	A City adapted to climate change	3.5 Understand the impacts of climate change and take a risk management approach to addressing these effects in future planning

**COMMUNITY ENGAGEMENT:**

Catalyse community perception survey data was analysed in the development of the CCP. Most recently, in October 2020, the Catalyse survey was distributed to 4,000 randomly selected households and received 285 responses to the City's efforts to promote and adopt sustainable practices and respond to climate change. An 83% positive rating was obtained with a performance score of 59 out of 100. This performance score was six points above the industry average of 53, however improvements can be made to reach the industry high score of 71.

An action within the CCP is the development and implementation of a community communication program, whereby the City will introduce community updates on performance and actions. This is intended to increase transparency, awareness, education, enthusiasm and trust from the community and build upon the City's capability to most effectively deliver climate change mitigation and adaptation outcomes.

**PUBLIC HEALTH IMPLICATIONS:**

The implementation of the CCP will help improve determinants of health with regard to the built environment, health behaviours and socio-economic aspects that will be impacted by climate change. The impacts of increased temperatures and frequency of extreme weather and fire events are of particular concern with regard to health implications. The high level of social disadvantage in the Kwinana community increases the population's vulnerability to these events, and further demonstrates the importance of the CCP in the mitigation of, and adaptation to, these impacts on the community.

## 16.1 CITY OF KWINANA CLIMATE CHANGE PLAN 2021-2026

**RISK IMPLICATIONS:**

The risk implications in relation to this proposal are as follows:

Risk Event	Failure to implement climate change mitigation and adaptation initiatives
Risk Theme	Inadequate asset sustainability practices Inadequate environmental management
Risk Effect/Impact	People/Health Financial Environment
Risk Assessment Context	Operational
Consequence	Major
Likelihood	Almost certain
Rating (before treatment)	Extreme
Risk Treatment in place	Reduce - mitigate risk
Response to risk treatment required/in place	Undertake a range of activities aimed at mitigation and adaptation to climate change impacts, in a prioritised, planned and coordinated manner through the implementation of the Climate Change Plan 2021-2026
Rating (after treatment)	Moderate

**COUNCIL DECISION**

384

**MOVED CR S LEE****SECONDED CR W COOPER**

**That Council adopt the City of Kwinana Climate Change Plan 2021-2026 as at Attachment A.**

**CARRIED  
6/0**

# Climate Change Plan 2021-2026



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## Executive Summary

Science has demonstrated that our climate is already experiencing changes, and these changes will continue to increase in intensity, severity and impact. In the future, Kwinana and the surrounding greater Perth region will experience higher average temperatures, increased frequency of hot days and heatwaves, lower rainfall, increased frequency and severity of extreme weather events, increased fire prone conditions and sea levels will continue to rise.

Long term feedback has clearly and consistently demonstrated that sustainability, environment and climate change response is of high importance to the Kwinana community. In addition, it is crucial that the City of Kwinana (the City) undertake ongoing mitigation and adaptation action to protect public health, assets and the local environment in the face of current and future climate change impacts. This *Climate Change Plan 2021-2026* (the Plan) defines how the City intends to continue our work in climate change mitigation, and how we will anticipate and respond to the changing climate. It follows on from the achievements of the previous *Climate Change Mitigation and Adaptation Plan 2015-2020* and will direct future actions in a prioritised, coordinated and best practice manner.

The Plan sets the following overarching mitigation and adaptation goals:

- Reduce carbon dioxide equivalent emissions generated by the City's operations by 5% per capita by 2025
- Identify and ensure that the increasing impacts of climate change are considered and addressed through the City's strategic framework and operations
- Support the community to better understand, prepare for and adapt to the impacts of climate change

There are a total of 41 actions outlined in the Plan that are aimed at achieving the goals and organisational objectives. These actions have been grouped into the following key categories:

- Energy generation and efficiency
- Vehicles
- Monitoring and review
- Trees and green cover
- Sustainability and environmental education
- Policy development and review
- Development and construction
- Coastal impacts
- Water sustainability
- Community and staff health
- Urban heat

The effects and local impacts of climate change will continue to worsen over time, and there will be an associated escalation of financial risks if the City takes a business-as-usual approach. Allowing climate change impact-related issues that appear insignificant now to continue and develop, may cause them to become financially unaffordable to address later on. Key actions to address this include the development of an Energy Sustainability Plan, the review of asset deterioration rates and the monitoring of human resourcing demands resulting from climate change impacts.

Any changes to budgeted future expenditure identified as necessary to the sustainability of the City's operations or assets will be appropriately justified and proposed through the City's Long Term Financial Plan process. All direct actions outlined in this Plan are considered achievable within the City's existing budgets and, as such, there are no direct financial implications resulting from the implementation of this Plan.

## 1. Introduction

### 1.1 Background

Australia's climate is on average  $1.4 \pm 0.24^{\circ}\text{C}$  warmer now than when national temperature records first began in 1910. In the southwest of Australia, annual rainfall has declined by approximately 16% since 1970, with the largest reduction in rainfall of 20% occurring between May and July (Bureau of Meteorology, 2020). It has become increasingly clear that greenhouse gas emissions caused by human activity have been the primary driver of the unprecedented and accelerating speed of changes to the Earth's climate since the Industrial Revolution (IPCC, 2014).

We can now say with certainty that the effects on the Earth's climate caused by anthropogenic greenhouse gas emissions will continue to intensify over time. In Perth this is experienced as increasing temperatures, reductions in winter rainfall, sea level rise and ocean warming and acidification (Bureau of Meteorology, 2020). These changes will have significant impacts on the future of City of Kwinana operations and its community.

At the same time, the Australian community are becoming increasingly aware of and concerned with the effects of climate change. 80% of Australians now believe that we are already experiencing impacts from climate change, and 71% think that Australia should be a world leader in identifying solutions to climate change (The Australia Institute, 2020).

### 1.2 Purpose

The Rio Declaration on Environment and Development, of which Australia is a signatory, states *"In order to protect the environment, the precautionary approach shall be widely applied by States according to their capabilities. Where there are threats of serious or irreversible damage, lack of full scientific certainty shall not be used as a reason for postponing cost-effective measures to prevent environmental degradation"* (United Nations, 1992).

As a developed country, Australia has the capacity to take decisive action to assist with mitigating climate change through reducing our reliance on activities that generate significant greenhouse emissions, and instead implementing solutions that are less greenhouse emissions intensive.

Negative impacts caused by climate change are already occurring, and are increasing in severity as we head into the future. Continuing with a business-as-usual approach in terms of corporate greenhouse emissions, strategies and operations, is not a viable option as the financial cost of climate change related impacts will escalate over time. The City of Kwinana will need to take proactive steps to adapt to these changes, to ensure continuity of current service levels across all of its operations. The City will also need to be prepared to be able to respond to climate-related incidents such as significant weather events and bushfires as they arise. Addressing impacts before they become unwieldy is therefore a prudent option.

Local Governments exist to serve the local community, and are in an ideal position to lead all parts of the community in making the transition towards a lower greenhouse emissions future. The City of Kwinana understands that we have a responsibility to lower our own corporate greenhouse gas emissions and adapt our operations, as well as support our local residents and businesses to be able to make achievable changes to lower their emissions and adapt to the effects of a changing climate.

The City of Kwinana population is the second fastest growing Local Government area within Western Australia. Using the Western Australia Tomorrow forecasts, the City's population is predicted to grow as follows: 2021 – 49,700 to 54,640; 2026 – 61,700 to 68,360; and 2031 – 71,610 to 78,630. The general trend demonstrates a population in the order of 85,000 within 15 years (Department of Lands, Planning and Heritage, 2020). The City of Kwinana will need to ensure that our services and community facilities continue to remain able to adequately serve the



needs of this rapidly growing resident population and that we maintain resilience in the face of a changing climate.

The City of Kwinana is also the most disadvantaged population in the Perth Metropolitan Area, and one of the most disadvantaged populations in the State. The City's Socio-Economic Indexes for Areas score is 972, with older suburbs being substantially lower. The level of disadvantage in the Kwinana community not only limits the community's capacity to adapt to impacts driven by a changing climate, but also requires the City's limited resources to be applied to key programs that are intended to support and build the community. In terms of unemployment, from 2006 to 2016 the rate in Kwinana grew from 5.8% to 10.8%, which was well above the all of WA unemployment rate of 7.8%. By June 2020, the unemployment rate increased to 11.3% (.id Community, 2020). The City will need to consider how some residents may require increasing access to City-run facilities and programs to seek advice and support to assist them to adapt to the impacts of climate change.

This Climate Change Plan defines how the City views climate change, its effects and the risks that climate change associated impacts may have on the City's corporate operations as well as on our community. The City will identify and organise relevant data to benchmark its current position and use this information to inform its long term planning and budget requirements in the context of climate change mitigation and adaptation.

The Plan contains a set of implementation actions, grouped into three categories:

- Mitigation actions will assist with the reduction of the City's corporate greenhouse gas emissions.
- Adaptation actions will assist the City and its community to anticipate and adapt to the progressive impacts of climate change.
- Actions addressing both mitigation and adaptation are listed in their own category.

Many of the actions proposed in this Plan that reduce the emission of greenhouse gases or adapt to a changing climate do not only address these specific challenges, but also provide additional benefits to our community. Examples include:

- Reducing our reliance on fossil fuel generated electricity and transport results in a reduction in pollutants released into our atmosphere, and cleaner air.
- Expanding our tree canopy cover in public open space and streetscapes not only mitigates against urban heat, but also improves the amenity of areas, encourages the use of these spaces by the community, and makes active transport (walking or cycling) more appealing. A more active lifestyle is associated with many potential physical and mental health benefits.
- Investing in energy efficiency retrofits and renewable power generation systems such as rooftop solar reduces ongoing electricity costs for City facilities.

### 1.3 International, National, State and Local Context

Australia formally ratified the *Paris Agreement*, a legally binding international treaty on climate change, on 10 November 2016. As part of this agreement, Australia committed to “*implement an economy-wide target to reduce greenhouse gas emissions by 26 to 28 per cent below 2005 levels by 2030*” (Australian Government, 2015).

The Australian Government has developed a variety of approaches that will contribute towards a reduction in overall national emissions. They have recognised that there needs to be significant investment in new and emerging technologies for Australia to be able to work towards its emissions reductions targets while maintaining a strong, competitive economy. The *Technology Investment Roadmap: First Low Emissions Technology Statement* identifies five priority technologies with expected emissions reductions for further research and development, while

offering the potential to either provide energy at a comparable price to that from high-emissions sources, or to create negative emissions outcomes or offsets:

- Clean hydrogen
- Energy storage
- Low carbon materials (steel and aluminium)
- Carbon capture and storage
- Soil carbon

(Commonwealth of Australia, 2020).

The Australian Government has also introduced a Climate Solutions Package that includes:

- A Climate Solutions Fund to incentivise corporate investment in emissions reductions.
- The provision of support to promote energy efficiency in homes, businesses and community organisations.
- The development of a national strategy that will assist in the development of electric vehicle infrastructure.
- Further investment in hydroelectric projects.

(Australian Government Department of Industry, Science, Energy and Resources, 2021).

The Government of Western Australia has set a target of net zero emissions for Western Australia by 2050 (Department of Water and Environmental Regulation, 2020). The City of Kwinana recognises that this ambitious goal will only be possible if all sectors contribute towards a reduction in greenhouse emissions.

Local Governments exist at the interface between government and community, and many Local Governments around Australia are leading the way with their response to climate change. However, of a total 139 local governments in Western Australia, only one third have publicly available climate change adaptation planning documents. The City of Kwinana is one of those Councils, with its previous *Climate Change Mitigation and Adaptation Plan 2015-2020* being recognised as one of the best in Western Australia (Climate Planning, 2020). This Plan will continue to build on the City’s progress and achievements to date and the City will continue to investigate opportunities and funding that align with Commonwealth and State Government programs that support climate change mitigation and adaptation activities.

## 1.4 Strategic Context

The City of Kwinana *Strategic Community Plan 2021-2031* has been developed in consultation with the local community and this is the overarching document that guides the City’s priorities. Outcomes and objectives directly relevant to this Climate Change Plan are as follows:

Our Outcomes	Our Strategic Objectives
<b>1. A naturally beautiful environment that is enhanced and protected</b>	1.1 Retain and improve our streetscapes and open spaces, preserving the trees and greenery that makes Kwinana unique
	1.2 Maintain and enhance our beautiful, natural environment through sustainable protection and conservation
<b>3. Infrastructure and services that are sustainable and contribute to health and wellbeing</b>	3.1 Develop quality, financially-sustainable infrastructure and services designed to improve the health and wellbeing of the community

The following City of Kwinana strategic documents influence or are influenced by this Climate Change Plan. Note that this list is not exhaustive, as the impacts of climate change would affect all of the City's operations and services to some degree. The actions set within this Plan will help to inform future reviews of these documents.

- Strategic Community Plan 2021-2031
- Corporate Business Plan 2021-2026
- Long Term Financial Plan
- Climate Change Policy
- Sustainable Water Management Plan 2018-2023
- Green Building Policy
- Risk Management Policy
- Local Planning Strategy 2021-2036
- Local Planning Policy 1 – Landscape Feature and Tree Retention
- Bike and Walk Plan 2018
- Public Health Plan 2019-2023
- Integrated Strategic Mosquito and Midge Management Plan
- Fleet Procurement Policy
- Asset Management Policy
- Building Asset Management Plan 2020-2021
- Stormwater Drainage Asset Management Plan 2020-2021
- Public Lighting Asset Management Plan 2020-2021
- Roads and Transport Asset Management Plan 2020
- Parks and Reserves Asset Management Plan 2020
- Landscape Development Guidelines 2020
- Irrigation Development Guidelines 2019
- Streetscape Upgrade Strategy 2019-2029
- Street Trees and Verge Treatments Policy
- Environmental Education Strategy 2019-2024
- Bushfire Risk Management Plan
- Kwinana Local Emergency Management Arrangements
- Waste Plan 2021-2025

## 1.5 Climate Change in Kwinana

Climate change projections for the future depend on the rate of greenhouse gas emissions over time. Therefore there are several different climate change scenarios, dependent on whether the world continues with a business-as-usual approach, or if actions are taken to reduce greenhouse gas emissions. The Intergovernmental Panel on Climate Change (IPCC), the leading international body on climate change, developed four representative scenarios based on projected atmospheric concentrations of carbon dioxide called the Representative Concentration Pathways (RCPs):

- RCP 2.6 represents a future where stringent mitigation actions have been taken on a global scale.
- RCP 4.5 represents a situation where some mitigation actions have been made.
- Pathways between RCP 6.0 and 8.5 represent an outcome following no additional efforts to limit emissions. In effect, these represent business-as-usual or 'baseline scenarios'.

(Intergovernmental Panel on Climate Change, 2014).

The following climate change projections relate to our local region, the south-west of Western Australia:

### *Rainfall*

- By 2030, winter rainfall is likely to decrease by up to 15% under all RCP scenarios.

- By 2090, winter rainfall is likely to decrease by up to 30% under RCP 4.5, or 45% under RCP 8.5.
- The length of periods of drought are likely to increase under all RCP scenarios.

#### Temperature

- By 2030, average temperatures are likely to increase by 0.5-1.2oC above those experienced between 1986-2005 under all RCP scenarios.
- By 2090, average temperatures are likely to increase by 1.1-2.1oC under RCP 4.5 and 2.6-4.2oC under RCP 8.5
- Extreme temperatures are likely to increase in line with the increases in average temperature, with a significant rise in maximum temperature experienced on hot days, as well as an increased frequency of hot days and heat waves.

#### Fire weather

- Climate change will contribute toward the worsening of fire weather into the future, the severity of which will be dependent on overall temperature increases and decreases in rainfall.

#### Sea level rise

- By 2030, our local sea level is likely to rise by 0.07-0.18m above the 1986-2005 level under all RCP scenarios.
- By 2090, our local sea level is likely to rise by 0.27-0.64m under RCP 4.5 and 0.38-0.84m under RCP 8.5.
- Our local ocean will also become more acidic over time, in proportion to emissions. (CSIRO, 2020).

### 1.6 Climate Change Effects, Impacts and Risks

Effects of climate change are being experienced on a worldwide scale. These include an increased frequency and severity of extreme weather events, rising sea levels due to glacial melting and water expansion, and varying temperatures and precipitation rates, with specific effects dependent on an area’s geographical location. The City of Kwinana is experiencing the following escalating climate change effects and their localised impacts:

Climate Change Effect	Localised Impacts
<b>Temperature change</b>	Increased number and severity of hot days and heat waves
	Increased evaporation from public open space areas, sports fields and parks
	Increased number of high fire risk days and potential for severe bushfires
<b>Reduced rainfall</b>	Drier vegetation and landscapes
	Reductions in groundwater recharge rates
	Reduction in water availability for properties reliant on rainwater tanks as their domestic water source
<b>Rising sea level</b>	Increased erosion and inundation of coastal areas
<b>Extreme weather events</b>	Localised flooding
	Severe wind events

The above localised climate change impacts have the potential to affect the City of Kwinana's operations and community in a variety of ways. Major potential risk categories to the City of Kwinana's operations and community associated with these localised impacts of climate change include the following:

#### *Damage or accelerated ageing of City and community infrastructure*

- An increase in the number of hot days and heat waves will potentially accelerate the rate of ageing of City owned or managed assets.
- An increase in the frequency of extreme weather events, with associated damaging wind speeds and localised flooding, may cause unanticipated damage to both City and community infrastructure.
- Reduced rainfall and water availability will lead to drier vegetation, with potential damage, increased prevalence of disease and fatalities of vegetation across public open space areas and streetscapes. Combined with increased average temperatures throughout the year and an increase in the number of hot days and heatwaves further exacerbating these effects, we will likely experience a decrease in the quality and amenity of vegetation over time.
- An increase in the number of fire risk days may lead to more frequent and/or severe bushfires, potentially damaging properties, buildings and other infrastructure.

#### *Public and City staff health and safety*

- An increase in the number of hot days and heat waves will elevate the risk of staff and residents experiencing heat stress-related effects and illness. The groups most vulnerable to this are outdoor workers, very young and elderly people, and residents (usually from lower socio-economic groups) who do not have mechanical cooling in their homes. Of those who do have access to mechanical cooling, some are financially unable to pay for the power needed to run it. During heat waves, sleep can be severely affected for people who do not have access to adequate mechanical cooling. This can affect their ability to function safely and effectively during the day, leading to potential injury and losses in productivity.
- An increase in the frequency of extreme weather events could expose City outdoor workers and local residents to the risk of potential harm or fatalities associated with damaging wind or localised flooding.
- An increase in the number of fire risk days may lead to more frequent and/or severe bushfires. These have the potential to significantly impact staff, local residents and businesses. People may need to be evacuated, and there could be injuries, illness or fatalities among those affected. There is also the potential for increased stress and mental illness among people in the community who have been affected by bushfire.
- Increased average temperatures combined with rainfall events can produce conditions that increase the risk of mosquito-borne viruses. In Western Australia, these include Ross River virus, Barmah Forest virus and Murray Valley encephalitis (Weeramanthri, 2020). Ross River Virus has recently been detected in mosquitoes sampled from the area of The Spectacles. This may indicate that changes in our local climate are already having an effect on mosquito species that are able to carry this virus.

#### *Water availability and cost*

- Reductions in rainfall over time have reduced the recharge of local groundwater aquifers, and groundwater reserves are diminishing. The City holds groundwater licences with the Department of Water and Environmental Regulation (DWER), enabling access to groundwater reserves for irrigating our public open spaces and streetscapes. There is no additional groundwater available in our local groundwater allocation area for the City to increase its licensed allocation, despite the continual expansion of the City's network of public open space areas associated with new residential developments.
- There is also the possibility in the future that DWER may reduce the City's licensed allocation in order to preserve local groundwater resources. Therefore the City will need to

be as efficient as possible with its groundwater use, and this may mean a prioritisation ranking of irrigated areas to identify those where irrigation rates can be further reduced or eliminated.

- Our rapidly growing local population will have a corresponding increase in demand on the Water Corporation-supplied scheme water system. Perth's rainfall has decreased significantly and will continue to decrease over time, and Water Corporation sources a large proportion of its water from energy intensive desalination plants. It is important that the City understands the need to educate and support our local residents and businesses to be as efficient as possible with their water use, to ensure we have an adequate supply of water into the future, as well as minimise the cost and greenhouse emissions associated with our scheme water supply.
- More frequent and severe bushfires will increase the demand for water needed to extinguish them. Local fire brigades source firefighting water from tanks fed by groundwater bores located at their fire stations. Larger fires can also require water to be taken from locally accessible water bodies that are suitable for the Department of Fire and Emergency Services aerial fleet to access. Section 44 of the Bush Fires Act 1954 grants the power to bush fire control officers and brigade officers to take water from any source apart from school or domestic water tanks for the purposes of fighting fires. This could potentially result in a lack of water at the properties and areas where this water is taken from, impacting on human activity and the health of ecosystems that rely on those water bodies.

#### *Vegetation health and biodiversity*

- Reduced rainfall and water availability combined with increased average temperatures throughout the year and an increase in the number of hot days and heatwaves will lead to drier, more stressed vegetation. This is likely to affect all City managed areas of vegetation, including natural reserves, parks and streetscapes. There will likely be a decrease in the quality and amenity of vegetation, plus an associated potential loss in local biodiversity over time, as species will not have enough time to adapt to a climate that is becoming hotter and drier at an unprecedented rate.
- As stated above, reductions in rainfall over time may result in the need for a prioritisation ranking of irrigated areas to identify those where irrigation rates can be further reduced or eliminated.
- An increase in the number of fire risk days may lead to more frequent and/or severe bushfires. Depending on the severity of a fire, it could lead to a loss of biodiversity among plant species that are not able to recover.

#### *Resource usage and cost*

- An increase in the number of hot days and heat waves will increase the need for mechanical cooling. This will generate additional costs associated with energy use to cool City owned facilities and City vehicles. There is also the potential that some City residents who are financially or otherwise unable to use mechanical cooling at home may access City facilities (such as the library, aquatic centre or community centres) more often to escape the heat.
- Energy use costs will increase for residents who choose to use mechanical cooling in their homes during hot weather.
- More frequent and severe bushfires or storm events may require additional mobilisation of City resources and staff. Resources will also be required to be allocated toward community support during the recovery period following major bushfire or weather incidents. This may impact increasingly on the City's business-as-usual operations and will need to be considered during future bushfire and emergency planning processes.

## 2. Progress to Date

The previous *Climate Change Mitigation and Adaptation Plan 2015-2020* set a climate change mitigation emissions reduction target to:

*Reduce carbon dioxide equivalent emissions from our own facilities by 10% per capita of resident population below 2009-2010 levels by 2020.*

In 2009-2010, annual City of Kwinana corporate greenhouse gas emissions were 4,527 tCO<sub>2</sub>-e (tonnes of carbon dioxide equivalent greenhouse gas emissions). In 2019-2020, the total was 3,161 tCO<sub>2</sub>-e. This represents a 30.2% reduction in annual corporate greenhouse gas emissions from the baseline year. This higher than expected overall reduction in the City's emissions was the result of a combination of the mitigation actions that the City implemented, along with a decreasing emissions factor by our electricity provider Western Power.

Over time, the relative proportion of Western Power's electricity generation powered by the combustion of fossil fuels has been decreasing, and there is now a greater mix of energy sources, including renewable sourced energy. This lower emissions factor feeds into the process that the City uses to calculate our own operational emissions, and has influenced our emissions total. The year 2019-2020 generated the lowest emissions out of the ten preceding years, and the temporary close down of City facilities associated with its COVID-19 response resulted in even lower emissions for that particular year.

The City has completed a number of actions since the previous *Climate Change Mitigation and Adaptation Plan 2015-2020* was reviewed in 2018. These include:

- Installation of solar photovoltaic systems onto John Wellard Community Centre (30kW), Fiona Harris Pavilion (10kW) and Wellard Pavilion (10kW).
- Continued street tree infill programs in the Kwinana Industrial Area and Bertram. Street trees were also planted that had been requested by residents and any street trees that had required removal were replaced. A total of 1,381 street trees were planted under these programs between 2016 and 2020.
- Completed an Australian Standards (AS/NZS 3598.1:2014) Level 3 energy audit and solar feasibility study at the Kwinana Recquatic. Two recommendations from the audit have been actioned:
  - Installation of occupancy sensors in the group fitness rooms that switch lights off when the rooms are not in use.
  - A localised control panel for the basketball stadium lighting, enabling users of the area to control the lights has also been installed. This represents a significant improvement, as the stadium lights had previously been kept on for the entire day each day the centre was operating.
- A seven-week sustainable living course run annually. Between 2015 and 2020, a total of 110 people completed the course. Feedback has consistently been very positive, with most participants making tangible and lasting changes to their lifestyle to improve sustainability.
- Continued to participate in the Cockburn Sound Coastal Vulnerability Project.
- Participated and reported to the Cities Power Partnership, a collective of local governments working to mitigate climate change.
- Continued to participate in the Switch Your Thinking sustainability and greenhouse emissions reduction initiative for local governments by hosting sustainability education workshops for the community and engaging in other education opportunities for sustainability officers in local government.
- Calculated an annual greenhouse gas (carbon dioxide equivalent) emissions inventory. Each year, the City collates its total electricity, gas and vehicle fuel consumption data. This data is used to calculate the amount of greenhouse gas emissions generated.

Other work that the City has completed with climate change-related benefits includes:

- The planting of around 70,000 local endemic species as part of the City's revegetation programs between 2016 and 2020. The City was recognised as a Local Government leader for having increased its green cover by 6% while experiencing the highest level of population growth in its climate category (Greener Spaces, Better Places, 2020). This is an encouraging sign that the City's revegetation and street tree expansion programs are having a demonstrated impact.
- Finalisation of the Landscape Development Guidelines 2020, which indicates a preference for local native species in landscaping within new developments.
- Completion of the updated Irrigation Development Guidelines 2019, specifying the efficient use of water in the irrigation of public open space areas. The City has also been progressively replacing older irrigation systems with more efficient systems. Of the City's 80 irrigation systems, 67 are now centralised control compatible, to enable more effective control over water application at different sites.
- The City installed an additional three data loggers on scheme water meters to add to its logger network on selected high water using facilities and parks. Leaks and water use anomalies have been identified using this network, that otherwise may have gone unnoticed until the end of the relevant billing period.

### 3. Mitigation

Greenhouse gas emissions generated by human activity have been proven to be the main contributor toward our rapidly changing climate (Bureau of Meteorology, 2020). In the context of this Plan:

*Climate change mitigation* refers to actions taken to reduce the direct or indirect generation of greenhouse emissions.

The City of Kwinana aims to minimise its greenhouse gas emissions as much as practicable. This will be achieved through a number of approaches, including:

- Increasing the energy efficiency of City facilities and public lighting.
- Installing renewable energy generation systems on facilities where an acceptable payback period can be demonstrated and budget is available.
- Investigating how the City can increase the efficiency of its vehicle fleet.
- Investigating the feasibility of acquiring electricity from energy providers that use renewable energy sources.

The City's greenhouse emissions in 2019-2020 were 30.2% lower than in 2009-2010. This reduction was achieved through the accumulated effects of the actions implemented from the previous *Climate Change Mitigation and Adaptation Plan 2015-2020* in combination with a decreasing emissions factor by our electricity provider, Western Power. Completed actions so far resulted in cost-effective significant emissions reduction outcomes, and further work in this area will likely have a relatively higher financial cost and/or result in relatively smaller reductions.

During the life of *Climate Change Plan 2021-2026*, the emissions reduction target will be more modest as the City's main focus will be on improving its capture and reporting of data relevant to its energy use and generation, as well as developing methodology to reliably represent its greenhouse gas emissions and carbon sequestration balance. Once the City has established a structured system of data collection, analysis and reporting to monitor its progress, this will enable accurate planning of its pathway towards a zero net emissions future.

The City has been a member of the Cities Power Partnership since 2018. This is a collective of Australian councils who are all working to mitigate climate change. The City committed to seven pledges associated with renewable energy, energy efficiency and community education that help



guide our climate change mitigation work, and report on these pledges annually. It is also a good platform for the City to learn from the great work of other councils.

The City's overarching climate change mitigation goal will be to achieve the following greenhouse gas emissions target during the life of this Climate Change Plan 2021–2026:

*To reduce carbon dioxide equivalent emissions generated by our own operations by 5% per capita of resident population below 2019-2020 levels by 2024-2025.*

The City will also work to offset its greenhouse emissions through the continuation and expansion of its tree planting and revegetation programs. The City's abovementioned recognition for achieving an overall increase in green cover despite rapid population growth represents an opportunity to further motivate our corporate programs and community to continue to green our local area. As identified through community consultation during the development of the City's *Strategic Community Plan 2021-2031*, our community highly values our local natural areas and green spaces, and this will help to drive further planting works. Actions within this Plan and the City's Local Planning Strategy will better formalise the City's work on expanding and protecting our network of urban trees and natural areas.

#### 4. Adaptation

The climate is changing at an unprecedented and accelerating rate (IPCC, 2014). Not only do we need to take action to mitigate against further greenhouse emissions, but the City and its community must also prepare for and adapt to its effects to ensure we can maintain a good quality of life for everyone. Impacts caused by our changing climate will continue to increase, and will require us to remain adaptable in how we approach potential and current issues that arise. This will allow us to remain financially sustainable into the future. In the context of this Plan:

*Climate change adaptation* refers to any decisions and actions taken either in anticipation of or in response to the impacts of climate change.

Major adaptations to climate change have already been made in the Perth Metropolitan area by the scheme water supplier, Water Corporation. Water Corporation has had to diversify its water sources due to changes in rainfall over time. During the 1980s, 65% of Perth's water came from storage dams linked to surface water catchments. Due to the reductions in annual rainfall, alongside an increase in average temperatures over time that increased evaporation rates, dams now only supply 15% of Perth's scheme water, with the largest source now being from climate-independent desalination using sea water (Water Corporation, 2020).

The City of Kwinana's overarching climate change adaptation goals are:

*To identify and ensure that the increasing impacts of climate change are considered and addressed through the City's strategic framework and operations, and*

*To support the community to better understand the impacts of climate change and to prepare for and adapt to them.*

#### 5. Implementation Plan

This section outlines actions that the City will take to continue to reduce its greenhouse gas emissions and to prepare for and adapt to local climate change-related impacts.

Climate change mitigation actions contribute toward a reduction in overall global emissions. This means that any emissions reductions achieved by the City of Kwinana will contribute towards how climate change progresses into the future, and all of its associated effects. It is for this reason that individual climate change effects and impacts are not listed in the Mitigation Actions section.

## 5.1 Mitigation Actions

Action Number	Action type	Action	Responsible position/ team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Risk categories addressed	Measure of success
<b>Energy Generation and Efficiency</b>								
1	Strategy	Develop an Energy Sustainability Plan to prioritise and direct capital works that increase efficiency and renewable energy generation. Each proposed project will involve a business case analysis.	Sustainability Officer	New	2021-2022	Determined by works plan	Resource usage and cost	Original Plan developed and reviewed annually.
2	Capital works	Install solar panels onto Council buildings as per the approved Energy Sustainability Plan. Appropriate facilities will be determined by historical electricity consumption patterns.	Sustainability Officer	Amended	Ongoing	Dependent on facilities chosen, majority funded through Revolving Energy Fund. Refer to Energy Sustainability Plan	Resource usage and cost	Installation of systems completed as planned in Energy Sustainability Plan.
3	Capital works	Implement energy efficiency retrofit works to Council facilities as per the approved Energy Sustainability Plan. Appropriate facilities will be determined by analysing previous energy consumption.	Sustainability Officer, Manager Asset Management Services	New	Ongoing	Refer to Energy Sustainability Plan	Resource usage and cost	Implementation of energy efficiency projects as planned in Energy Sustainability Plan.
4	Investigation	Investigate viability of participation in a WALGA developed Green Power Purchase Agreement. Commit to participation if cost-beneficial.	Sustainability Officer, Contracts Officer, Manager Asset Management Services	New	2021-2022	Budget to match or be cheaper than electricity tariffs currently paid	Resource usage and cost	Green Power Purchase Agreement entered into if it matches current electricity tariffs paid.
5	Capital works	LED sports lighting to be considered for oval lighting renewal or new installations.	Asset Management Services	Existing	Ongoing	Captured in capital works/asset renewal budget	Resource usage and cost	All new/replaced sports lighting is energy efficient.
6	Investigation	Conduct an Australian Standards Level 3 Energy Audit at Darius Wells Library and Resource Centre.	Sustainability Officer	Existing	2021-2022	\$10,000 (in budget)	Resource usage and cost	Energy audit completed and actions selected for inclusion in LTFP.
7	Investigation	Optimise the existing real time energy monitoring at the Darius Wells Library and Resource Centre to provide data that can be used to refine HVAC system settings and inform other energy efficiency work throughout the facility.	Sustainability Officer, Coordinator Facility Maintenance	Existing	2021-2022	\$1,000 (in budget)	Resource usage and cost	Real time energy monitoring system suits our data collection requirements.
8	Investigation	Prepare a costed project proposal to undertake a pilot retrofit of a selected number of Western Power-owned streetlights to LED to reduce energy cost and emissions. This would be used to inform future revisions of the Public Lighting Asset Management Plan and operating expenditure.	Technical Officer Civil Infrastructure Assets, Engineering Services	Amended	2022-2023	Captured in asset renewal budget	Resource usage and cost	Business case completed for retrofitting selected streetlights to LED.
<b>Vehicles</b>								
9	Investigation	Investigate setting maximum emission thresholds for fleet vehicles and potential for plug in hybrid electric vehicles (PHEV) or electric vehicles (EV), to inform amendment of the Fleet Procurement Policy.	Human Resources (Policy owner), City Operations Technical Services (Policy implementation)	Amended	2022-2023	Staff time	Resource usage and cost	All new fleet vehicles are within specified emission threshold, once established.

Action Number	Action type	Action	Responsible position/ team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Risk categories addressed	Measure of success
10	Investigation	Investigate feasibility of electric vehicle charging stations within the City of Kwinana.	Manager Economic Development, Sustainability Officer, Manager Asset Management Services, Manager Engineering Services, Planning Services	New	2025-2026	Staff time	Resource usage and cost	Investigation completed.
<b>Monitoring and Review</b>								
11	Participation	Continue to participate in the Cities Power Partnership (CPP). The City committed to seven pledges as part of joining the CPP in 2018, and has already implemented a number of actions.	Sustainability Officer	Existing	Annually	Staff time	Resource usage and cost	Reporting completed annually on the City's seven pledges.
12	Participation	Consult with relevant stakeholders annually to monitor progress on actions and amend action planning as necessary.	Sustainability Officer, various officers	Amended	Ongoing	Staff time	Resource usage and cost	Consultation undertaken annually.
13	Investigation	Identify approved methodology that can be used to calculate the carbon offsets achieved by the City's tree planting and revegetation programs. Also investigate the potential for all vegetation within City managed parks and reserves to be included as carbon sequestration assets.	Sustainability Officer	Amended	2022-2023	Staff time	City and community infrastructure, Vegetation health and biodiversity	Appropriate methodology selected and annual calculation of carbon offsets has commenced.

## 5.2 Actions addressing both Mitigation and Adaptation

Action Number	Action type	Action	Responsible position/ team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Relevant climate change effect(s)	Relevant localised climate change consequences addressed	Risk categories addressed	Measure of success
<b>Trees and green cover</b>										
14	Strategy	Develop and adopt an Urban Forest Plan that will outline how the City will preserve, expand and manage its urban forest.	Senior Environmental Planner, Landscape Architects, Coordinator Technical Operations	Existing	2021-2022	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, increased evaporation from POS areas, sports fields and parks, drier vegetation and landscapes	Vegetation health and biodiversity	Urban Forest Plan adopted by Council.
15	Strategy	Develop a centralised database of all City planted and removed street trees. The database is to be updated following any works, to track planting and mortality numbers and inform future planting programs.	Coordinator Technical Operations, Landscape Architects	New	2021-2022	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, increased evaporation from POS areas, sports fields and parks, drier vegetation and landscapes	Vegetation health and biodiversity	Database of street tree plantings developed and utilised.

Action Number	Action type	Action	Responsible position/team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Relevant climate change effect(s)	Relevant localised climate change consequences addressed	Risk categories addressed	Measure of success
16	Strategy	Develop and maintain a centralised Tree Asset Database, including digital spatial data on all City managed trees. Data from the above action (15) will form a component of this.	Coordinator Technical Operations, Landscape Architects, Asset Management services	Amended	2024-2025	Dependent on approximate number of trees and specific data needing capture.	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, increased evaporation from POS areas, sports fields and parks, drier vegetation and landscapes	Vegetation health and biodiversity	Tree Asset Database developed.
17	Strategy	Develop and implement processes that ensure protection of City managed trees through relevant Local Laws, Policy and statutory means.	Essential Services, Governance Services, Sustainability Officer, City Operations, Engineering Services, Planning Services	New	2022-2023	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, increased evaporation from POS areas, sports fields and parks, drier vegetation and landscapes	City and community infrastructure, Vegetation health and biodiversity	Statutory framework in place to enforce protection of City managed trees.
18	Education	Develop a Street Tree Education Program to use in conjunction with Living Green and the annual street tree planting program to encourage residents to value and care for their street trees.	Sustainability Officer, Landscape Architects, City Operations, Marketing and Communications	Amended	2021-2022	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, drier vegetation and landscapes	Vegetation health and biodiversity, Public and City staff health and safety	Street Tree Education Program material in use.
19	Capital works	Continue street tree infill planting programs throughout the City as set in annual works plans.	Coordinator Parks, Landscape Architects	Existing	Annually	Refer to relevant works plans	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, increased evaporation from POS areas, sports fields and parks, drier vegetation and landscapes	Vegetation health and biodiversity	Street tree infill programs completed as planned.
<b>Sustainability and Environmental Education</b>										
20	Education	Continue to deliver an annual seven week sustainable living course.	Sustainability Officer	Existing	Annually	\$7500/year (in budget)	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, drier vegetation and landscapes, reductions in groundwater recharge rates	Public and City staff health and safety, Resource usage and cost, Water availability and cost, Vegetation health and biodiversity	Course delivered annually.
21	Education	Identify and collate environmental, sustainability and waste education resources, tools and services into an integrated package that the City can offer to schools, local businesses, community groups and sporting groups. This will form a component of the review and update of the Environmental Education Strategy.	Sustainability Officer, Bushcare Officer, Waste Education Officer, Marketing and Communications	Existing	Ongoing	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, drier vegetation and landscapes, reductions in groundwater recharge rates	Resource usage and cost	Environmental, sustainability and waste education package developed. Relevant groups have been offered these resources.

Action Number	Action type	Action	Responsible position/team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Relevant climate change effect(s)	Relevant localised climate change consequences addressed	Risk categories addressed	Measure of success
22	Participation	Continue to utilise the Switch Your Thinking program: run three community workshops per annum and promote the Rewards for Residents scheme.	Sustainability Officer	Existing	Ongoing	\$5000/year (in budget)	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, drier vegetation and landscapes	Resource usage and cost	Community workshops delivered annually, promotion of Rewards for Residents undertaken.
23	Educate	Develop and implement an annual communications plan that promotes the City's climate change-related work and achievements with our community and stakeholders.	Sustainability Officer, Marketing and Communications team	New	Ongoing	Staff time	Temperature change, reduced rainfall, rising sea level, extreme weather events	Increased number and severity of hot days and heat waves, increased evaporation from public open space areas, sports fields and parks, increased number of high fire risk days and potential for severe bushfires, drier vegetation and landscapes, reductions in groundwater recharge rates, increased erosion and inundation of coastal areas, localised flooding, severe wind events	All categories indirectly	Climate change work and achievements communicated to our community at least twice a year.
<b>Policy Development and Review</b>										
24	Strategy	Update the Climate Change Policy to align with the updated Climate Change Plan. Include the development of a Revolving Energy Fund Framework to formalise the allocation and use of funds saved through the implementation of renewable energy generation and energy efficiency projects.	Sustainability Officer, Manager Finance, Governance Services	New	2021-2022	Staff time	Temperature change, reduced rainfall, rising sea level, extreme weather events	Increased number and severity of hot days and heat waves, increased evaporation from public open space areas, sports fields and parks, increased number of high fire risk days and potential for severe bushfires, drier vegetation and landscapes, reductions in groundwater recharge rates, increased erosion and inundation of coastal areas, localised flooding, severe wind events	All risk categories	Updated Climate Change Policy adopted by Council.

Action Number	Action type	Action	Responsible position/team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Relevant climate change effect(s)	Relevant localised climate change consequences addressed	Risk categories addressed	Measure of success
25	Strategy	Update the Green Building Policy to be able to accommodate regular improvements in available technologies and efficiency levels. This may involve the development of a related set of guidelines referring to technologies and efficiency standards available at the time of planning new building projects or renovations.	Sustainability Officer, Technical Officer – Building Infrastructure Assets, Manager Asset Management Services	New	2023-2024	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves	Resource usage and cost, Water availability and cost.	Green Building Policy updated to accommodate regular improvements in available technologies and efficiency levels.
<b>Development and Construction</b>										
26	Investigation	Investigate opportunities to encourage applicants to include sustainability related initiatives in their developments via the planning approvals process. Also consider opportunities to better retain endemic native vegetation within development sites.	Sustainability Officer, Planning Services	New	2022-2023	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, drier vegetation and landscapes	Resource usage and cost	Sustainability related development provisions identified and considered for inclusion in Structure Planning, subdivision design, Local Development Plans and/or Development Applications.
27	Investigation	Investigate opportunities to encourage applicants to include sustainability related initiatives in private construction projects via the building approvals process.	Sustainability Officer, Building Services	New	2022-2023	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heatwaves, drier vegetation and landscapes	Resource usage and cost	Enforcement of any sustainability provisions set in Local Development Plans.
<b>Monitoring and Review</b>										
28	Investigation	Align with and report to the specific <i>UN Sustainability Development Goals</i> (United Nations, 2015) that the City of Kwinana has identified as relevant to the City's operations in the <i>Strategic Community Plan 2021-2031</i> . These goals are: Goal 1 – No Poverty (indirect influence) Goal 2 – Zero Hunger (indirect influence) Goal 3 – Good Health and Well-being (direct influence) Goal 5 – Gender Equality (direct influence) Goal 8 – Decent Work and Economic Growth (direct influence) Goal 9 – Industry, Innovation and Infrastructure (direct influence) Goal 10 – Reduced Inequality (direct and indirect influence) Goal 11 – Sustainable Cities and Communities (direct influence) Goal 12 – Responsible Consumption and Production (direct and indirect) Goal 13 – Climate Action (direct and indirect influence) Goal 14 – Life Below Water (indirect influence) Goal 15 – Life on Land (direct and indirect) Goal 16 – Peace and Justice Strong Institutions (direct and indirect) Goal 17 – Partnerships to achieve the Goal (direct and indirect influence)	Sustainability Officer	New	2022-2023, Ongoing	Staff time	Temperature change, reduced rainfall, rising sea level, extreme weather events	Increased number and severity of hot days and heat waves, increased evaporation from public open space areas, sports fields and parks, increased number of high fire risk days and potential for severe bushfires, drier vegetation and landscapes, reductions in groundwater recharge rates, increased erosion and inundation of coastal areas, localised flooding, severe wind events	Resource usage and cost, Water availability and cost, Vegetation health and biodiversity	Reporting of relevant actions from this Plan against the City's selected UN Sustainability Goals completed annually.

### 5.3 Adaptation Actions

Action Number	Action type	Action	Responsible position/team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Relevant Climate Change effect(s)	Relevant localised climate change consequences addressed	Risk categories addressed	Measure of success
<b>Coastal Impacts</b>										
29	Strategy	Develop a Sea Level Rise Policy incorporating the projections completed as part of the Cockburn Sound Coastal Alliance project.	Senior Environmental Planner, Coastal and Marine Program Manager	Existing	2022-2023	Staff time	Rising sea level, extreme weather events	Increased erosion and inundation of coastal areas, localised flooding, severe wind events	City and community infrastructure, Public and City staff health and safety	Sea Level Rise Policy adopted by Council.
30	Participation	Continue to participate in the Cockburn Sound Coastal Vulnerability Project.	Coastal and Marine Program Manager	Existing	Ongoing	Staff time	Rising sea level, extreme weather events	Increased erosion and inundation of coastal areas, localised flooding, severe wind events	City and community infrastructure	City staff attending meetings and contributing toward Cockburn Sound Coastal Vulnerability Project.
<b>Sustainability and Environmental Education</b>										
31	Education	Investigate options for including the community in citizen science and education initiatives to raise awareness of the effects of climate change locally and connect community members with activities that assist in adapting to the effects of climate change.	Sustainability Officer, Community Engagement team	New	2023-2024	Staff time, grants/industry sponsorship	Temperature change, reduced rainfall, rising sea level, extreme weather events	Increased number and severity of hot days and heat waves, drier vegetation and landscapes, increased erosion and inundation of coastal areas, localised flooding, severe wind events	City and community infrastructure	Community education initiatives identified and planned for implementation.
<b>Water Sustainability</b>										
32	Participation	Retain Gold Waterwise Council endorsement to promote corporate water efficiency. Aim to achieve Platinum Waterwise Council status.	Sustainability Officer	New	Gold Annually – Platinum 2025-2026	Staff time	Temperature change, reduced rainfall	Increased number and severity of hot days and heat waves, increased evaporation from public open space areas, sports fields and parks, drier vegetation and landscapes	Water availability and cost, Resource usage and cost, Vegetation health and biodiversity	Gold Waterwise Council endorsement maintained. Platinum endorsement achieved.
33	Participation	Attain and retain Gold Waterwise Aquatic Centre endorsement. Aim to achieve Platinum endorsement.	Recquatic Centre Operations Supervisor	New	Gold Annually – Platinum 2025-2026	Staff time plus any proposed capital works	Temperature change, reduced rainfall	Increased number and severity of hot days and heat waves	Water availability and cost, Resource usage and cost	Recquatic Gold Waterwise Aquatic Centre endorsement maintained and Platinum endorsement achieved.
34	Investigation	Investigate further opportunities to hydrozone parks and/or replace traditional plants with waterwise/endemic species.	Coordinator Parks, Technical Officer Parks Operations, Technical Officer Public Open Space Infrastructure Assets, Landscape Architects	New	Ongoing / 2025-2026	Staff time plus any proposed retrofit works	Temperature change, reduced rainfall	Increased number and severity of hot days and heat waves, increased evaporation from public open space areas, sports fields and parks, drier vegetation and landscapes, reductions in	Water availability and cost, Resource usage and cost, Vegetation health and biodiversity	All parks are hydrozoned where possible and plants are Waterwise and/or appropriate for their location.

Action Number	Action type	Action	Responsible position/team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Relevant Climate Change effect(s)	Relevant localised climate change consequences addressed	Risk categories addressed	Measure of success
								groundwater recharge rates		
35	Investigation	Investigate opportunities for expanding Water Sensitive Urban Design (WSUD) components throughout the City's stormwater drainage infrastructure at time of asset renewal.	Coordinator Engineering Design, Manager Asset Management, Coordinator Infrastructure Operations	New	2024-2025	Asset renewal budget	Temperature change, reduced rainfall, rising sea level, extreme weather events	Drier vegetation and landscapes, reductions in groundwater recharge rates, increased erosion and inundation of coastal areas, localised flooding	City and community infrastructure, Vegetation health and biodiversity, Water availability and cost	WSUD is considered at the time of asset renewal/replacement for drainage assets.
<b>Community and Staff Health</b>										
36	Strategy	Review and update Public Health Plan to consider the increasing effects of climate change on vulnerable members of our community, and how to ensure appropriate support.	Manager Environment and Health, Manager Community Engagement	New	2022-2023	Staff time	Temperature change, reduced rainfall, extreme weather events	Increased number and severity of hot days and heat waves, increased number of high fire risk days and potential for severe bushfires, localised flooding, severe wind events	Public and City staff health and safety, Vegetation health and biodiversity	Updated Public Health Plan adopted by Council.
37	Strategy	Review and update risk assessments and adapt relevant working environment procedures for any City staff who undertake work outside of temperature controlled environments to minimise the risk of heat related illness or injury.	Health, Safety and Injury Management Advisor	New	2021-2022	Staff time	Temperature change	Increased number and severity of hot days and heat waves	Public and City staff health and safety	Risk assessments reviewed and updated to include provisions to mitigate against the effects of heat on employees.
<b>Urban Heat</b>										
38	Strategy	Develop a Shade Policy to respond to increasing temperatures and investigate opportunities for the City to increase the amount of shade throughout public areas consistent with the Urban Forest Strategy and Landscape Development Guidelines. This may involve a combination of tree planting and shade infrastructure.	Sustainability Officer, Planning Services, Landscape Architects, Coordinator Parks, Asset Management, Coordinator Technical Operations	New	2024-2025	Staff time plus any proposed capital works	Temperature change, reduced rainfall	Increased number and severity of hot days and heat waves, increased evaporation from public open space areas	Vegetation health and biodiversity	Shade Policy adopted and shade available at all POS areas.
39	Investigate	Investigate the possibility of developing a 'heat refuge' program, where specific air-conditioned buildings around the City of Kwinana could be made accessible to vulnerable community members on hot days for those who otherwise do not have access to a cooled environment.	Library and Community Resource Centre Manager, Manager Community Engagement	New	2025-2026	Staff time, potential cost of extended opening hours for some community centres	Temperature change	Increased number and severity of hot days and heat waves	Public and City staff health and safety	Heat refuge program developed.



Action Number	Action type	Action	Responsible position/team	New/ Existing/ Amended Action	Timeframe for completion	Budget	Relevant Climate Change effect(s)	Relevant localised climate change consequences addressed	Risk categories addressed	Measure of success
<b>Monitoring and Review</b>										
40	Strategy	Investigate and develop a centralised system for recording City resources utilised in response to climate change related events (such as storm and bushfire events). This system will be used to enable monitoring of the ongoing impact that these events have on the financial cost of the diversion of staff and other resources. This data will be used to better inform ongoing resource planning. Information collected may also demonstrate a need for additional preventative works to mitigate against escalating impacts from these events.	Sustainability Officer	New	2025-2026	Staff time	Temperature change, reduced rainfall, extreme weather events	Increased number and severity of hot days and heat waves, increased evaporation from public open space areas, sports fields and parks, increased number of high fire risk days and potential for severe bushfires, drier vegetation and landscapes, increased erosion and inundation of coastal areas, localised flooding, severe wind events	Resource usage and cost, Water usage and cost, Public and City staff health and safety	System for monitoring resource usage has been developed and data is being collected.
41	Strategy	Asset management and renewal processes consider the potential acceleration of expected ageing of infrastructure due to the effects of climate change.	Manager Asset Management Services	New	Next review of Asset Management Plans	Asset renewal budget	Temperature change, reduced rainfall, rising sea level, extreme weather events	Increased number and severity of hot days and heat waves, increased evaporation from public open space areas, sports fields and parks, increased number of high fire risk days and potential for severe bushfires, drier vegetation and landscapes, increased erosion and inundation of coastal areas, localised flooding, severe wind events	City and community infrastructure, Resource usage and cost, Water usage and cost	Asset Management Plan anticipates accelerated infrastructure ageing due to climate change impacts.

## 6. Financial Implications

Energy efficiency and renewable energy technologies are rapidly evolving. Research and investment in these areas is being driven by the increasing cost of non-renewable sourced energy, a push to reduce greenhouse emissions and public sentiment. Advances in these technologies has led to them becoming more affordable, and this will further assist the City in transitioning its infrastructure and operations to a low emissions future.

The effects and relevant local impacts of climate change will continue to worsen over time, and there will be an associated escalation of financial risks if the City takes a business-as-usual approach in terms of its strategic position and operations. Allowing climate change impact-related issues that appear insignificant now to continue and develop, may cause them to become financially unaffordable to address later on. It makes economic and environmental sense for the City to undertake reasonable adaptive steps now to enable an acceptable standard of operations and services to continue, sustainably, into the future.

The City of Kwinana has a total replacement value of around \$160M in building assets, \$57M in stormwater drainage assets, \$380M in roads and transport assets, \$5.7M in public lighting, \$40M in depreciable and \$44M in non-depreciable assets in its parks and reserves. Climate change related increases in temperature and the frequency of extreme storm events over time will likely affect the rate of ageing of these infrastructure items and facilities. Asset management planning and budgeting will need to consider the potential for renewal work timeframes to become progressively shorter over time.

As an action within this Plan, an Energy Sustainability Plan will be developed to guide the timing and budget requirements for relevant energy-related works. Proposed works will be considered in terms of priority, financial implications, projected energy and cost savings and the subsequent payback period for return on investment. Necessary capital and operating expenditure that has not already been budgeted for will be identified and proposed for funding through the Long Term Financial Plan process.

There is also an action within this Plan to develop a more formal framework for the existing Revolving Energy Fund. The intent will be to establish a clearer process for the calculation of energy costs saved with the implementation of energy efficiency and renewable energy works. Clearer criteria will also be developed for the prioritisation of future energy efficiency and renewable energy projects to be funded with the savings resulting from the Revolving Energy Fund program.

## 7. Monitoring and Review

There are a variety of actions specified within the Implementation Plan, each assigned to specific positions or teams. The progress of each action will be reviewed annually against its relevant measure of success and considered in the context of the priorities of the organisation and community at the time. In addition, during the mid-life Climate Change Plan review in 2023-2024, actions may be modified, added to or removed where appropriate in response to changing priorities, technologies, opportunities and challenges.

The City's greenhouse emissions data is calculated annually, and this data will be used to demonstrate how the City is tracking toward its new corporate emissions reduction target. This target will be reviewed at the time of the mid-life Plan review to ensure it remains realistic and achievable within the life of the Plan.

Once the Revolving Energy Fund framework is developed, this will be used to calculate any financial savings through reductions in energy consumption at locations where energy

efficiency or renewable energy generation works have been implemented. These calculated savings will be allocated to fund additional energy efficiency or generation works.

The City of Kwinana will continue to monitor for any changes in government regulations or standards that apply to energy use, efficiency or generation that may affect our adaptation actions or corporate operations, and respond accordingly.

## 8. References

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The Coordinator Strategic Planning entered the Council Chambers at 5:34pm

## 17 Reports – Built Infrastructure

### 17.1 Adoption of the draft Local Planning Strategy (2021 – 2036)

#### **DECLARATION OF INTEREST:**

There were no declarations of interest declared.

#### **SUMMARY:**

Council is being asked to adopt the draft Local Planning Strategy (2021 – 2036) (draft LPS) so that it can be submitted to the Western Australian Planning Commission (WAPC) for certification to formally advertise the draft Local Planning Strategy (Attachment 1 – draft LPS Parts 1 and 2).

The draft LPS forms the precursor and basis for the new Local Planning Scheme No. 4 which will supersede the current Town Planning Schemes No. 2 and 3.

The draft LPS was developed following the preparation of numerous technical reports, extensive community consultation, and numerous Council briefings. The draft LPS has been developed alongside the review of the Strategic Community Plan (SCP) and follows the sustainable development outcomes envisioned through that document. The draft LPS is aligned with the SCP by ensuring the four themes of the SCP, economy, community, environment and infrastructure are replicated throughout the draft LPS and form the structure of the draft LPS. It is noted that the SCP is currently undergoing a major review. It is anticipated that the draft LPS will be updated to reflect the adoption of a revised SCP prior to any community consultation exercise being undertaken.

The draft LPS is aimed at delivering sustainable outcomes for the community and is underpinned by 6 key planning principles. The implementation of draft LPS is through strategic directions and actions that will influence the formation of the City's local planning framework, primarily the preparation of a new Local Planning Scheme and the revision of existing or creation of new Policies, that will ultimately shape future development in the City of Kwinana.

It is recommended that Council endorse the draft LPS for referral to the WAPC to request certification to formally advertise the draft LPS.

#### **OFFICER RECOMMENDATION:**

That Council:

1. Adopt the draft Local Planning Strategy (2021 – 2036) as detailed in Attachment A; and
2. Pursuant to clause 12 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, request the Western Australian Planning Commission certify the draft Local Planning Strategy (2021 – 2036).

## 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

### **DISCUSSION:**

#### **Background**

The City has sought to progress and complete a local planning strategy on two previous occasions; in the early 2000's and then in 2014/15. On both occasions, for reasons of either uncertainties in the direction of strategic planning or constraints to resourcing, the local planning strategies were never finalised. Two sub-strategies, the Local Housing Strategy (2007) and the Local Commercial and Activity Centres Strategy (2014) were adopted by Council to guide land use planning and have been valuable documents for the City.

This historic work, important to the City over the years, is now superseded or will be reviewed by the Local Planning Strategy (once finalised) which sets a fresh direction for the City in line with current Strategic Community Plan and contemporary 2021 Kwinana aspirations.

In order to meet the requirements of the *Planning and Development (Local Planning Schemes) Regulations 2015* (Regulations), on the 11 April 2018, Council resolved to advise the WAPC (which subsequently agreed in August 2018) that the preparation of a new local planning scheme for the City of Kwinana be undertaken, and that upon the gazettal of the new planning scheme, existing Local Planning Schemes No's 2 and 3 be repealed. The preparation of the new scheme requires the preparation of a new Local Planning Strategy and it is this decision which formally resulted in the commencement of the current Local Planning Strategy process.

The Draft Local Planning Strategy (2021 – 2036) (draft LPS), therefore, provides the strategic basis for the development of a new Local Planning Scheme for the City and provides the foundation from which the City can shape future growth and development.

#### **Purpose of a Strategy**

A local planning strategy is a high-level, long-term land use plan to guide the growth and change of the City for the next 15 years.

Accordingly, draft LPS will inform the City's new Local Planning Scheme and will guide the development of local planning policies, precinct structure plans, standard structure plans and other planning tools to create a logical and interconnected local planning framework that will shape planning decisions and implement the City's vision for land use planning in Kwinana.

#### **Format of a Strategy**

The draft LPS consists of two parts, which are attached to this report:

- Part One contains a brief overview of the issues facing the City along with specific Strategic Directions and Strategic Actions to address each issue. An implementation table is contained at the rear that consolidates all Strategic Directions and Actions and lists an anticipated time frame for realisation; and
- Part Two provides the planning context, background information and analysis that leads to the identification of the issues and discusses the preferred option for dealing with the issues.

### 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

Both parts follow the same four key themes, being Community, Economy, Environment and Infrastructure which is consistent with the City's Strategic Community Plan and the State Planning Framework.

The draft LPS has also been aligned with the WAPC's latest guidelines on the form and manner of local planning strategies to the best extent possible, whilst still representing the unique local characteristics and planning challenges that face the City of Kwinana.

#### **Process for adopting a new local planning strategy**

The Regulations define the steps to be followed in preparing and finalising a local planning strategy. These are summarised as follows:

1. new local planning strategy required, as local government does not have an existing local planning strategy, in response to a "report of review" to prepare a new local planning scheme;
2. local government to prepare draft local planning strategy;
3. local government to adopt draft local planning strategy and request the WAPC certify the draft local planning strategy for the purpose of proceeding to advertise the draft local planning strategy;
4. WAPC may require the local government to modify the draft local planning strategy;
5. WAPC certifies that the draft local planning strategy is suitable for advertising;
6. local government to advertise the draft local planning strategy;
7. local government to consider submissions and may support the draft local planning strategy with or without modifications;
8. local government to make recommendation for final approval to WAPC; and
9. WAPC to endorse the Strategy.

The City is now at step three, whereby Council is being asked to adopt the draft LPS, following which it will be sent to the WAPC for consent to advertise. The WAPC may require modifications prior to granting consent.

It is important to note that whilst the Regulations 2015 do not require preliminary stakeholder consultation, the City undertook an extensive consultation process in April and May 2019 as part of the preparation of the draft LPS.

#### **Development of draft Local Planning Strategy (2021 – 2036)**

Following the report of review recommending a new local planning scheme be developed, and WAPC agreement of that recommendation in August 2018, City Officers briefed Councillors at an Elected Members Forum in October 2018 about the staged process for the creation of a local planning strategy. A conceptual timeframe was presented to Councillors, broken in to three stages:

- Stage one included the preparation of five land use studies and associated discussion papers, which formed the conceptual basis for preliminary community engagement.
- Stage two involved the preparation of the draft local planning strategy, which was to include informal advertising of the draft local planning strategy, and adoption by Council.
- Stage three was to be the formal advertising period, as set out by the Regulations 2015, following certification from the WAPC and ultimately final adoption by Council and the WAPC.

### 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

In accordance with stage one, the City prepared five land-use planning studies to inform the discussion around where we are now and raise questions around key issues and opportunities for the future.

1. **A Kwinana Housing Study**  
The focus of this study was recognising the land-use impacts of the City's changing demographics and population growth. This study identified changing residential and lifestyle requirements in the community. City Officers addressed this study by starting conversations with the community around the following housing related topic issues: density and zoning, diversity in housing typology and design of the built form.
2. **An Integrated Landuse and Transport Study**  
This examined the synergy between land-use and the City's connectivity, focussing on integrated transport accessibility (all modes), vehicle traffic, community travel routes and destinations and parking requirements of the community now and in the future.
3. **An Employment and Economic Development Analysis**  
This study focused on analysing the local economy and identified the relationship between population and industry, employment deficits and opportunities for economic growth and additional employment.
4. **A Local Biodiversity Study**  
This study examined the extent of the City's natural bushland and wetland biodiversity and recommends measure to ensure its protection.
5. **A Rural Lands Study**  
The purpose of this study was to examine the stock of rural lands and identify competing contemporary land use and environmental challenges.

Following the completion of the studies, discussion papers for each topic were distributed and made available to the community and stakeholders. The discussion papers provided a concise description of the topic and broke them down into key themes, issues and opportunities. These themes were then the focus of an extensive community engagement exercise that was undertaken, which included community workshops, survey questionnaires and focus group sessions, from which a Community Outcomes Report was generated.

The Community Outcomes Report was reported to Council on 25 September 2019 and has been essential in informing how the draft LPS responds to issues.

In accordance with stage two of the local planning strategy development process, a number of Councillor briefings were held, from November 2019 to February 2021, which discussed the following:

- Review community outcomes and prioritise issues and solutions;
- Determine an overall concept/land use vision to guide the local planning strategy;
- Determine planning principles which support the concept and focuses the aim of the local planning strategy;
- Examine the draft LPS map/s and the Strategic Directions and Actions for each theme.
- Discuss feedback relating to any changes required to the document prior to finalising the draft for Council consideration.



### 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

Through these Councillor briefings the draft LPS was developed and refined to include the communities' values expressed in the Community Outcomes Report. They also included the development of Council's priorities and directions that determined a sustainable concept for a future Kwinana and the planning principles that focus the draft LPS and continue to influence the City's local planning framework.

The planning principles determined by Council are:

#### Liveability

Ensure communities are safe, enable an active healthy lifestyle and offer a high quality of life and amenity.

#### Character

Promote and celebrate natural and cultural assets and landscape elements that are synonymous with the character and identity of Kwinana.

#### Prosperity

Maintain a strong and sustainable local, regional and international economy with a range of business and employment opportunities.

#### Accessibility and Connectivity

Focus on safe efficient and effective movement networks that improve legibility and permeability, link places and spaces through an enhanced pedestrian and cycle network at the local level, and connect regional complexes.

#### Sustainable Development

Incorporate sustainable development principles into everyday practices and so that there is a balance between development and conservation to ensure the needs of future generations can be met.

#### Resilience

Ensure that communities have the capacity to adapt and respond flexibly to future challenges and opportunities.

These principles have a strong emphasis on sustainability with the focus on local amenity and connected communities, the natural environment and a robust, flourishing economy.

The development of the draft LPS coincided with the timing of the development of the City's Strategic Community Plan and Climate Change Plan. This enabled collaboration in the development of all three key documents and ensured integration and consistency of several fundamental aspects.

It is noted that the Vision and Objectives derived from the Strategic Community Plan are likely to be updated with the adoption of a revised Strategic Community Plan. It is anticipated that the draft LPS would also be updated, prior to any community consultation exercise being undertaken.

### 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

The format and content of the draft LPS has also been aligned with a number of planning reform changes that the Department of Planning, Lands and Heritage (DPLH) have been developing over the last 12 months. The changes, whilst still to be implemented by the DPLH, aim to guide the development, content, manner and form of local planning strategies and will become a new template for all local governments in Western Australia to follow. The preparation of the draft LPS has regard to this new template, whilst still delivering the unique requirements for Kwinana, which should assist in avoiding significant changes being required to be made to the draft LPS by the DPLH prior to consent being granted for advertising.

#### **Content and focus of the draft Local Planning Strategy (2021 – 2036)**

Kwinana's population is anticipated to grow by approximately 45,000 additional people and 15,000 new dwellings over the next 15 years. By 2036, the population of Kwinana is expected to grow to approximately 85,000 people. Not only will the population grow during this time but our population will change to include a growing number of family households, multi-generational households and single person households.

The overall driving vision of the draft LPS is towards a sustainable future, where local amenity and connectivity are key aims and green urban settings can integrate with the natural environment. Housing choice and a robust, strong local economy is a key focus of the strategy.

A key direction of the draft LPS is to enhance and develop Kwinana's local and neighbourhood activity centres as 'urban villages' within the suburban landscape, with a focus on walking and cycling to the centres for daily shopping needs and as meeting places and be the location of more intensive and diverse housing (providing for a range housing need).

Another key focus of the draft LPS is to ensure that the natural amenity and character of the urban landscape is retained and enhanced. This is particularly important for established areas such as Medina and Calista, but would also apply to new growth areas in Kwinana. The City will seek to retain urban landscape, protect street trees and promote unique and sustainable built form character.

Providing open space and facilities to accommodate programs and services to our growing and changing community in an equitable and financially sustainable manner is also a key focus.

Kwinana also has large areas of land dedicated to supporting a semi-rural lifestyle. This type of land use facilitates natural bushland retention and groundwater protection which aligns with the City's approach to a sustainable future. The draft LPS is supportive of maintaining this semi-rural lifestyle, with a renewed focus on improving environmental outcomes in these areas.

The most significant economic driver for Kwinana, is its current and potential future industrial estates. These include the heavy industrial land in the Kwinana Industrial Area, the adjacent light and general industrial land in Latitude 32 and spin off industries associated with the future Kwinana Outer Harbour. A key direction of the draft LPS is to ensure that industrial areas are adequately separated from sensitive land uses elsewhere in Kwinana. This will enable industry to grow as it responds to the market but will also protect residential areas from potential adverse impacts of industry.

### 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

The draft LPS identifies the development of service commercial precincts, which take advantage of Kwinana Freeway interchange locations, at Thomas and Anketell Roads. A key action is to investigate the feasibility of a large mixed use – office based precinct at the southern side of Thomas Road / Kwinana Freeway interchange and its integration to the Kwinana Train Station for a range of office, commercial and residential uses.

The growth of the Kwinana City Centre is a priority as the primary activity centre in Kwinana. The draft LPS focuses on enhancing its position as the pre-eminent place for retail shopping, commercial, educational, civic and recreational activities as well as entertainment, within the City. Other activity centres include the future Wandi District Centre as well as smaller neighbourhood and local centres, all of which are important as they will provide focal points for communities with a range of district and local shopping needs.

A key element of the draft LPS in terms of sustainable development, liveability, character and resilience is the environment. This ranges from tree canopy cover in suburban areas, to the protection of ecological linkages and adapting to climate change.

The draft LPS recommends preparing a Local Biodiversity Strategy to provide a framework for the protection and management of significant local natural areas in Kwinana, in addition to those areas already set aside for protection by the State Government (eg. Leda Reserve and The Spectacles).

The City has recognised its responsibility to act on climate change and has adopted a Climate Change Plan (2021). With an increasing population within Kwinana, climate change adaptation and improving community resilience are an important part of land use and infrastructure planning under the draft LPS.

In terms of a sustainable urban form and resilience, access and connectivity will be critical, particularly local movements connecting activity centres, transport nodes, parks and recreation facilities. The connections to local centres, neighbourhood centres and the Kwinana City Centre will be crucial to creating vibrant 'urban villages'. The intention is to create communities where cycling and walking are the preferred choices for short trips and everyone has the opportunity to choose a healthy lifestyle.

The movement of freight through Kwinana provides essential economic benefits to the overall Western Australian economy, thus protection of dedicated freight routes is a key consideration in terms of the overall efficiency and viability of the Western Trade Coast and Outer Harbour.

It is anticipated that there will be an increasing transition towards electric vehicles and automated vehicles within the next 15 years. Considerations for Kwinana include requirements for electric vehicle charging facilities in new developments and decreased requirements for public and private car parking spaces as the car share economy grows.

#### **Next steps**

The original project plan for stage two of the draft local planning strategy included an informal consultation process, however given the extent of the community engagement exercise undertaken in 2019 and the embedding of those outcomes into the draft LPS, it is considered that this informal advertising step is no longer required.

### 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

As part of the regulatory process for the consideration and adoption of the draft LPS, a formal advertising period is required and it is considered appropriate to focus on this as the main consultation exercise to engage the community.

Accordingly the next steps for the adoption of the draft LPS are:

1. Council adopts the draft LPS;
2. The draft LPS is sent to the WAPC for its consideration;
3. The WAPC may require modifications to the draft LPS;
4. If required, modifications are made and the draft LPS is resubmitted to the WAPC;
5. The WAPC then certifies the draft local planning strategy (for the purpose of proceeding to advertise);
6. The City then begins formal advertising to consult with the community on the draft LPS;
7. Council considers all submissions and may make modifications to the draft LPS before sending the draft LPS to the WAPC;
8. The WAPC assesses the draft LPS and may require modifications to the draft LPS;
9. If required, modifications are made and the draft LPS resubmitted to the WAPC;
10. The WAPC then endorses the local planning strategy.

Prior to formal advertising (step 6), City Officers will prepare a Community Engagement Plan outlining the stakeholders to be engaged and the methods of engagement.

Should the WAPC require significant modifications to the draft LPS, City Officers will also bring those to Council for discussion.

#### **LEGAL/POLICY IMPLICATIONS:**

For the purpose of Councillors considering a financial or impartiality interest only, the proponent/owner is the City of Kwinana.

Relevant legislation applicable to this item:

*Planning and Development Act 2005*

*Planning and Development (Local Planning Schemes) Regulations 2015*

#### **FINANCIAL/BUDGET IMPLICATIONS:**

There are no financial/budget implications as a result of this proposal as the draft LPS has been prepared largely in-house and the actions will be largely implemented through existing resourcing.

#### **ASSET MANAGEMENT IMPLICATIONS:**

There are no asset management implications as a result of this proposal.

## 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

**ENVIRONMENTAL IMPLICATIONS:**

There are no environmental implications as a result of this proposal, however a number of actions from the draft LPS seek to implement provisions into the City's future local planning framework that would aim to protect valued environmental features and increase the sustainability of the City.

**STRATEGIC/SOCIAL IMPLICATIONS:**

This proposal will support the achievement of the following outcomes and objectives detailed in the Strategic Community Plan and Corporate Business Plan.

Plan	Outcome	Objective
Strategic Community Plan	A unique identity	1.1 Develop and strengthen community identity to create a sense of belonging
Strategic Community Plan	A safe and welcoming place	1.3 Facilitate improved community safety and reduced crime levels
Strategic Community Plan	Services for an active community	1.4 A healthy and active community with services for everyone's needs
Strategic Community Plan	A sense of place and heritage	1.8 Respect and promote Kwinana's unique heritage
Strategic Community Plan	Accessibility for everyone	1.9 Improve levels of disability access and inclusion throughout the community
Strategic Community Plan	Varied job opportunities	2.1 Residents have access to ample local job opportunities.
Strategic Community Plan	Quality education for all ages	2.2 The community has a choice of quality public and private facilities to meet their education and training needs throughout their life time.
Strategic Community Plan	A bustling retail scene	2.3 The City Centre is home to a thriving range of specialty shops, restaurant and family entertainment venues and an active night-life while neighbourhood centres are revitalised.
Strategic Community Plan	A powerhouse industrial area	2.4 The Western Trade Coast is developed with maximum leverage being gained from investments in new infrastructure.
Strategic Community Plan	A thriving local economy	2.5 Stimulate economic development and encourage diversification.

## 17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)

Strategic Community Plan	Innovative approval system	2.6 Provide a best practice development approvals system that attracts and retains business investment in the area.
Strategic Community Plan	A beautiful environment	3.1 Improve conservation of biodiversity and protection of native vegetation whilst achieving high levels of environmental protection in new developments.
Strategic Community Plan	A City adapted to climate change	3.5 Understand the impacts of climate change and take a risk management approach to addressing these effects in future planning.
Strategic Community Plan	Great public places	4.1 Residents are provided with a range of multifunctional community places and accessible recreation facilities.
Strategic Community Plan	Well-kept green spaces	4.2 The community has easy access to well equipped, quality parks and public open spaces.
Strategic Community Plan	A well planned City	4.4 Create diverse places and spaces where people can enjoy a variety of lifestyles with high levels of amenity.
Strategic Community Plan	A well maintained City	4.5 Actively improve the appearance of public areas and streetscapes throughout Kwinana.
Strategic Community Plan	A connected transport network	4.6 Provide a safe and efficient integrated network of roads, footpaths and cycles routes supported by a good public transport system.
Corporate Business Plan	A well planned City	4.4.1 Develop and implement the Local Planning Strategy

**COMMUNITY ENGAGEMENT:**

There are no community engagement implications as a result of this report, however, as discussed, previous community engagement has occurred to inform the development of the draft LPS and following certification by the WAPC formal advertising will be required. A Community Engagement Plan will be prepared to support the formal advertising of the draft LPS.

**17.1 ADOPTION OF THE DRAFT LOCAL PLANNING STRATEGY (2021 – 2036)****PUBLIC HEALTH IMPLICATIONS:**

The draft LPS has the potential to help improve the following determinants of health –

- Built Environment – Environmental Quality; Neighbourhood Amenity;
- Socio-economic Factors – Education; Employment; Income.

**RISK IMPLICATIONS:**

The risk implications in relation to this proposal are as follows:

Risk Event	Draft LPS does not proceed through adoption, certification and endorsement.
Risk Theme	Failure to fulfil statutory regulations or compliance requirements
Risk Effect/Impact	People/Health Environment Property
Risk Assessment Context	Strategic
Consequence	Moderate
Likelihood	Unlikely
Rating (before treatment)	Moderate
Risk Treatment in place	Reduce - mitigate risk
Response to risk treatment required/in place	Reduce – draft LPS has been prepared with Councillor involvement, community engagement and in line with the latest DPLH guidelines for local planning strategies
Rating (after treatment)	Low

**COUNCIL DECISION**

385

**MOVED CR M ROWSE****SECONDED CR P FEASEY**

**That Council:**

1. **Adopt the draft Local Planning Strategy (2021 – 2036) as detailed in Attachment A; and**
2. **Pursuant to clause 12 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, request the Western Australian Planning Commission certify the draft Local Planning Strategy (2021 – 2036).**

**CARRIED  
6/0**

ATTACHMENT A -  
DRAFT LOCAL PLANNING STRATEGY -  
PART 1 - STRATEGY

Local Planning  
Strategy **2021-2036**



**DRAFT**



Version	Date	Description
v1.0	24 March 2021	Draft for Council adoption



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The City of Kwinana acknowledges the Traditional Custodians of the land in which we live, work and play, the Nyoongar people, and we pay our respect to Elders past and present.

# Message From The Mayor

I am pleased to present the City of Kwinana's Local Planning Strategy which builds upon Kwinana's rich planning history and provides the guiding vision for Kwinana's growth and development over the next 15 years.

The Local Planning Strategy is vital in providing a long-term strategic planning direction on how Kwinana will be continue to be a place that is rich in spirit, alive with opportunities and surrounded by nature. The Local Planning Strategy aims to strengthen these elements ensuring they are reflected through the planning of innovative and quality development in a sustainable manner.

Kwinana as we know it today has its origins in the 1950s when the State Government appointed one of Western Australia's first qualified town planners, Margaret Feilman, to prepare plans for a townsite to accommodate the workforce for the BP refinery and the many other industries being planned for this area. Feilman based her design on what was then a new style of planning, which involved designing communities so that local shops and community facilities were within a walkable distance and retained major bushland and landscape features.

This commitment to environmental conservation in the design of Kwinana is still evident today, almost 70 years after the area was first developed. Kwinana now projects a sense of place with undulating landscape, mature native trees and bushland. These principles of creating neighbourhoods within bushland and natural landscape have been entrenched in this Local Planning Strategy to ensure that Kwinana's natural heritage is preserved for future generations.

Within the timeframe of this strategy, the Kwinana Outer Harbour, supporting industries and transport freight corridors will be developed. The development of the Outer Harbour will trigger a new era of growth and development in Kwinana. An important function of this Strategy is to ensure the land requirements of the supporting industrial areas and transport corridors are planned in an orderly and proper manner and, just as importantly, that Kwinana prospers from this period of growth.

The Local Planning Strategy has been five years in the making, based on in-depth community and stakeholder consultation, paired with comprehensive research and analysis of current needs and projected future trends.

The Local Planning Strategy is a first step in the ongoing process of refining and detailing planning within Kwinana. It will be reviewed after five years and ongoing refinement will continue through the local planning scheme, strategies, policies, structure planning, subdivision and development.

The challenge now is to implement the Local Planning Strategy effectively. While the City will take the lead, effective implementation can only be achieved through a whole of-community commitment. This is critical, as plans are only as good as the will to implement their vision on behalf of communities, now and into the future.

**City of Kwinana Mayor,  
Carol Adams OAM**



# Executive Summary

The City of Kwinana's (the City) Local Planning Strategy sets out the land use planning and development directions and actions for a 15 year horizon (2036). This timeframe will be marked by continued high growth in Kwinana's residential population and considerable change and expansion to Kwinana's industrial areas.

The Local Planning Strategy (the Strategy) has been developed and informed by the following set of integrated planning principles which are aimed at guiding our community, our economy and our environment towards a sustainable future. This follows extensive community consultation undertaken by the City about what the community considers to be important.

**Liveability**

Ensure communities are safe, enable an active healthy lifestyle and offer a high quality of life and amenity.

**Character**

Promote and celebrate natural and cultural assets and landscape elements that are synonymous with the character and identity of Kwinana.

**Prosperity**

Maintain a strong and sustainable local, regional and international economy with a range of business and employment opportunities.

**Accessibility and Connectivity**

Focus on safe efficient and effective movement networks that improve legibility and permeability, link places and spaces through an enhanced pedestrian and cycle network at the local level, and connect regional complexes.

**Sustainable Development**

Incorporate sustainable development principles into everyday practices and so that there is a balance between development and conservation to ensure the needs of future generations can be met.

**Resilience**

Ensure that communities have the capacity to adapt and respond flexibly to future challenges and opportunities.

The focus on these sustainability principles, to inform and guide Kwinana's growth and development, is fundamental to the vision of the Local Planning Strategy. These principles apply across the four key elements addressed in the Strategy, being community, economy, environment and infrastructure.

## Community



Kwinana's population is anticipated to grow by approximately 45,000 additional people and 15,000 new dwellings over the next 15 years. By 2036, the population of Kwinana is expected to be about 85,000 people.

Not only will the population grow during this time but our population will change to include a growing number of family households, multi-generational households and single person households.

With a view to accommodating this growth and change, liveability is a defining principle in the Local Planning Strategy. A key direction of the Strategy is to enhance and develop Kwinana's local and neighbourhood activity centres as 'urban villages' within the suburban landscape, with a focus on walking and cycling to the centres for daily shopping needs and as meeting places.

Analysis of demographic data suggests that the community's housing needs will be more diverse as the population grows over the next 15 years, and offering a choice of housing types is a key direction of the Strategy. The intent is that neighbourhood centres and the Kwinana City Centre will be the location of more intensive and diverse housing. This will reinforce and support these centres with an increased population close to shopping areas and create opportunities for housing choice close to shops and transport. This would include housing for singles and couples as well as families and the elderly to allow residents to stay and live in Kwinana as they age.

Key actions of the Local Planning Strategy include precinct planning for neighbourhood centres to determine how best to achieve these outcomes and identify walkable catchments where housing diversity can be explored.

Another key action of the Local Planning Strategy is to ensure that the natural amenity and character of these centres and surrounding residential environment is retained and enhanced. This is particularly important for established heritage areas such as Medina and Calista, but should also apply to new growth areas in Kwinana. The

City will establish and apply policies to retain urban landscape, protect street trees and promote unique and sustainable built form character within new urban areas.

Providing open space and facilities to accommodate programs and services to our growing and changing community in an equitable and financially sustainable manner is a key focus of the Local Planning Strategy.

Kwinana has large areas, on the eastern side of the Kwinana Freeway, set aside for rural living (Special Rural and Rural Resource zoned land). This type of land use facilitates natural bushland retention and groundwater protection which aligns with the City's approach to a sustainable vision. The Local Planning Strategy is supportive of rural living continuing in the long term.



## Economy and Employment



The most significant economic driver for Kwinana, is its current and potential future industrial estates. These include the heavy industrial land in the Kwinana Industrial Area, the adjacent light and general industrial land in Latitude 32 and spin off industries associated with the future Kwinana Outer Harbour.

A key Direction of the Local Planning Strategy is to ensure that industrial areas are adequately separated from sensitive land uses elsewhere in Kwinana. This will enable industry to grow as it responds to the market but will also protect residential areas from potential adverse impacts of industry.

Kwinana's industrial areas form part of the broader Western Trade Coast. It is critical (particularly with the proposed development of the Outer Harbour) that comprehensive master planning take place for the entire Western Trade Coast. The last comprehensive study of the Western Trade Coast was the *Fremantle Rockingham Industrial Area Regional Strategy* (WAPC, 1999). The existing core of heavy industry areas in the Kwinana Industrial Area is rapidly filling and an updated master plan should be undertaken to optimise the use of the Kwinana Industrial Area to resolve strategic land use issues such as the impact of the proposed Outer Harbour, the associated intermodal facility and the supply of high impact - heavy industrial sites. Also requiring clear foresight is the most appropriate location of storage, freight, logistics and office uses which need not be located on core heavy industry sites but further afield, close to 24/7 freight links with easy access to customers and suppliers. This in turn highlights the work required to inform light industrial land uses that will support core industrial land uses and provide future employment opportunities.

The growth of the Kwinana City Centre is a priority as the primary activity centre in Kwinana. The Local Planning Strategy focuses on enhancing its position as the pre-eminent place for retail shopping, commercial, educational, civic and recreational activities as well as entertainment, within the City.

Other activity centres include the future Wandri District Centre as well as smaller neighbourhood and local centres, all of which are important. The City supports the planning and development of service commercial precincts which take advantage of Kwinana Freeway interchange locations at Thomas and Anketell Roads. A key action is to investigate the feasibility of a large mixed use - office based precinct at the southern side of Thomas Road / Kwinana Freeway interchange and its integration to the Kwinana Train Station for a range of office, commercial and residential uses.

While Kwinana's industrial areas produce a significantly higher proportion of jobs in the manufacturing sector than other areas of Perth, most of these jobs are filled by people living outside the City of Kwinana. It is evident that employment self-sufficiency is less than that of the surrounding south west region and amongst the lowest in the Perth metropolitan area. Unemployment rates in Kwinana are one of the highest in the Perth metropolitan area and higher than the surrounding south west region with youth unemployment high. Employment growth has levelled out while population growth continues to increase so new and diverse employment opportunities are needed.

An important objective of the Local Planning Strategy is to identify land uses which will improve local employment opportunities. While the growth of the Kwinana Industrial Area and proposed Outer Harbour will provide jobs for skilled workers, research shows that industries such as retail, accommodation, health and food services provide unskilled and youth employment. The development of service commercial precincts at the Kwinana Freeway interchanges and mixed use activity within the Kwinana City Centre and neighbourhood centres would promote greater employment diversity.

The growth of home business and advocacy for training and education facilities are also opportunities for employment and will be pursued.

## Environment



A key element of the Local Planning Strategy in terms of sustainable development, liveability, character and resilience is the environment. This ranges from tree canopy cover in suburban areas, to the protection of ecological linkages and adapting to climate change.

Key directions of the Local Planning Strategy are to:

- enhance tree canopy cover to cool residential streets and open spaces during extreme heat, provide shade to encourage walking and cycling, create leafy neighbourhoods, and enhance local biodiversity; and
- identify ecological linkages which link locally and regionally significant natural areas and provide stepping stones for flora and fauna. These linkages would support the ongoing management of regional sites and provide opportunities for integrated walking trails with interpretive signage.

The Local Planning Strategy recommends preparing a *Local Biodiversity Strategy* to provide a framework for the protection and management of significant local natural areas in Kwinana, in addition to those areas already set aside for protection by the State Government (eg. Leda Reserve and The Spectacles).

The large areas of 'Special Rural' and 'Rural Resource' zoned land also represents an opportunity to retain biodiversity and maintain the rural living lifestyle. It is important that clearing and degradation of bushland is limited in these areas to maintain biodiversity objectives. There needs to be a balance between land clearing and ensuring protection of people and property through bush fire management, as much of these areas have a high fire risk.

Western Australia, particularly the south-west, is vulnerable to the impacts of climate change.

The City has recognised its responsibility to act on climate change and has adopted a *Climate Change Plan (2021)*. With an increasing population within Kwinana, climate change adaptation and improving community resilience are an important part of land use and infrastructure planning under the Local Planning Strategy.



## Infrastructure



Typical of Perth's outer suburbs, residential and commercial development within Kwinana is low density and spread out and Kwinana's residents are largely dependent on private vehicles to travel to schools, shops, services, employment and recreation.

In terms of a sustainable urban form and resilience, access and connectivity will be critical, particularly local movements connecting activity centres, transport nodes, parks and recreation facilities. The connections to local centres, neighbourhood centres and the Kwinana City Centre will be crucial to creating vibrant 'urban villages'.

The City adopted the *Bike and Walk Plan (2018)* to provide a safe network of cycle routes and walking paths that are convenient for people of all ages and abilities to ride or walk to local destinations such as schools, shops, parks, bushland, bus stops and train stations. The intention is to create communities where cycling and walking are the preferred choices for short trips and everyone has the opportunity to choose a healthy lifestyle. The Local Planning Strategy will promote the continued roll out of the *Bike and Walk Plan*.

An integrated transport network to distribute traffic within Kwinana and to provide linkages to other parts of the metropolitan region is crucial in facilitating access for the residents of Kwinana. Encouraging public transport and the provision of a high quality integrated train and bus system is important to the

community and was a particularly common theme during the City's community consultation process. The Local Planning Strategy seeks to reinforce public transport usage with an action to advocate for the best bus services possible with links to the train stations.

The movement of freight through Kwinana provides essential economic benefits to the overall Western Australian economy and is a key consideration in terms of the overall efficiency and viability of the Western Trade Coast and Outer Harbour. The efficiency of freight movement is reliant on the provision of designated road and rail corridors that are separated from local traffic and are not restricted due to their potential impact on the amenity of incompatible land uses. The City will manage land use conflict in the vicinity of freight corridors by designating adjoining land uses appropriately and by ensuring sensitive land uses are adequately buffered from vehicle noise and vibration impacts.

It is anticipated that there will be an increasing transition towards electric vehicles and automated vehicles within the next 15 years. Considerations for Kwinana include requirements for electric vehicle charging facilities in new developments and decreased requirements for public and private car parking spaces as the car share economy grows.



# 1. INTRODUCTION

## 1.1 Purpose of the Local Planning Strategy

The purpose of the Local Planning Strategy is to:

- identify land use planning issues and opportunities;
- outline the strategic land use development directions and actions within Kwinana until 2036, having regard to the City's *Strategic Community Plan*;
- link strategic planning in Kwinana with State and regional planning, including current strategies, structure plans and strategic development initiatives;
- coordinate the existing plans and strategies adopted and developed by the City;
- set a strategic framework for the establishment of a new local planning scheme along with rationale for the zoning and reservation of land and for the provisions of the scheme relating to development and development control;
- provide a strategic framework for assessment and decision-making in relation to proposed scheme amendments, subdivision and development;
- identify the need for further studies or investigation to address longer-term strategic planning and development issues within Kwinana; and
- outline how the Strategy will be implemented including reference to the development of any local planning mechanisms, including policies and guidelines which may be required.



## 1.2 How the Local Planning Strategy has been prepared

The Local Planning Strategy has been developed in accordance with the statutory requirements of the *Planning and Development Act 2005*, the *Planning and Development (Local Planning Schemes) Regulations 2015* and with regard to the Local Planning Manual.

The Local Planning Strategy was prepared in the following three stages:

### Stage One – Understanding where we are now

In order for the City to develop a comprehensive understanding of the important elements that affect Kwinana, it was necessary to undertake detailed studies on the following topics:

#### Housing Study

The focus of this study was to recognise the land-use impacts of Kwinana's growing and changing demographics and population growth. This study identified changing residential and lifestyle requirements in the community.

#### Integrated Land use and Transport Study

This examined the transport needs for the community and also includes an analysis of the City's land use changes and how that may affect connectivity, accessibility, traffic and parking requirements now and in the future.

#### Employment and Economic Development Analysis

This study focused on analysing the local economy and identified the relationship between population and industry, employment deficits and opportunities for economic growth and additional employment.

#### Local Biodiversity Study

This study examined the extent and protection of Kwinana's natural bushland and wetland biodiversity.

#### Rural Lands Study

This study examined the stock of rural lands and identified competing contemporary land-use and environmental challenges.

Furthermore, the City was able to draw upon a range of existing studies and adopted strategies to inform the existing context and future implications. These include:

- *Westport Future Recommendations – Stage 2 Report (2020)*
- *Climate Change Mitigation and Adaptation Plan (2015-2020)*
- *Strategic Community Plan (2019 – 2029)*
- *Parks Upgrade Strategy (2019/2029)*
- *Public Health Plan (2019 – 2023)*
- *Kwinana Parks Upgrade Strategy (2019 – 2029)*
- *Disability Access and Inclusion Plan (2019 – 2021)*
- *Kwinana City Centre Master Plan and Design Guidelines (2019)*
- *Bike and Walk Plan (2018)*
- *Sustainable Water Management Plan (2018)*
- *Multicultural Action Plan 2017 – 2020*
- *Hope Valley Wattleup Redevelopment Project Master Plan (2005)*
- *Municipal Heritage Inventory (2015)*



- *Access and Equity Policy (2015)*
- *Local Commercial and Activity Centres Strategy (2014)*
- *Local Biodiversity Strategy (Draft 2013)*
- *Community Infrastructure Plan (2011 – 2031)*
- *Eastern Residential Intensification Concept (2005)*
- *Postans Precinct Study (2011)*
- *Medina Neighbourhood Centre Revitalisation Strategy (2010)*
- *Medina Residential Design Guidelines (2009)*
- *Eastern Residential Intensification Concept (2005)*

Those planning strategies, background studies and analysis of planning matters contributed to preparation of the Local Planning Strategy, and are outlined in Part 2 through:

- A synopsis of the relevant State and regional planning context explaining the planning framework to which the Local Planning Strategy, and other local planning documents, must respond.
- An analysis of relevant local government documents that inform the City's position and local profiles.
- A description of the major physical, social, economic, environmental and infrastructure elements.
- Outlines of the key issues and description of the most effective strategies to be considered.

## Stage Two – Identify Issues and Opportunities

The City undertook extensive community consultation in the development of the Local Planning Strategy and the community's feedback formed the basis of Local Planning Strategy. Engagement with the community and stakeholder groups included:

1. Making available draft planning studies and related discussion papers.
2. A survey focused on the key topics raised in the studies and discussion papers as well as raising some broader questions about the community's planning vision for Kwinana.
3. Facilitation of:
  - Four workshops;
  - Two information sessions for specific Stakeholder Groups;
  - Focus sessions with an Aboriginal Community Group and a Youth Group; and
  - A *Getting to Know Your Community Event* held at the Market Place.

The Focus Group Sessions sought responses from the community and stakeholder groups and were structured around the following five broad strategic planning issues:

- Responding to population growth and the changing residential and lifestyle needs of the community by addressing housing issues such as density, diversity, built form and residential character;
- Supporting local employment and the economy by considering the land use requirements for economic growth and additional employment and identifying opportunities;
- The synergy between transport, traffic, travel, and parking requirements of the community now and in the future;
- The protection of Kwinana's environment (including biodiversity and heritage); and
- Effective use of rural lands in the context of competing, contemporary environmental issues and challenges.

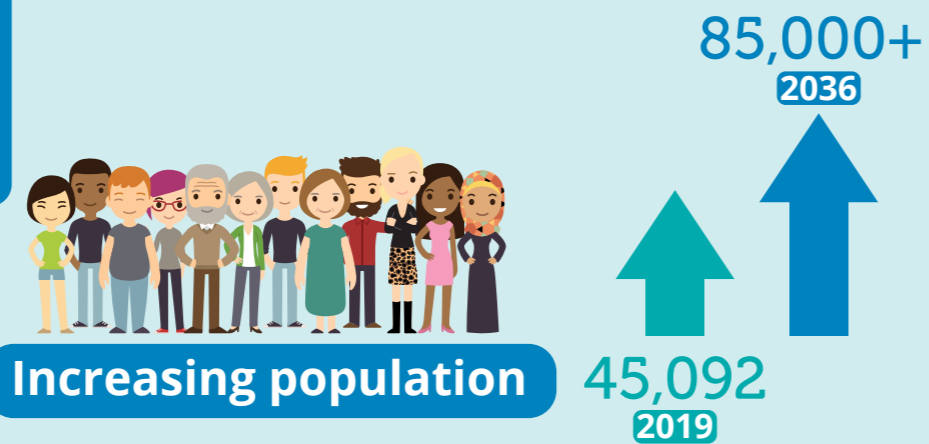
The community's feedback has informed and shaped the vision for the future of Kwinana.

## Stage Three – Directions and Actions for the Future

Following consultation with the community and key stakeholders, a series of workshops were undertaken with the Elected Members to establish planning principles and the directions and actions of the Strategy.

The planning principles guide and underpin all future land use planning and decision making and the actions set out specific ways the City will achieve the directions.

# Community



# Economy

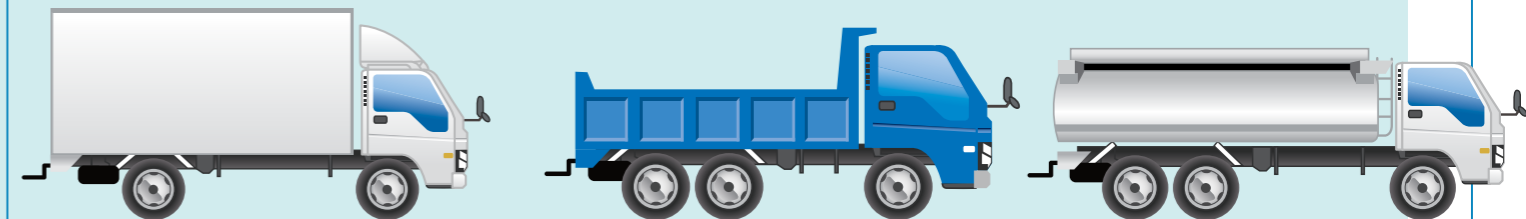


**New Port opportunities**

**Increased job diversity**



# Infrastructure



**Secure regional freight routes**

# Environment

**Strengthen ecological linkages**

**Improved tree canopy cover**



### 3. THE CITY'S VISION AND PRINCIPLES

#### 3.1 Our Sustainable City 2021 - 2036


The City of Kwinana's vision as stated in the *Strategic Community Plan* is "**Rich in spirit, alive with opportunities, surrounded by nature – it is all here**"<sup>1</sup> and represents the community's aspirations for the future and captures the essence of what it will be like to live in Kwinana in the future. This Local Planning Strategy sets directions and actions focused on realising this vision.

As shown in Table 1 the Local Planning Strategy has been prepared using the key themes of

- Community;
- Economy;
- Environment; and
- Infrastructure.

It is through implementation of the Local Planning Strategy, that a sustainable future for Kwinana will be achieved.

The Local Planning Strategy will be continuously improved to align future land use planning and development with the community's aspirations and long term vision.

Themes derived from the Strategic Community Plan	Objectives derived from the Strategic Community Plan
 <p><b>Community</b> "Rich in spirit"</p> <ul style="list-style-type: none"> <li>• Housing and Population Diversity</li> <li>• Character Identity and Urban Design</li> <li>• Public Open Space and Community Facilities</li> </ul>	<ul style="list-style-type: none"> <li>Develop and strengthen community identity to create a sense of belonging.</li> <li>Facilitate improved community safety and reduced crime levels</li> <li>A healthy and active community with services for everyone's needs.</li> <li>Respect and promote Kwinana's unique heritage.</li> <li>Improve levels of disability access and inclusion throughout the community.</li> <li>Actively improve the appearance of public areas and streetscapes throughout Kwinana.</li> <li>Create diverse places and spaces where people can enjoy a variety of lifestyles with high levels of amenity.</li> </ul>

<sup>1</sup> The City of Kwinana is currently reviewing the Strategic Community Plan and the draft Local Planning Strategy will be modified to include the revised vision once the Strategic Community Plan has been advertised and adopted by Council.

Themes derived from the Strategic Community Plan	Objectives derived from the Strategic Community Plan
 <p><b>Economy</b> "Alive with opportunities"</p> <ul style="list-style-type: none"> <li>• Industry and the Outer Harbour</li> <li>• Kwinana City Centre and other Activity Centres</li> <li>• Employment</li> <li>• Education and Training</li> <li>• Agriculture</li> </ul>	<ul style="list-style-type: none"> <li>Residents have access to ample local job opportunities.</li> <li>The community has a choice of quality public and private facilities to meet their education and training needs throughout their life time.</li> <li>The City Centre is home to a thriving range of specialty shops, restaurant and family entertainment venues and an active night-life while neighbourhood centres are revitalised.</li> <li>The Western Trade Coast is developed with maximum leverage being gained from investments in new infrastructure.</li> <li>Stimulate economic development and encourage diversification.</li> <li>Provide a best practice development approvals system that attracts and retains business investment in the area.</li> <li>Create diverse places and spaces where people can enjoy a variety of lifestyles with high levels of amenity.</li> <li>Ensure the Kwinana community is well serviced by government and non-government services.</li> </ul>
 <p><b>Environment</b> "Surrounded by nature"</p> <ul style="list-style-type: none"> <li>• Protection of Local Natural Areas, Landscapes and Ecological Linkages</li> <li>• Climate Change Mitigation and Adaptation</li> </ul>	<ul style="list-style-type: none"> <li>Improve conservation of biodiversity and protection of native vegetation whilst achieving high levels of environmental protection in new developments.</li> <li>Understand the impacts of climate change and take a risk management approach to addressing these effects in future planning.</li> </ul>
 <p><b>Infrastructure</b> "It's all here"</p> <ul style="list-style-type: none"> <li>• Transport</li> <li>• Cycling and Walking Network</li> <li>• Utilities</li> </ul>	<ul style="list-style-type: none"> <li>Residents are provided with a range of multifunctional community places and accessible recreation facilities.</li> <li>The community has easy access to well equipped, quality parks and public open spaces.</li> <li>Create diverse places and spaces where people can enjoy a variety of lifestyles with high levels of amenity.</li> <li>Actively improve the appearance of public areas and streetscapes throughout Kwinana.</li> <li>Provide a safe and efficient integrated network of roads, footpaths and cycles routes supported by a good public transport system.</li> </ul>

Figure 1: Summary of Themes and Objectives



### 3.2 Local Planning Strategy Principles – An Integrated Approach towards Sustainability

In addition to the objectives included in Table 1, the Local Planning Strategy has been developed and informed by the following set of integrated planning principles which are based on social, economic and environmental objectives aimed at guiding our community, economy and environment towards a sustainable future.

An appropriate sustainable planning framework in place that is based on these principles (including planning strategies, plans and policies enforced through a local planning scheme) will guide Kwinana towards a more sustainable future.

The sustainability principles upon which the Local Planning Strategy is based are:

**Liveability**

Ensure communities are safe, enable an active healthy lifestyle and offer a high quality of life and amenity.

**Character**

Promote and celebrate natural and cultural assets and landscape elements that are synonymous with the character and identity of Kwinana.

**Prosperity**

Maintain a strong and sustainable local, regional and international economy with a range of business and employment opportunities.

**Accessibility and Connectivity**

Focus on safe efficient and effective movement networks that improve legibility and permeability, link places and spaces through an enhanced pedestrian and cycle network at the local level, and connect regional complexes.

**Sustainable Development**

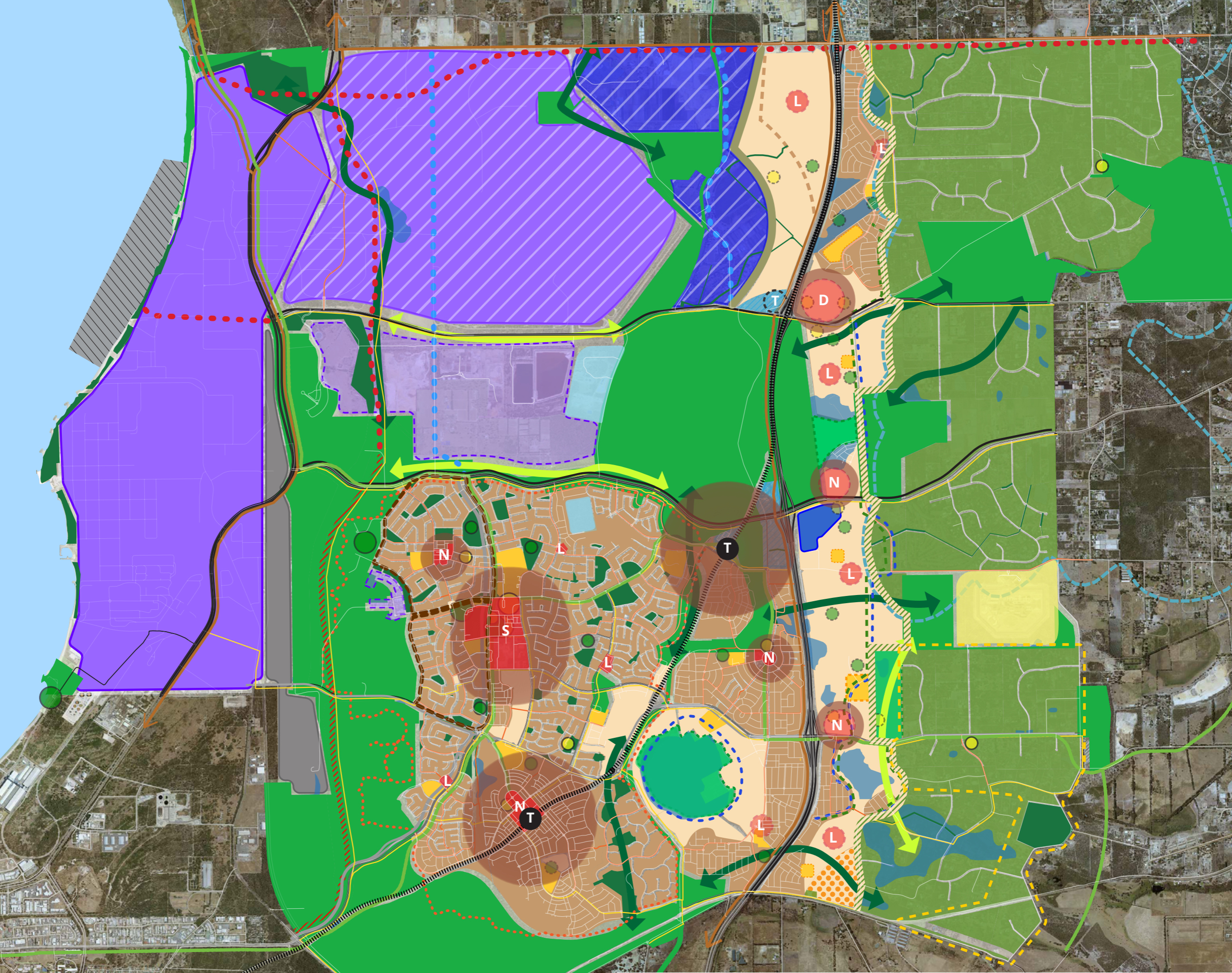
Incorporate sustainable development principles into everyday practices and so that there is a balance between development and conservation to ensure the needs of future generations can be met.

**Resilience**

Ensure that communities have the capacity to adapt and respond flexibly to future challenges and opportunities.



- KEY**
- Existing Urban
  - Future Urban
  - Rural Living
  - Regional Parks
  - Local Parks
  - Park & Reserve Investigation
  - Industry
  - Industry Expansion
  - Industry Investigation
  - Improvement Plan 47
  - Service Commercial
  - Station Precinct Investigation Area
  - Existing Activity Centres
  - Proposed Activity Centres
  - Secondary
  - District
  - Neighbourhood
  - Local
  - Walkable Catchment Area
  - Schools
  - Proposed schools
  - Recreational Facility
  - Community Facility
  - Proposed Recreational Facility
  - Proposed Community Facilities
  - Character Investigation Area
  - Proposed Primary Roads
  - Fremantle-Rockingham Primary Regional Road Investigation
  - Proposed Secondary Roads
  - Kwinana Loop Trail
  - Primary Cycle Routes
  - Secondary Cycle Routes
  - Local Cycle Routes
  - Proposed Cycle Routes
  - Regional Bus Route
  - Future Bus Routes
  - Ecological Corridors
  - Proposed Local Links
  - Water Treatment
  - Casuarina Prison
  - Wetlands Conservation Area
  - Public Drinking Water Source
  - Holding yard buffer
  - Westport
  - Rail Yards
  - Rural to Urban Interface
  - Sub-regional framework investigation area
  - Train Station
  - Proposed Train Station
  - Primary Roads
  - Secondary Roads
  - Commuter Rail



4. LOCAL PLANNING STRATEGY MAP

# COMMUNITY



## 5. COMMUNITY

### 5.1 Housing and Population Diversity

An understanding of the characteristics of Kwinana's future population is essential to planning for the housing needs of future residents.

Kwinana has undergone rapid population growth in recent years and is forecast to grow by approximately 45,000 people (to a total population of 85,000) by 2036 (forecast .id), requiring approximately 15,000 additional dwellings. The majority of these dwellings will be located in the future suburbs of Casuarina, Anketell, Wellard and Mandogalup, and based on current trends these areas will be occupied predominantly by couples and families.

The majority of dwellings across Kwinana are family homes (3-4 bedrooms) set on residential lots reflecting its suburban nature. This type of housing will continue to be in strong demand, however, there is a need to provide a diversity of dwelling options (particularly more one and two bedroom dwellings) to accommodate the growing number of smaller households.

By 2036, there will be more than 3,900 additional seniors (70-84 years old) and elderly (85 years old and over) living in Kwinana the majority of whom will reside in the established suburbs.

The City will encourage a greater range of housing types to be developed in established areas close to the City Centre and neighbourhood centres, where there is convenient access to the services needed by seniors and the elderly such as shopping, social services, medical facilities, community facilities and public transport. This will support the provision of diverse types of housing and also contribute to the transformation of these neighbourhood centres into vibrant, mixed-use community hubs, integrated with public transport connections that meet the infill target set by the *Perth Peel Sub-regional Framework* of 1,365 dwellings to be built within the established areas of Kwinana by 2050.

This is an important initiative of the Local Planning Strategy with its focus on liveability and sustainable development. A key direction is to enhance and develop Kwinana's local and neighbourhood activity centres as key focal points of suburban communities. The aim is to develop these centres as 'urban villages' within the suburban landscape, with a focus on walking and cycling to activity centres for daily shopping needs and as a meeting place.



Affordable housing options are available in Kwinana with low to moderate income households comprising the largest proportion of all households. Although the existing housing stock in Kwinana is less expensive than other parts of the metropolitan area, affordable housing should continue to be dispersed in appropriate areas to accommodate low income households.

Kwinana is amongst the most culturally and linguistically diverse municipalities in the Perth metropolitan area with more than 40% of all residents born overseas and nearly 30% speaking a language other than English at home.

In the past decade, there has been a decrease in the proportion of people of English and Australian ancestry and a notable increase in the proportion of people of Filipino, Indian and

Chinese ancestry, particularly in the newer suburbs. Kwinana has a higher than Perth average of people who identify as Aboriginal and Torres Strait Islanders. The City recognises the need to encourage a diverse range of dwellings including larger houses for extended families.

Rural Living areas (1-4ha) areas provide an important rural lifestyle opportunity within Kwinana. These areas preserve amenity and retain natural bushland to create areas with rural character. There is a need to manage the interface between the urban and rural interface due to the intensity of urban development that directly abuts large rural living lots.

Rural smallholdings are highly valued by the community and provide an alternative housing option for Kwinana that has provided an appropriate land use transition from urban residential lots to rural land on the eastern side of Kwinana.

#### Strategic Directions



Promote activity centres as key nodes for shopping, entertainment, community facilities and where appropriate business services, social services, health services and housing diversity.

Ensure a range of lot sizes and dwelling types for a diversity of households, allowing residents to stay in their communities as they age as well as providing a range of housing options for young people and families with a high level of amenity.

Encourage high quality housing developments targeted to households at different life stages and income levels, including a mix of lower to moderate income groups.

#### Strategic Actions

1. Prepare precinct structure plans for the City Centre, district and neighbourhood centres to activate and enhance the viability of these centres by:
  - providing diverse, high quality housing types within walkable catchments of the City Centre, District and Neighbourhood Centres;
  - optimising accessibility to and around each activity centre via cycle routes and walking paths; and
  - facilitating an appropriate mix of land uses within activity centres.
2. Encourage a diverse range of high quality housing suitable for the changing needs of the community, dispersed within the walkable catchments of activity centres, including:
  - smaller and accessible dwellings for an aging population and people with disabilities;
  - larger houses for extended families and other household types; and
  - supporting initiatives geared towards the provision of special needs housing including good quality - affordable housing, social housing and housing for the age.
3. Monitor housing need and supply within Kwinana every two years to determine if further interventions are required through appropriate planning mechanisms.
4. Develop planning controls that implement best practice sustainable development principles for climate responsive site and building design, energy efficiency, waste and recycling and water management.
5. Ensure 'Special Rural' lots are not fragmented through subdivision and that development intensity is minimised to protect environmental values and lifestyle.

## 5.2 Character, Identity and Urban Design

Kwinana has a number of distinct characters:

- To the west is the industrial landscape of the Kwinana Industrial Area situated along the shore of Cockburn Sound. This industrial landscape is visible from Fremantle to Rockingham along the coast.
- To the east, behind a natural limestone ridge are the leafy established character suburbs of Kwinana set amongst areas of bushland. The Spectacles wetlands and Bollard Bulrush wetland provide large natural bushland that break up the urban landscape. Thomas Road and Anketell Road are surrounded by bushland and provide natural green entry points to Kwinana.
- Further east are new suburbs in close proximity to the Kwinana Freeway and Perth Mandurah Train Line. The areas east of the Kwinana Freeway include new residential areas that demonstrate a changing rural to suburban character with rural living further east, depicting a rural character and rural lifestyle.

Kwinana as we know it today has its origins in the 1950s, when the WA Government entered into an agreement with the Anglo-Iranian Oil Company (now known as BP) to construct an oil refinery on the shores of Cockburn Sound. To complement this industrial development, the State Government appointed WA's first qualified town planner, Margaret Feilman, to prepare the plans for Medina and Calista to accommodate the refinery's workforce and their families. The characteristics of the built form within the established parts Kwinana reflect the prevailing architectural forms and subdivisional design at the time of their development, including larger residential lots, large front yards, wide streets and retention of native trees. Early planning for Kwinana also provided a bushland greenbelt around a large part of Kwinana, creating a distinct community surrounded by bushland.

The challenge is to ensure the character of parts of the established areas of Kwinana such as Medina and Calista is retained for the future generations and natural bushland and landscapes are retained or re-established in new growth areas. This involves retaining prominent or distinctive landscape elements (e.g. ridge-lines, topography, views and vistas, vegetation texture in the landscape) and heritage sites that are valued by the community.

Medina was declared a *Historic Precinct* by the National Trust in May 2007, as it was specifically built to support the development of the Kwinana Industrial Area and has retained its original design principles and character. Medina was designed by WA's first qualified town planner, Margaret Feilman who was appointed the State Government, to prepare the plans for Medina and Calista to accommodate the Kwinana Industrial Area workforce and their families.

To ensure the character of Kwinana's established suburbs is retained, the City intends to review its *Residential Design Guidelines 2009* (Local Planning Policy) for the established areas of the City to determine how effective the policy has been, to ensure new development is respectful to existing character in terms of built form, appearance and impact on the streetscape and adjoining properties.

### Strategic Direction

Recognise and strengthen Kwinana's unique cultural, natural and built identity to foster a sense of place.



### Strategic Actions

6. Review and update the City of Kwinana's *Municipal Heritage Inventory* (2015) in accordance with the *Heritage Act 2018* to create a *Local Heritage Survey and Local Heritage List*.
7. Following the review of the *Municipal Heritage Inventory* develop planning measures for providing guidance and/or incentives for the protection of heritage-listed properties, where appropriate
8. Develop planning measures to ensure new development contributes to:
  - intended future character of new and future suburbs is respectful to setbacks, site coverage and built form; and
  - the character of established areas in terms of built form, landscape, appearance and impact on the streetscapes and adjoining properties.
9. Review the City of Kwinana's *Medina Neighbourhood Centre Revitalisation Strategy and Residential Design Guidelines 2009* (Local Planning Policy) as part of the preparation of the Medina precinct structure plan and include appropriate elements of the *Medina Place Plan*.
10. As part of a review of the City of Kwinana's *Residential Design Guidelines 2009* undertake a character assessment of Medina and Calista (with potential inclusion of Orelia, Parmelia and Leda) to determine what elements should be retained in established areas to ensure new developments complement and enhance the established character of the area.
11. Introduce provisions into the City of Kwinana's *Local Planning Scheme* to prevent intensification of urban development in rural areas and local natural areas, particularly within the *Jandakot Groundwater Protection Area*.
12. Review the City of Kwinana's planning provisions in relation to landscape protection, tree retention, bushfire management to improve outcomes during the preparation and assessment of structure plans and assessment of development applications.
13. Include appropriate provisions in the planning framework to ensure that subdivision and development is site responsive, incorporates site features, includes interconnected grid based road and active transport networks that maximise connectivity, accessibility, choice, and legibility, and are aligned to take advantage of views and create vistas to important buildings and spaces.
14. Include appropriate provisions in the planning framework that consider the interface between urban and rural land to ensure land use and development control conflicts are minimised.
15. Prepare a *Visual Landscape Assessment* to protect the amenity of the rural landscape, view corridors and ecological linkages.
16. Maintain and enhance the appearance and treatment of gateway points into Kwinana and their importance in contributing to character and identity (Rowley Road, Anketell Road, Thomas Road, Mortimer Road, Gilmore Road and Rockingham Road).

### 5.3 Public Open Space and Community Facilities

Public open spaces and community facilities bring people together, maintain quality of life and develop the skills, networks and relationships essential to building strong communities. Investing in community infrastructure is essential for the health, wellbeing and economic prosperity of communities.

There are significant differences in the forecast age profile and household types between the newer suburbs on the eastern side of Kwinana and the older established areas on the western side of Kwinana and the type of community infrastructure and services that will be required in these areas. The young families in the newer residential areas will need playgrounds, sporting grounds and community facilities and services. In the older established suburbs on the western side of Kwinana the population is aging and there will a greater need for health services and programs which provide the opportunity to interact and connect with others.

The City has prepared a *Community Infrastructure Plan* to guide the planning, development of open space and community facilities (libraries, community centres and recreation centres) within Kwinana. The *Community Infrastructure Plan* will be regularly reviewed to provide a range of multi-purpose community and recreation facilities that meet the changing needs of the community and support healthy and socially connected communities. A key premise of the plan is to ensure community facilities and public open space are provided as part of the shared use agreements with school sites and/or other facilities.

It is also important to identify infrastructure needs to enable development contribution plans to be prepared for community infrastructure. This ensures that developments will contribute to the provision of the essential community infrastructure required to sustain communities.

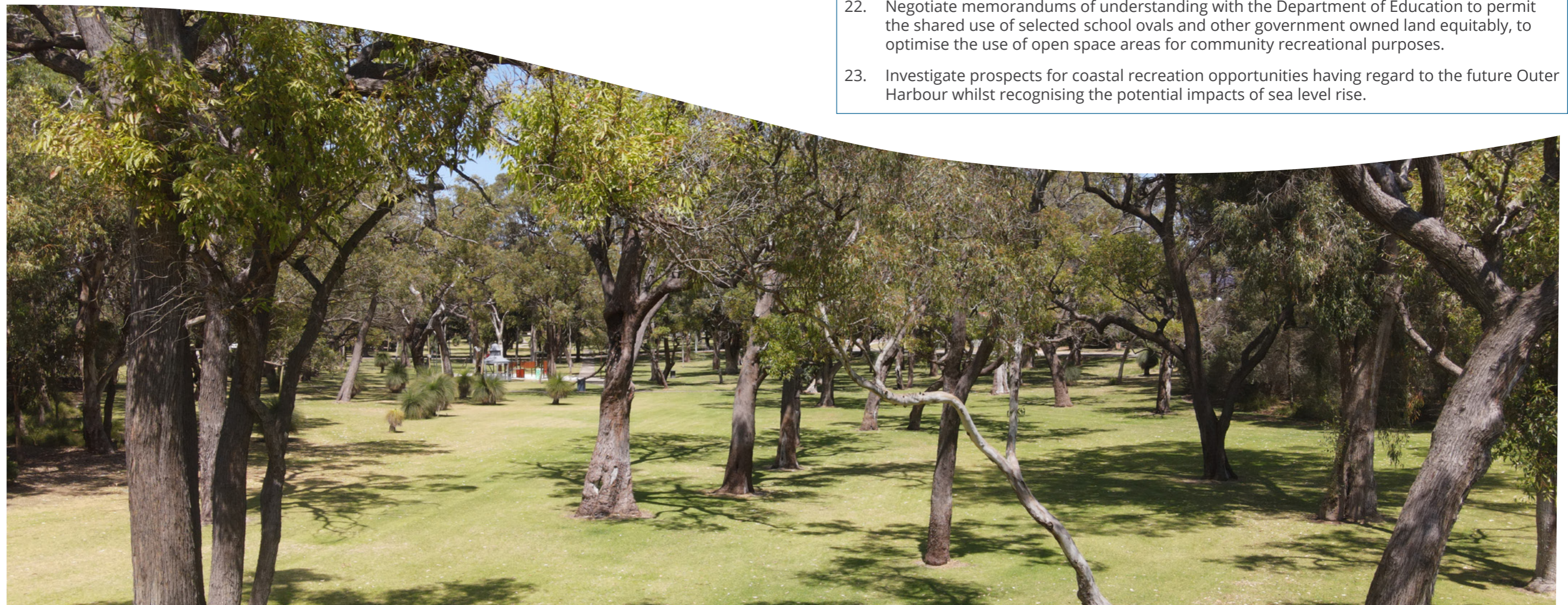
#### Strategic Direction

To deliver an equitable distribution of accessible and integrated multi-functional public open spaces, community infrastructure and recreation facilities that supports healthy and socially connected communities.



#### Strategic Actions

17. Regularly review the City of Kwinana's *Community Infrastructure Plan* to provide a range of multi-purpose community and recreation facilities that meet the changing needs of the community and support healthy and socially connected communities in a financially sustainable manner. Any consideration of a regional recreation facility should have regard to the needs and requirements of the south-west metropolitan region.
18. Implement the City of Kwinana's *Parks Upgrade Strategy (2019 – 2029)*.
19. Pursue development contributions for community, public open space, social, road and other infrastructure items for improvement or provision as appropriate, in accordance with the City of Kwinana's *Community Infrastructure Plan* and approved local structure plans.
20. Implement, and regularly review, the City of Kwinana's development contribution plans in accordance with *State Planning Policy 3.6 - Development Contributions for Infrastructure*.
21. Promote cultural heritage and history through the design of public open space, public facilities and public art to foster a sense of belonging and connection.
22. Negotiate memorandums of understanding with the Department of Education to permit the shared use of selected school ovals and other government owned land equitably, to optimise the use of open space areas for community recreational purposes.
23. Investigate prospects for coastal recreation opportunities having regard to the future Outer Harbour whilst recognising the potential impacts of sea level rise.

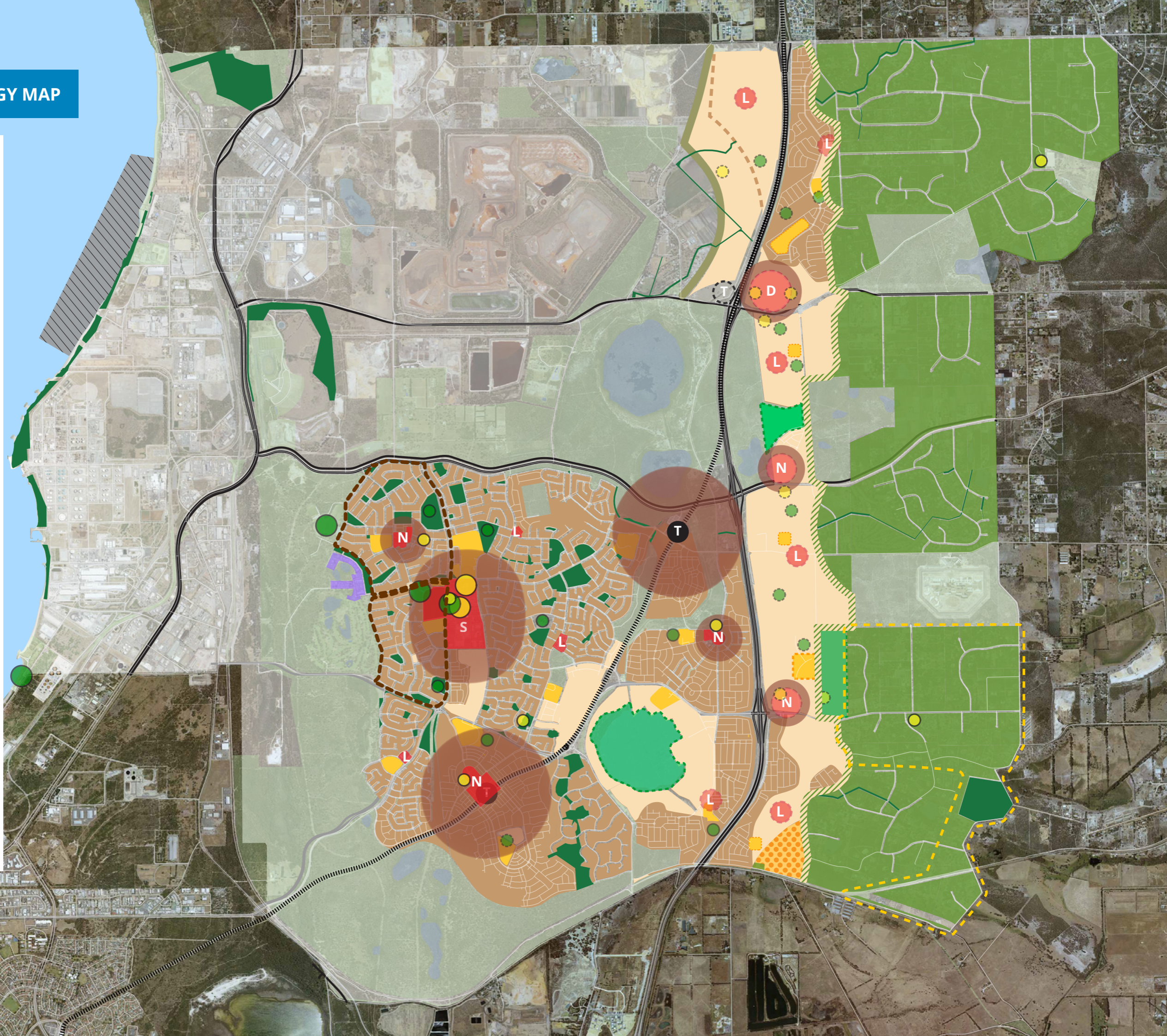





# COMMUNITY STRATEGY MAP

## KEY

- Existing Urban
- Future Urban
- Rural Living
- Medina Light Industrial
- Local Parks
- Existing Activity Centres
- Proposed Activity Centres
- S Secondary
- D District
- N Neighbourhood
- L Local
- Recreation Facility
- Community Facilities
- Proposed Recreation Facility
- Proposed Community Facilities
- Park & Reserve Investigation
- Schools
- Proposed schools
- Holding yard buffer
- Westport
- Rural to Urban Interface
- Sub-regional framework investigation area
- Industry/Urban Transition
- Walkable Catchment Area
- Character Investigation Area
- Primary Roads
- Secondary Roads
- Commuter Rail
- T Train Station
- T Train Station Proposed



## Community – Implementation Table

Strategic Direction	Actions	Time *	Lead
<b>COMMUNITY</b>			
<b>Housing and Population Diversity</b>			
<p>Promote activity centres as key nodes for shopping, entertainment, community facilities and where appropriate business services, social services, health services and housing diversity.</p> <p>Ensure a range of lot sizes and dwelling types for a diversity of households, allowing residents to stay in their communities as they age as well as providing a range of housing options for young people and families with a high level of amenity.</p> <p>Encourage high quality housing developments targeted to households at different life stages and income levels, including a mix of lower to moderate income groups.</p>	<p>1 Prepare precinct structure plans for the City Centre, district and neighbourhood centres to activate and enhance the viability of these centres by:</p> <ul style="list-style-type: none"> <li>providing diverse, high quality housing types within walkable catchments of the City Centre, District and Neighbourhood Centres;</li> <li>optimising accessibility to and around each activity centre via cycle routes and walking paths; and</li> <li>facilitating an appropriate mix of land uses within activity centres.</li> </ul> <p>2 Encourage a diverse range of high quality housing types suitable for the changing housing needs of the community dispersed within the walkable catchments of activity centres, including</p> <ul style="list-style-type: none"> <li>smaller and accessible dwellings for an aging population and people with disabilities;</li> <li>larger houses for extended families and other household types; and</li> <li>supporting initiatives geared towards the provision of special needs housing including good quality - affordable housing, social housing and housing for the aged.</li> </ul> <p>3 Monitor housing need and supply within Kwinana every two years to determine if further interventions are required through appropriate planning mechanisms.</p> <p>4 Develop planning controls that implement best practice sustainable development principles for climate responsive site and building design, energy efficiency, waste and recycling and water management.</p> <p>5 Ensure 'Special Rural' lots are not fragmented through subdivision and that development intensity is minimised to protect environmental values and lifestyle.</p>	<p>Short</p> <p>Ongoing</p> <p>Ongoing</p> <p>Short</p> <p>Ongoing</p>	<p>City Development and Sustainability</p> <p>City Development and Sustainability</p> <p>City Development and Sustainability</p> <p>City Development and Sustainability</p> <p>City Development and Sustainability</p>

Strategic Direction	Actions	Time *	Lead
<b>Character, Identity and Urban Design</b>			
<p>Recognise and strengthen Kwinana's unique cultural, natural and built identity to foster a sense of place.</p>	<p>6 Review and update the City of Kwinana's Municipal Heritage Inventory (2015) in accordance with the Heritage Act 2018 to create a Local Heritage Survey and Local Heritage List.</p> <p>7 Following the review of the City of Kwinana's Municipal Heritage Inventory develop planning measures for providing guidance and/or incentives for the protection of heritage-listed properties, where appropriate.</p> <p>8 Develop planning measures to ensure new development contributes to:</p> <ul style="list-style-type: none"> <li>intended future character of new and future suburbs is respectful to setbacks, site coverage and built form; and</li> <li>the character of established areas in terms of built form, landscape, appearance and impact on the streetscapes and adjoining properties.</li> </ul> <p>9 Review the City of Kwinana's Medina Neighbourhood Centre Revitalisation Strategy and Residential Design Guidelines 2009 (Local Planning Policy) as part of the preparation of the Medina precinct structure plan and include appropriate elements of the Medina Place Plan.</p> <p>10 As part of a review of the City of Kwinana's Residential Design Guidelines 2009 undertake a character assessment of Medina and Callista (with potential inclusion of Orelia, Parmelia and Leda) to determine what elements should be retained in established areas to ensure new developments complement and enhance the established character of the area.</p> <p>11 Introduce provisions into the Local Planning Scheme to prevent intensification of urban development in rural areas and local natural areas, particularly within the Jandakot Groundwater Protection Area.</p> <p>12 Review the City of Kwinana's planning provisions in relation to landscape protection, tree retention, bushfire management to improve outcomes during the preparation and assessment of structure plans and assessment of development applications.</p>	<p>Short</p> <p>Short</p> <p>Medium</p> <p>Medium</p> <p>Short</p> <p>Short</p> <p>Short</p>	<p>City Development and Sustainability</p> <p>City Development and Sustainability</p> <p>City Development and Sustainability</p> <p>City Development and Sustainability</p> <p>City Development and Sustainability</p> <p>City Development and Sustainability</p> <p>City Development and Sustainability</p>

Short term	1-3yrs
* Medium term	4-10yrs
Long term	10+yrs



Strategic Direction (Continued)	Actions	Time *	Lead
Recognise and strengthen Kwinana's unique cultural, natural and built identity to foster a sense of place.	13 Include appropriate provisions in the planning framework to ensure that subdivision and development is site responsive, incorporates site features, includes interconnected grid based road and active transport networks that maximise connectivity, accessibility, choice, and legibility, and are aligned to take advantage of views and create vistas to important buildings and spaces.	Short	City Development and Sustainability
	14 Include appropriate provisions in the planning framework that consider the interface between urban and rural land to ensure land use and development control conflicts are minimised.	Short	City Development and Sustainability
	15 Prepare a Visual Landscape Assessment to protect the amenity of the rural landscape, view corridors and ecological linkages.	Short	City Development and Sustainability Needs to be done before 12 12&14 to be done concurrently
	16 Maintain and enhance the appearance and treatment of gateway points into Kwinana and their importance in contributing to character and identity (Rowley Road, Anketell Road, Thomas Road, Mortimer Road, Gilmore Road and Rockingham Road).	Ongoing	City Development and Sustainability

Strategic Direction	Actions	Time *	Lead
Public Open Space and Community Facilities To deliver an equitable distribution of accessible and integrated multi-functional public open spaces, community infrastructure and recreation facilities that supports healthy and socially connected communities.	17 Regularly review the City of Kwinana's Community Infrastructure Plan to provide a range of multi-purpose community and recreation facilities that meet the changing needs of the community and support healthy and socially connected communities in a financially sustainable manner. Any consideration of a regional recreational facility should have regard to the needs and requirements of the south west metropolitan region.	Ongoing	City Engagement
	18 Implement the City of Kwinana's Parks Upgrade Strategy (2019 - 2029).	Ongoing	City Infrastructure
	19 Pursue developer contributions for community, public open space, social, road and other infrastructure items for improvement or provision as appropriate, in accordance with the City of Kwinana's Community Infrastructure Plan and approved local structure plans.	Ongoing	City Development and Sustainability
	20 Implement, and regularly review, the City's Developer Contribution Plans in accordance with State Planning Policy 3.6 - Development Contributions for Infrastructure.	Ongoing	City Development and Sustainability
	21 Promote cultural heritage and history through the design of public open space, public facilities and public art to foster a sense of belonging and connection.	Ongoing	City Engagement City Infrastructure
	22 Negotiate memorandums of understanding with the Department of Education to permit the shared use of selected school ovals and other government owned land equitably, to optimise the use of open space for community recreational purposes.	Ongoing	City Engagement
	23 Determine opportunities for coastal recreation having consideration to the future Outer Harbour and recognising the potential impacts of sea level rise.	Medium	

Short term	1-3yrs
Medium term	4-10yrs
Long term	10+yrs

\*

# ECONOMY



6

## 6 ECONOMY

### 6.1 Industry and the Outer Harbour

The City of Kwinana's Gross Regional Product is estimated at 3.77 billion (2018) with much of this economic base heavily concentrated in the industrial sector located in the Kwinana Industrial Area. The Kwinana Industrial Area is the largest and most complex heavy and general industry site in the State and is of critical importance to the prosperity of the State and the Perth metropolitan area.

The Kwinana Industrial Area also includes key port facilities for industry, particularly bulky goods trade. The State Government has recently announced its intent to develop the Kwinana Outer Harbour in the Kwinana Industrial Area as the State's new container port.

The *Hope Valley Wattleup Redevelopment Act 2000 Area* (Latitude 32) is partly located in Kwinana and, while the Flinders Precinct was developed over the past decade, much of the Latitude 32 area remains undeveloped. Controlled by Development WA, Latitude 32 is situated immediately adjacent the Kwinana Industrial Area effectively adding to a large area of land set aside for industry in the City. In turn, the Kwinana Industrial Area and Latitude 32 form a significant part of the Western Trade Coast which also includes industry based land in the City of Rockingham and City of Cockburn. Additional land for industry has also been identified as Industrial Expansion or Industrial Investigation (Mandogalup and Postans) in Kwinana which would add to the Western Trade Coast. Collectively, these areas make up large tracts of developed and undeveloped land set aside for industrial development.

The economy of the Kwinana Industrial Area and broader Western Trade Coast continues to grow and this should continue into the next two decades and beyond. The challenges are to ensure that this growth is optimised and occurs in a well-planned and integrated way to maximise the strategic value of the locality for the State and Kwinana community.

Key to this is orderly planning for the Western Trade Coast, particularly in relation to the proposed Outer Harbour to ensure that the separation between heavy and general industry and sensitive land uses (such as residential areas) is maintained and reinforced as part of future planning. This protects industry growth and ensures that residential suburbs such as Leda, Calista and Medina are avoided the negative impacts of industry such as poor air quality, odour and noise. The same considerations should apply to the planning for the industrial expansion and investigation areas.

It is also appropriate that a comprehensive master plan be prepared by key agencies including local government for the Western Trade Coast. Long overdue, this is particularly relevant given the State Governments announcement of the new port. The master plan would then inform statutory planning instruments including local planning schemes to guide optimal outcomes.

The master plan should consider:

- separation of industry from sensitive land uses;
- industrial land use types and future need such as the;
  - supply of high impact heavy industry sites given the existing core of heavy industry areas in the Kwinana Industrial Area A (largely west of Rockingham Road) is rapidly filling. Additional sites need to be considered for expansion, possibly parts of Latitude 32 at Wattleup.
  - most appropriate locations of storage, freight and logistics and office uses which need not be located on core heavy industry sites but further afield close to 24/7 freight links and easy access to customers and suppliers
  - form and function of the Industrial Expansion and Industrial Investigation Areas including Postans and the Alcoa Residue Disposal Area at Mandogalup

- key transport links including freight links to the Kwinana Outer Harbour and intermodal facilities as well as the function and connectivity of local road systems;
- strategic funding opportunities and seed funding to open up areas constrained by land assembly issues; and
- staging and implementation plans across the Western Trade Coast.

It is not since the *Fremantle Rockingham Industrial Area Regional Strategy*, prepared two decades ago that any comprehensive planning for the Western Trade Coast and its component parts was undertaken. Given the strategic importance and complexity of this locality, it is considered crucial that key stakeholders re-engage and prepare a master plan. The City will advocate strongly in this regard as part of the Local Planning Strategy.

The Local Planning Strategy recognises that by 2036 the Kwinana Outer Harbour will be in place and operational. The provision of freight corridors to connect the future harbour to the broader metropolitan transport network requires a number of east-west links to be upgraded. Whilst Anketell Road will be the major freight route to the Outer Harbour, Rowley Road and Thomas Road and the Fremantle Rockingham Controlled Access Highway will be important links to the Kwinana Industrial Area and port areas.

Further, the Outer Harbour will operate 24 hours a day and heavy freight vehicles may cause noise and vibration impacts to properties in the vicinity of freight corridors. Planning for the land adjacent to the freight corridors should take into consideration the potential impact of noise emissions from heavy freight vehicles (particularly Anketell Road).

Postans, which comprises 87ha of land west of the proposed Fremantle Rockingham Controlled Access Highway and approximately 50ha of land adjacent to the Motorplex on the Alcoa Residue Disposal Area, is recognised as an Industrial Expansion Area in the Perth Peel Sub-regional Framework. Draft structure planning for this precinct was undertaken by the then Department of Planning, Landcorp and City of Kwinana in 2012 and recognised the strategic value of the site. This planning should be completed in the context of the wider Master Plan for the Western Trade Coast.

There is future opportunity for Kwinana as a residential and service location for personnel involved with the defence assets maintenance phase at the Australian Marine Complex and for any expansion of Navy operations at Garden Island. The Australian Defence Force will also require significant parcels of appropriately located land in the medium and longer term seeking to locate in the Western Trade Coast.

There is 6.27ha of Light Industry zoned land located in Medina surrounded by Parks and Recreation reserves and Kwinana Golf Club. This industrial area contains 42 lots with area ranging from 728m<sup>2</sup> to 2,750m<sup>2</sup> and have an assortment of light industrial activities. The location of this light industrial zoned land is a historical legacy and remains a local employment and service hub for the community. Consideration should be given to the long term development of light Industry within the Postans Precinct area and whether there is any potential for alternative land uses in the Medina Light Industrial Area in the long term.



## Strategic Directions



Recognise the importance of the Kwinana Industrial Area and future Kwinana Outer Harbour to secure future employment opportunities for Kwinana residents.

Expand land use options for a range of industrial land uses to be located in Kwinana having regard to land use compatibility.

## Strategic Actions

24. Advocate for the development of a comprehensive masterplan for the Western Trade Coast (including the Alcoa Residue Disposal Area and Kwinana Outer Harbour) to facilitate a more proactive approach to industrial land use planning and provide an ongoing supply of industrial land to cater for employment in Kwinana.
25. Include provisions in the City of Kwinana's *Local Planning Scheme* to protect the Kwinana Industrial Area from encroachment by incompatible development and sensitive land uses. The area to be protected should be based on the following reviews:
  - Review of the *Kwinana Air Quality Buffer – Position Paper* (2008); and
  - Kwinana Air Quality Review: Options and Recommendations for a Revised Buffer for Areas 5 and 9 (2010) (adopted by the WAPC in 2011).
26. Review and adopt the City of Kwinana's *Postans Study* with consideration to land requirements for light industrial and service commercial land uses and the long term future of the Medina Light Industrial complex.
27. Review the future industrial land requirements including the:
  - potential to zone appropriate parts of the Alcoa Residue Disposal Area for 'General Industry'; and
  - optimal use of the 'Service Commercial' zone on the east and west side of Rockingham Road, Naval Base (between Lionel Street and Beard Street).

## 6.2 Kwinana City Centre and other Activity Centres

The Kwinana City Centre and a network of activity centres and service commercial precincts are also important economic drivers for Kwinana and a key focal point for retail and commercial growth over the next 15 years.

### Kwinana City Centre

The Kwinana City Centre is designated as a Secondary Centre under the activity centres hierarchy and *Southern Metropolitan Peel Sub-regional Planning Framework* and is to remain the largest, and by far the most important, activity centre in the City. It contains significant retail, other commercial, educational, civic, cultural and recreational activities. The prosperity of the Market Place Shopping Centre is critical to the attraction of the City Centre. The ongoing growth and development of the City Centre is a key focus of the Local Planning Strategy.

The City Centre has been guided by the *Kwinana Town Centre Master Plan and Design Guidelines* and *Local Planning Scheme No. 3*. It is recommended that the Master Plan continue to serve as the primary guiding plan for the City Centre but the master plan and LPS3 be reviewed and updated

into contemporary planning mechanisms, most likely a Precinct Centre Plan and appropriate zoning within a single planning scheme.

The City Centre is evolving, and is faced with a number of challenges, including:

- lack of a developed Main Street;
- lack of entertainment facilities available in the evening;
- separation from open space and residential areas to the west;
- high number of vacant tenancies; and
- large expanses of car parking.

There are currently around 1,850 people employed in the Kwinana City Centre, and this could potentially double by 2036. The development of the Kwinana Outer Harbour and the Australian Defence Force maintenance facilities at the Australian Marine Complex and Garden Island will provide development opportunities within the City Centre, such as the potential to develop training facilities. The City is also a significant landowner in the City Centre which also provides opportunities for new development at strategic sites.

The City will review the *Local Commercial and Activity Centres Strategy* to facilitate the development of retail, entertainment, child care, health, education, training and community facilities in the City Centre.



## Activity Centres

A key objective of the Local Planning Strategy is to develop the role of the City's neighbourhood and local centres to be key vibrant community focal points for meeting the day to day needs of residents and to create places where people can meet. The focus is on developing these centres as 'urban villages' within the suburban landscape of the City. The enhancement of neighbourhood centres will contribute in many ways to community, but it is important that every support be given to the economic growth of the centres. The focus is on more intensive and diverse housing within and in close proximity to the City Centre, districts centre and neighbourhood centres creating housing choice that is accessible to retail options, local jobs and transport links. This will encourage a local and sustainable approach to urban form.

Precinct centre planning for district and neighbourhoods will be undertaken over the course of the Local Planning Strategy's implementation to identify and realise opportunities for each centre having regard to the immediate walkable catchment to inform planning measures and controls for established activity centre. Master plans are to be considered to support the public realm and urban design improvements to address issues such as connectivity, car parking and landscaping.

Two of the centres are transit oriented developments (Kwinana and Wellard Village) which offer great opportunity for mixed development and serve as important transport functions for the community. It is important the right balance is achieved for these centres so they serve the local community and provide appropriate transport services for the wider community.

A district centre at Wandí is proposed to be developed at the corner of the Kwinana Freeway and Anketell Road which is intended to serve the new local communities at Wandí, Mandogalup and Anketell and potentially customers further afield, given its location on the freeway interchange. The narrowness of the urban corridor in the vicinity of the district centre is also something of a constraint on the Wandí centre's retail floor space potential, but this is being offset to some extent by the relatively high suburban residential

densities being planned in the locality. The site has some land assembly constraints to be overcome but it is well located for growth which would need to be considered against the best interests of the Kwinana City Centre. There is also considerable potential for other-retail and bulky goods floor space in the Wandí district centre.

As part of the Local Planning Strategy, the City will be investigating the potential for a mixed use - office based precinct occupying an area spanning from the Kwinana Freeway and Thomas Road intersection to the Kwinana Train station. Much of the site is vacant urban land with some areas set aside for conservation. The intent is to capitalise on the following locational advantages:

- spatial location along the Kwinana Freeway which provides good locational access across the Perth metropolitan area from the Perth CBD to Mandurah;
- the immediate traffic volumes passing the site on a daily basis along the freeway and using Thomas Road (movement economy) of the site;
- the Kwinana train station providing access to the Perth CBD in 30 minutes;
- the attractive outlook over The Spectacles, a large conservation estate;
- the immediate link with growing service commercial land uses on the eastern side of the Kwinana Freeway / Thomas Road interchange; and
- the opportunity to attract offices across the southern part of Perth metropolitan area but also those which may be located in the Western Trade Coast as that area gradually develops for general industry.

The Kwinana Freeway provides opportunities for car based bulky and white good land uses that have a metropolitan wide catchment. Properties situated around the Kwinana Freeway off-ramps are well placed to take advantage of this exposure. The City supports the planning and development of service commercial precincts which take advantage of Kwinana Freeway interchange locations at Thomas and Anketell Roads with these precincts now in the development phase. The City will also explore the opportunity for a service commercial based precinct in

Mandogalup on the north western corner of the Anketell Road interchange with the Kwinana Freeway. While Anketell Road is a critical freight link to the outer harbour, the location at the Freeway interchange and potential for a future train station at this location (a train station reserve is reserved but not part of current planning for passenger train stations) lends itself to an investigation for an activity centre at this location.

The *Local Commercial and Activity Centres Strategy* provides a detailed list of actions for each activity centre and has guided the City's decision making in relation to the preparation and assessment of local structure plans and development applications.

The Local Planning Strategy seeks to review the:

- hierarchy as identified in the *Local Commercial and Activity Centres Strategy* to reinforce the viability and vibrancy of the existing centres;
- opportunity for a mixed use - office based activity centre at both the Anketell Road and Thomas Road interchange west of the Kwinana Freeway; and
- Promote and develop service commercial precincts which take advantage of Kwinana Freeway interchange locations at Thomas and Anketell Roads.

## Strategic Direction

To define the role and extent of the City's activity centres that will establish a focus for everyday shopping, recreation and entertainment requirements of the residents and provide employment, community meeting places and associated land uses.



## Strategic Actions

28. Review the City of Kwinana's *Local Commercial and Activity Centre Strategy* to define:
  - an appropriate hierarchy of mixed use activity centres that meet the everyday shopping, recreation and entertainment requirements of residents and provide local employment;
  - appropriate land uses (commercial / office) at each of the employment nodes / interchanges along the Kwinana Freeway; and
  - downgrade Leda neighbourhood centre to a local centre
29. Promote the City Centre as the primary centre within the City of Kwinana and prepare and implement a *City Centre Precinct Structure Plan* to:
  - optimise the potential of the City Centre as a key location for retail, restaurant, entertainment (night and day), arts, childcare, housing, education, health services, social services and short term accommodation;
  - investigate options for high density residential development and short-term accommodation;
  - review the design of Chisham Avenue so that it will function as a 'Main Street';
  - investigate the potential for strategic sites within the City Centre to lead investment and redevelopment within Kwinana; and
  - review the function of Gilmore Road;
  - differentiate the City Centre by creating and enhancing links to the local bushland and open spaces; and
  - develop advocacy and partnership plans to support the City Centre development.

30. Prepare precinct structure plans for each of the district and neighbourhood centres that:
  - support a mix of land uses to activate and enhance the viability of these centres;
  - maximise access to and within activity centres by walking, cycling and public transport; and
  - ensure development within the activity centre is well designed cohesive and functional.
31. Review *Local Planning Scheme No. 2* to:
  - facilitate the establishment of local retail uses in local centres for the convenience of local residents; and
  - ensure the primacy of the Kwinana City Centre is reinforced and out-of-centre development that undermines the hierarchy of activity centres is discouraged.
32. Limit the extent of commercial incursion into residential and industrial areas by appropriately designating industrial and commercial land to meet the demand for such land uses to 2036.
33. Prepare a master plan for the Bertram Neighbourhood Centre to provide a framework for public realm improvements including pedestrian access, disability access, car parking, landscaping, street furniture, wayfinding and the design of public spaces.
34. Encourage redevelopment of the following local centres:
  - Orelia local centre – facilitate mixed use redevelopment
  - Calista local centre – rezone the site to residential development.
  - Summerton Road – rezone the site to residential development.



### 6.3 Employment

The City of Kwinana is a key generator (in the Perth metropolitan area) of jobs in the manufacturing and construction industries. This is illustrated by Figure 2: Overall Employment in the City of Kwinana below, which compares overall employment generated in the City of Kwinana against jobs created elsewhere across greater Perth. Also evident is that the City has comparatively less jobs in retail trade, health care and professional and technical fields.

The historic presence of heavy and general industry, and manufacturing and construction jobs plays a key role in the growth of these industries in Kwinana. It is of value to the State both as a strategic economy base and employer and is promoted as part of the Local Planning Strategy.

Consideration should be given, however, to the increasing automation of industry, particularly manufacturing and transport logistics and the erosion of lower skilled employment in such industries (which can use large tracts of general industry zoned land).

This demonstrates that whilst development of industrial land for manufacturing purposes continues to take place, the employment numbers will not necessarily be growing at the same rate.

#### Overall Employment: Kwinana Strong in Manufacturing, Construction

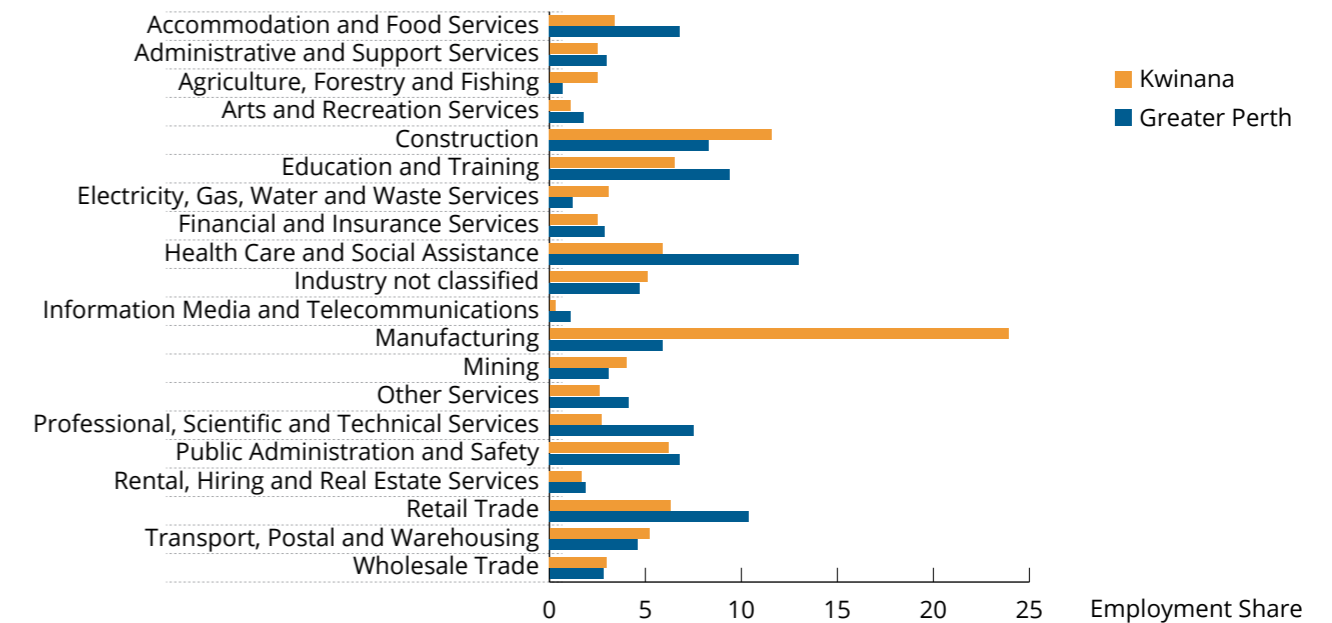


Figure 2: Overall Employment in the City of Kwinana

#### Employment Self-Sufficiency and Self Containment

Most of these manufacturing and construction industries jobs are filled by people living outside the City of Kwinana however and it is evident that Kwinana's employment self-sufficiency (measure of the relative participation and potential participation of a local workforce in locally available jobs) is less than that of the surrounding south west region and amongst the lowest in the Perth metropolitan area.

The size of the City's total labour force in 2016 was 18,829, which includes all persons aged above 15. Overall, 89.2% of the labour force was employed and 10.8% unemployed, compared with 91.7% and 8.3% respectively for South West Group. When added to the people looking for work, either full or part-time, 21.6% of the labour force is not in work. By comparison the South West Group equals 16.5%.

While there has been some recent growth in employment in Kwinana in recent years, employment is not keeping up with the rapid population growth. As can be seen on Figure 3: Kwinana Employment Growth below, from 2006 to 2018 employment in Kwinana has remained steady at around 15,500 jobs, while the population has increased from 24,000 to over 42,000 over the same time period.

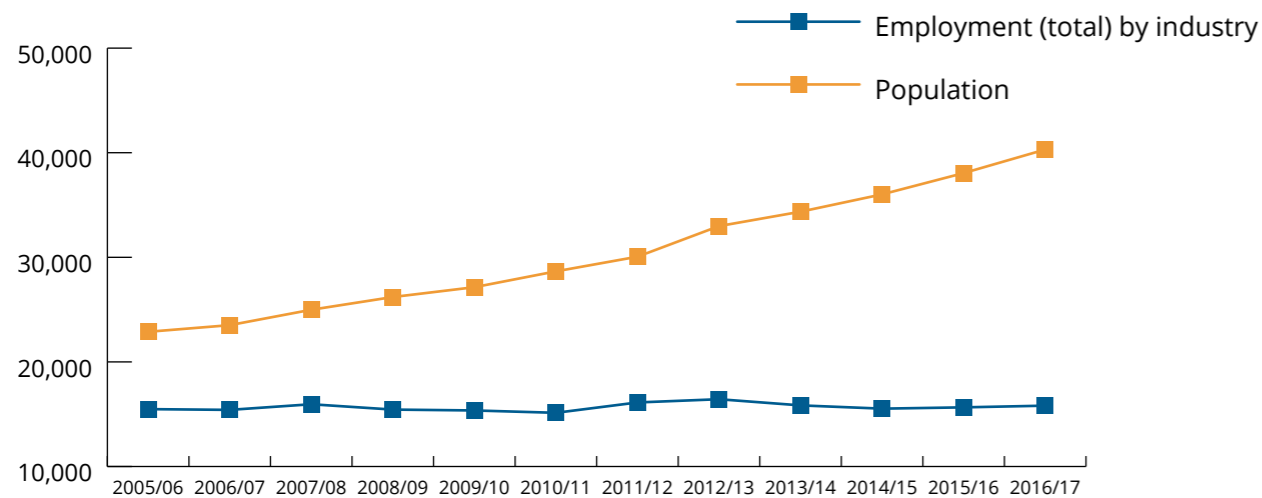


Figure 3: Kwinana Employment Growth

Further, it is evident that approximately 25% of local jobs are filled by local residents. It indicates that local residents are not dominant in filling local jobs and travel outside of the City for work. The major reasons for Kwinana's resident workers seeking employment outside of Kwinana could be the nature of employment opportunities versus the residents skills and qualifications; transport options available and commuting times; relationship between wages and salaries (people will travel further for higher paid jobs); and house prices in the local area.



## New Employment Opportunities

The challenge for the City is to consider opportunities for job creation for the local population as part of planning for 2036 so that it better matches population growth and population skills and training. Figure 4 shows an assessment of the gap and the distribution of jobs types to 2036, and, some strategic direction as to how the gap could be breached.

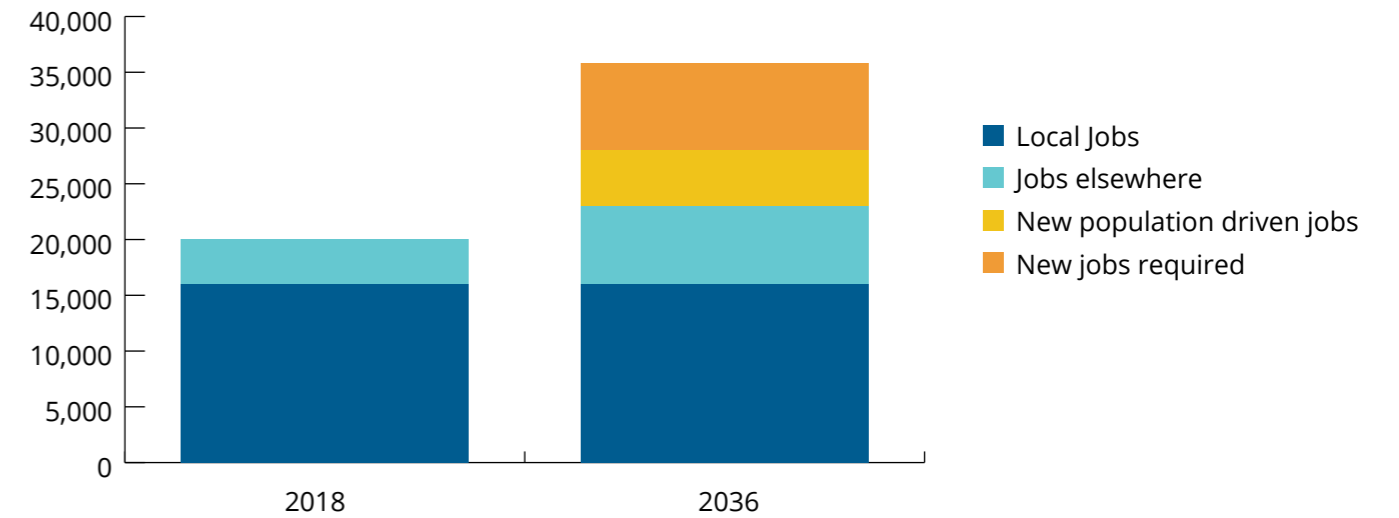


Figure 4: Employment Self-Sufficiency

It assumes that a proportion of local residents will travel to work outside of the City and it is important that transport systems and opportunities are supporting people travelling out of Kwinana and into the City to work.

Further, it assumes that a proportion of these jobs (around one-third) will be population-driven, namely jobs that arise to directly service the local population. They include local retail and business services, health services, education services (particularly primary and secondary education) and personal services.

Expanding on this, an analysis of the jobs held by the resident population in the City in 2016 shows the three most popular industry sectors were:

- Health Care and Social Assistance (1,973 people or 11.7%)
- Retail Trade (1,808 people or 10.8%)
- Construction (1,780 people or 10.6%)

It can be expected that the number of people employed in these sectors will grow as the population of Kwinana and the region grows but it is critical that future planning seeks to maximise employment in these areas.

This is especially relevant when considering there is a gap of approximately 7,000 jobs that would need to be found within the City by 2036 to meet this aspirational target.

Employment in retail and services is very important to the employment prospects of Kwinana residents, particularly as they offer jobs for the young and unskilled workforce. This is best illustrated in Figure 5 below which highlights employment by employment categories. Shown by the blue arrows are the higher proportion of jobs for youth and unskilled persons in 'retail trade' and 'accommodation and food services'. This is relevant given unemployment in younger age groups which is well above the Perth average in Kwinana. Opportunities for expanded retail and entertainment uses such as café and restaurants as well as service commercial uses should be explored in the City's Centres and other key locations.

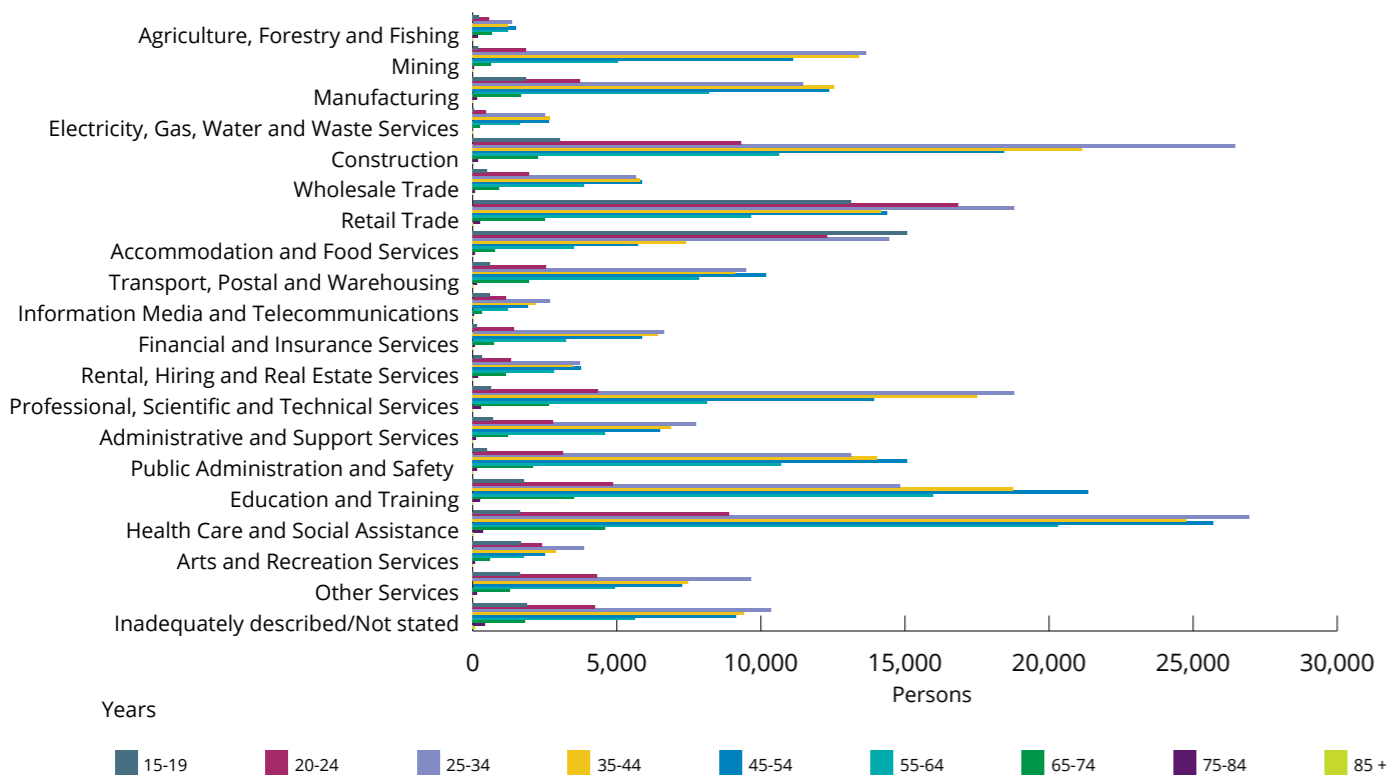


Figure 5: Youth Employment in the Perth Metropolitan Area

The increases in small, often home based business over the past decade also provides an opportunity for a range of employment in the City, particularly in the provision of professional services, information technology, home based food business and many other services.

Enabling then the growth of such industries and businesses that will provide employment opportunities for Kwinana residents is fundamental to the Local Planning Strategy with a focus on ensuring sufficient land is available for industry and local businesses.

### Health Services

Between 2016 and 2036, Kwinana's population in the 70-84 age group will increase by approximately 3,100 and by 835 persons in the 85 and above age group. Kwinana's expanding aged population will increase the demand for health services and generate the need for at least 2-3 additional aged care facilities plus in-home services. These are large employers - the

aged care workforce could almost double by 2036.

An expanding population will also mean increased demand for community based health services which should locate within the City Centre.

### Service Commercial

As previously noted, the Local Planning Strategy seeks to promote Service Commercial Precincts to take advantage of the location and exposure of the Kwinana Freeway interchanges. As noted, retail employment opportunities offer the best option to make inroads into unemployment in younger age groups which is well above average in Kwinana.

These sites provide the best strategic opportunity to seek to close the gap between employment growth and population growth. Provided they are appropriately managed under the new local planning scheme, their presence would have minimal effect on

existing activity centres, including the City Centre itself. The development of these precincts takes advantage of a comparative advantage the City has with sites like these available on the Kwinana Freeway.

There are opportunities for Service Commercial Precincts to the:

- east side of the Thomas Road and Kwinana Freeway interchange where land has already been approved under structure plans for service commercial land uses. This area has considerable potential for development into a large service commercial area offering a range of bulky and white goods retail services.
- East side of Anketell Road - Kwinana Freeway interchange where the future Wandil District Centre is located (with the long term potential to develop the Anketell Road Train Station).
- west side of the Anketell Road - Kwinana Freeway interchange in Mandogalup where service commercial uses would be an appropriate interface between Anketell Road and future urban land uses and may be linked to a railway station in the longer term).

### Mixed Use - Office Based Precinct

The City will investigate the feasibility of two mixed use - office based precincts to the

- south-west of the Thomas Road / Kwinana Freeway interchange with integration to the Kwinana Train Station; and
- Anketell Road - Kwinana Freeway interchange in Mandogalup.

The two centres may be suitable for a range of office, commercial and residential uses. The opportunity is for a centre which provides a range of employment opportunities including professional and administrative jobs which can attract business into the City from further afield.

### Tourism

The Spectacles, Kwinana Loop Trail, Tramways Trail and the Wildflower Walk provide unique opportunities to walk and cycle through bushland areas within the metropolitan area. The general public may not be aware of these areas and the City will investigate opportunities for eco-tourism within Kwinana to provide employment opportunities and promote environmental stewardship. The ecological links between the reserves combined with cycle and walking paths provide an opportunity for local tourism unique to Kwinana.

The opportunity for heavy Industry focused tourism opportunities should not be underestimated with many countries actively promoting industrial tourism. There is a market for people interested in learning about a range of industry with the Kwinana Industrial Area containing a wide range of industry at impressive scales from major processing plants, power plants, desalination plants and waste to energy plants. An integrated masterplan for the Kwinana Industrial Area and the City's future *Economic Development Strategy* will provide a key platform for such opportunities in to the future.





## Small Business

There are a total of 1,945 businesses in the City and while there are many large businesses located in Kwinana, approximately 1,851 of these, or 95%, are small businesses. In fact, despite Kwinana appearing to be the home of large industries, there are only 68 businesses based in Kwinana that employ over 20 people.

- The major areas of small business operations are in the roles of:
- Transport, Postal and Warehousing;
- Construction; and
- Professional Scientific and Technical Services.

Over the last year, businesses grew at over 2 per week to 134 new businesses commencing in Kwinana, appearing to be exclusively small businesses. Most of the new businesses established in the past 12 months are from the following industries:

- Transport, Postal and Warehousing;
- Administrative and Support Services; and
- Rental, Hiring and Real estate services.

Additionally, there are a total of 97 home based businesses that operated in Kwinana in 2019, predominantly within Wellard, Bertram, Wandi and Parmelia. The City will optimise the potential of home based businesses as an employment growth sector to improve self-employment opportunities in Kwinana. In this respect, the City has the capacity to reduce barriers to home based business via reduced administrative overheads and integrated assessment processes.

## Manufacturing and Industry

Industry remains an important employer for the City's residents and its growth will benefit the local community in this regard. This includes the heavy industrial land uses to light Industry and there is considerable potential for significant expansion into the longer term with the growth of Latitude 32 and growth in the industrial expansion and industrial investigation areas such as Postans. There are opportunities for Light Industry / Service Commercial uses in particular to expand into the Postans locality providing local employment opportunities.



## Strategic Direction

To create diverse employment opportunities for local residents and maintain sustained economic growth by ensuring that suitable serviced employment land is available.



## Strategic Actions

35. Identify the Thomas Road - Kwinana Freeway interchange as an investigation area for the purposes of office / mixed use employment. This should include consideration of:
  - the advantages of being located along the Kwinana Freeway including:
    - good locational access across the Perth metropolitan area from the Perth Central Business District to Mandurah.
    - the immediate traffic volumes passing the area on a daily basis along both the Freeway and Thomas Road (i.e.: the movement economy).
  - integration with the Kwinana train station.
  - providing for expanding or relocating office and business uses from the Kwinana Industrial Area, which would free up sites in industrial areas for industrial uses and provide employment at this location which is much better served by public transport;
  - regionally and locally significant environmental values;
  - proximity to the service commercial area and neighbourhood centre on the eastern side of the Thomas Road - Kwinana Freeway interchange;
  - short-term accommodation and conference centre options; and
  - traffic flow and safety of the transport network.
36. Promote the development of a range of Service Commercial (Bulky / White Good) precincts at key Kwinana Freeway interchanges of Anketell Road and Thomas Road. Investigate the north western corner of Anketell Road - Kwinana Freeway interchange for service commercial land uses.
37. Investigate opportunities for eco-tourism within Kwinana to provide employment opportunities and promote environmental stewardship as part of the City of Kwinana's future *Economic Development Strategy*, including the development of an eco-tourism centre alongside established natural attractions such as The Spectacles, Kwinana Loop Trail, Tramways Trail and the Wildflower Walk.
38. Optimise the potential of home based businesses as a key employment growth sector.
39. Investigate options for an integrated development process across the City of Kwinana administration (Planning, Building, Health, Environment, Communities and Engineering) to integrate and streamline social, economic and environmental processes and considerations across the agency.

## 6.4 Education and Training

Opportunities for attracting education and training facilities into the City should be explored to boost the local economy but also to provide local youth with skills relevant to local jobs and employment generally.

The Australian Defence Force plans to establish an office / training facility easily accessible to the Australian Marine Complex provides an opportunity for employment opportunities within Kwinana. There may also be the potential for the Kwinana TAFE campus to expand to provide semi-professional and trade training for the defence industry and the Kwinana Industrial Area.

### Strategic Direction

The community has a choice of quality public and private facilities to meet their education and training needs.



### Strategic Actions

40. Investigate appropriate locations for education land as part of the preparation of precinct structure plans for the City Centre, district and neighbourhood centres.
41. Introduce provisions in the City of Kwinana's *Local Planning Scheme* to allow for training needs of the community within Neighbourhood, District and the City Centres.
42. Advocate for the establishment of training facilities within Kwinana to provide semi-professional and trade training for the defence industry and other industries with the Kwinana Industrial Area.

## 6.5 Agriculture

Until recently large parts of Kwinana were rural, however following the extension of the Kwinana Freeway and the Perth-Mandurah Train Line, areas east of the Freeway have been rezoned to facilitate the establishment of new suburbs. This has occurred in Wandi, Anketell, Casuarina, Wellard and Mandogalup with urban development continuing to expand in these areas for the next decades. The last remaining rural areas are situated further east of the Kwinana Freeway and are zoned 'Rural A', 'Special Rural' and 'Rural Water Resource'.

The *South Metropolitan Peel Subregional Planning Framework* identified part of the rural area to the east of the Kwinana Freeway and south of Casuarina Prison as a Planning Investigation Area. This includes part of the last remaining 'Rural A' zoned areas and the Western Australian Planning Commission will ultimately determine the land uses in the Planning Investigation Area.

As a result of the planning changes over the past two decades, the role of agriculture in the City's economy has declined and many rural industries have moved away from the constraints of suburban sprawl and rural living.

In this regard, the City should investigate the best long term use of land within the remaining 'Rural A' zoned land in *Local Planning Scheme No. 2*. This will be influenced considerably by the Western Australian Planning Commission's decision on the Planning Investigation Area and the City's focus on environmental factors such as the need to retain biodiversity, and manage and minimise water quality impacts on both the Jandakot Groundwater Mound and drainage catchment of the Peel - Harvey Estuarine System.

There are a number of agricultural businesses operating under non-conforming uses rights within areas set aside for future urban development or in localities where environmental legislation has changed over the past decades meaning that the use may no longer be appropriate if new approval was sought today. These uses may ultimately seek to relocate away from constraints but efforts should be made by the City to seek to incorporate these uses into Service Commercial areas where it is possible.

### Strategic Direction

To review the City's rural land use with a view to medium and long term opportunities.



### Strategic Actions

43. Investigate the long term future of the remaining 'Rural A' zoned land in the City of Kwinana Local Planning Scheme in view of its identification as an 'Investigation Area' in the *Sub-Regional Framework Planning Investigation Area* and the City's objectives to retain biodiversity and manage and minimise water quality impacts on both the Jandakot Groundwater Mound and drainage catchment of the Peel - Harvey Estuarine System.
44. Where feasible, seek to incorporate agricultural uses operating under non-conforming use rights into Service Commercial zones.

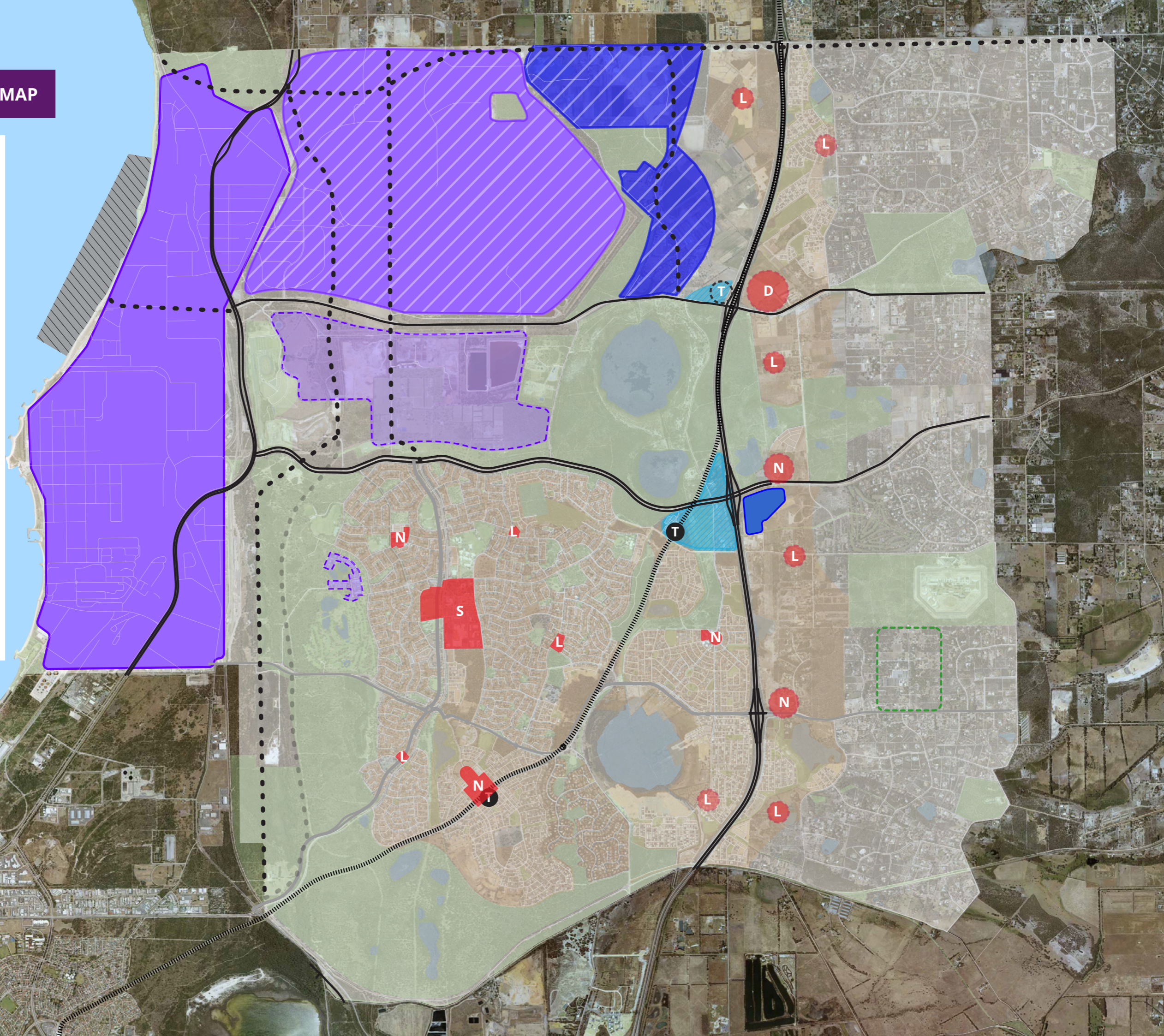




# ECONOMY STRATEGY MAP

## KEY

- Westport
- Industry
- Industry Expansion
- Industry Investigation
- Future Roads
- MainRoads WA proposed future roads
- Existing Activity Centres
- Proposed Activity Centres
- Secondary
- District
- Neighbourhood
- Local
- Improvement Plan 47
- Commercial/Mixed Business Investigation
- Service Commercial
- Rural Investigation area
- Primary Roads
- Secondary Roads
- Train Station
- Proposed Train Station
- Commuter Rail



## Economy – Implementation Table

Strategic Direction	Actions	Time *	Lead	
<b>ECONOMY</b> <b>Industry and the Outer Harbour</b> Recognise the importance of the Kwinana Industrial Area and future Kwinana Outer Harbour to secure future employment opportunities for Kwinana residents. Expand land use options for a range of industrial land uses to be located in Kwinana having regard to land use compatibility.	24	Advocate for the development of a comprehensive masterplan for the Western Trade Coast (including the Alcoa Residue Disposal Area and Kwinana Outer Harbour) to facilitate a more proactive approach to industrial land use planning and provide an ongoing supply of industrial land to cater for employment in Kwinana.	Ongoing	City Development and Sustainability
	25	Include provisions in the City of Kwinana's Local Planning Scheme to protect the Kwinana Industrial Area from encroachment by incompatible development and sensitive land uses. The area to be protected should be based on the following reviews: <ul style="list-style-type: none"> <li>Review of the Kwinana Air Quality Buffer – Position Paper (2008); and</li> <li>Kwinana Air Quality Review: Options and Recommendations for a Revised Buffer for Areas 5 and 9 (2010) (adopted by the WAPC in 2011).</li> </ul>	Short	City Development and Sustainability
	26	Review and adopt the City of Kwinana's Postans Study with consideration to land requirements for light industrial and service commercial land uses and the long term future of the Medina Light Industrial complex.	Medium	City Development and Sustainability
	27	Review the future industrial land requirements including the: <ul style="list-style-type: none"> <li>potential to zone appropriate parts of the Alcoa Residue Disposal Area for General Industry; and</li> <li>optimal use of the 'Service Commercial' zone on the east and west side of Rockingham Road, Naval Base (between Lionel Street and Beard Street).</li> </ul>	Medium	City Development and Sustainability



Strategic Direction	Actions	Time *	Lead	
<b>Kwinana City Centre and other Activity Centres</b> To define the role and extent of the City's activity centres that will establish a focus for everyday shopping, recreation and entertainment requirements of the residents and provide employment, community meeting places and associated land uses.	28	Review the City of Kwinana's Local Commercial and Activity Centre Strategy to define: <ul style="list-style-type: none"> <li>an appropriate hierarchy of mixed use activity centres that meet the everyday shopping, recreation and entertainment requirements of residents and provide local employment;</li> <li>appropriate land uses (commercial/office) at each of the employment nodes/interchanges along the Kwinana Freeway; and</li> <li>downgrade Leda Neighbourhood Centre to a Local Centre.</li> </ul>	Short	City Development and Sustainability
	29	Promote the City Centre as the primary centre within the City of Kwinana and prepare and implement a City Centre Precinct Structure Plan to: <ul style="list-style-type: none"> <li>optimise the potential of the City Centre as a key location for retail, restaurant, entertainment (night and day), arts, childcare, housing, education, health services, social services and short term accommodation;</li> <li>investigate options for high density residential development and short-term accommodation;</li> <li>review the design of Chisham Avenue so that it will function as a 'Main Street';</li> <li>investigate the potential for strategic sites within the City Centre to lead investment and redevelopment within Kwinana; and</li> <li>review the function of Gilmore Road;</li> <li>differentiate the City Centre by creating and enhancing links to the local bushland and open spaces; and</li> <li>develop advocacy and partnership plans to support the City Centre development.</li> </ul>	Ongoing	City Development and Sustainability

Strategic Direction	Actions	Time *	Lead
<b>(Continued)</b> To define the role and extent of the City's activity centres that will establish a focus for everyday shopping, recreation and entertainment requirements of the residents and provide employment, community meeting places and associated land uses.	30 Prepare precinct structure plans for each of the district and neighbourhood Centres that <ul style="list-style-type: none"> <li>• supports a mix of land uses to activate and enhance the viability of these centres;</li> <li>• maximises access to and within activity centres by walking, cycling and public transport; and</li> <li>• ensures development within the activity centre is well designed cohesive and functional.</li> </ul>	Medium	City Development and Sustainability
	31 Review Local Planning Scheme No. 2 to: <ul style="list-style-type: none"> <li>• facilitate the establishment of local retail uses in Local Centres for the convenience of local residents; and</li> <li>• ensure the primacy of the Kwinana City Centre is reinforced and out-of-centre development that undermines the hierarchy of activity centres is discouraged.</li> </ul>	Short	City Development and Sustainability
	32 Limit the extent of commercial incursion into residential and industrial areas by appropriately designating industrial and commercial land to meet the demand for such land uses to 2036.	Ongoing	City Development and Sustainability
	33 Prepare a master plan for the Bertram Neighbourhood Centre to provide a framework for public realm improvements including pedestrian access, disability access, car parking, landscaping, street furniture, wayfinding and the design of public spaces.	Medium	City Development and Sustainability
	34 Encourage redevelopment of the following local centres: <ul style="list-style-type: none"> <li>• Orelia local centre - facilitate mixed use redevelopment</li> <li>• Calista local centre – rezone the site to residential development.</li> <li>• Summerton Road - rezone the site to residential development.</li> </ul>	Short	City Development and Sustainability

**Strategic Direction****Diversifying Employment**

To create diverse employment opportunities for local residents and maintain sustained economic growth by ensuring that suitable serviced employment land is available.

Strategic Direction	Actions	Time *	Lead
To create diverse employment opportunities for local residents and maintain sustained economic growth by ensuring that suitable serviced employment land is available.	35 Identify the Thomas Road - Kwinana Freeway Interchange as an investigation area for the purposes of office / mixed use employment. This should include consideration of: <ul style="list-style-type: none"> <li>• The location advantages of the site along the Kwinana Freeway including:               <ul style="list-style-type: none"> <li>• good locational access across the Perth metropolitan area from the Perth Central Business District to Mandurah.</li> <li>• the immediate traffic volumes passing the site on a daily basis along the freeway and using Thomas Road (movement economy) of the site.</li> <li>• integrating the Kwinana train station.</li> </ul> </li> <li>• providing for expanding or relocating office and business uses from the Kwinana Industrial Area which would free up sites in industrial areas for industrial uses and provide employment at sites much better served by public transport than the Kwinana Industrial Area;</li> <li>• regionally and local significant environmental values;</li> <li>• proximity to the service commercial area on the eastern side of the Thomas Road - Kwinana Freeway interchange;</li> <li>• short-term accommodation and conference centre options;</li> <li>• traffic flow and safety of the transport network; and</li> <li>• alignment of the Perth Bunbury Natural Gas Pipeline.</li> </ul>	Medium	City Development and Sustainability
	36 Promote the development of a range of Service Commercial (Bulky / White Good) Precincts at key Kwinana Freeway interchanges at Anketell Road and Thomas Road. Investigate the north western corner of Anketell Road - Kwinana Freeway interchange for service commercial land uses.	Medium	City Development and Sustainability
	37 Investigate opportunities for eco-tourism within Kwinana to provide employment opportunities and promote environmental stewardship as part of the City of Kwinana's future Economic Development Strategy, including the development of an eco-tourism centre alongside established natural attractions such as The Spectacles, Kwinana Loop Trail, Tramways Trail and the Wildflower Walk.	Medium	City Development and Sustainability

Strategic Direction	Actions	Time *	Lead
<b>(Continued)</b> To create diverse employment opportunities for local residents and maintain sustained economic growth by ensuring that suitable serviced employment land is available.	38 Optimise the potential of home based businesses as a key employment growth sector.	Ongoing	City Development and Sustainability
	39 Investigate options for an integrated development process across the City of Kwinana administration (Planning, Building, Health, Environment, Communities and Engineering) to integrate and streamline social, economic and environmental processes and considerations across the agency.	Short	City of Kwinana
<b>Education and Training</b>			
The community has a choice of quality public and private facilities to meet their education and training needs.	40 Investigate appropriate locations for education land as part of the preparation of precinct structure plans for the City Centre, District and Neighbourhood Centres.	Medium	City Development and Sustainability
	41 Introduce provisions in the City of Kwinana's Local Planning Scheme to allow for training needs of the community within Neighbourhood, District and the City Centres.	Short	City Development and Sustainability
	42 Advocate for the establishment of training facilities within Kwinana to provide semi-professional and trade training for the defence industry and other industries with the Kwinana Industrial Area.	Ongoing	City Development and Sustainability
<b>Agriculture</b>			
To review the City's rural land use with a view to medium and long term opportunities.	43 Investigate the long term future of the remaining 'Rural A' zoned land in the City of Kwinana Local Planning Scheme in view of its identification as an 'Investigation Area' in the Sub-Regional Framework Planning Investigation Area and the City's objectives to retain biodiversity and manage and minimise water quality impacts on both the Jandakot Groundwater Mound and drainage catchment of the Peel - Harvey Estuarine System.	Long	City Development and Sustainability
	44 Where feasible, seek to incorporate agricultural uses operating under non-conforming use rights into Service Commercial zones.	Ongoing	City Development and Sustainability

Short term	1-3yrs
* Medium term	4-10yrs
Long term	10+yrs



# ENVIRONMENT



# 7. ENVIRONMENT

## 7.1 Local Natural Areas, Landscapes and Ecological Linkages

Bushland, wetlands and landscape features contribute significantly to Kwinana's sense of place and quality of life within urban and rural areas, and provide important habitats to sustain unique ecosystems and support biodiversity.

Kwinana's population and housing forecasts indicate significant growth over the next 15 years. This will place considerable pressure on the local environment and present ongoing challenges associated with the retention of bushland, wetlands and landscapes. It is important that future population growth in Kwinana does not impact on bushland and wetlands and that areas of significant vegetation are protected.

Prior to European settlement there was nearly 12,000ha of native bushland (including wetlands) within Kwinana, of which approximately 45% remains (20% of which is protected through *Bush Forever* and State Government conservation reserves).

Figure 6 identifies significant areas of local bushland within each of the major vegetation complexes. These bushland areas are of regional and/or local environmental significance and are under threat from subdivision and development approvals.

The LPS will ensure a *Local Biodiversity Strategy* is adopted to provide a framework for the protection and management of significant local bushland in Kwinana that aren't currently designated for conservation purposes.

Kwinana's leafy suburban character needs to be protected and enhanced. The City has an established street tree planting policy which has been in place since 1992 and has contributed to an 8% increase from bare ground to green cover since 2016 and is an important ongoing program to retain and where possible expand to ensure the following can be realised.

- cooling residential streets and open spaces during extreme heat;
- contributing to a green leafy neighbourhoods;
- providing shade to encourage walking and cycling; and
- enhancing local biodiversity.

The interface between the urban and rural areas is an important planning consideration for areas east of the Kwinana Freeway as identified in Section 5 - Community. This is particularly the case in regards to landscape values of the rural areas, landscape features and ecological linkages.

### Ecological Linkages

Ecological Linkages connect locally and regionally significant natural areas and provide stepping stones for flora and fauna. Ecological connectivity is vital to the long-term viability of native flora and fauna, and is recognised as one of the most effective tools available for conserving biodiversity.

A number of ecological linkages are identified in the *Local Biodiversity Strategy* which link locally and regionally significant natural areas and provide stepping stones for flora and fauna. These are shown in Figure 6 below:

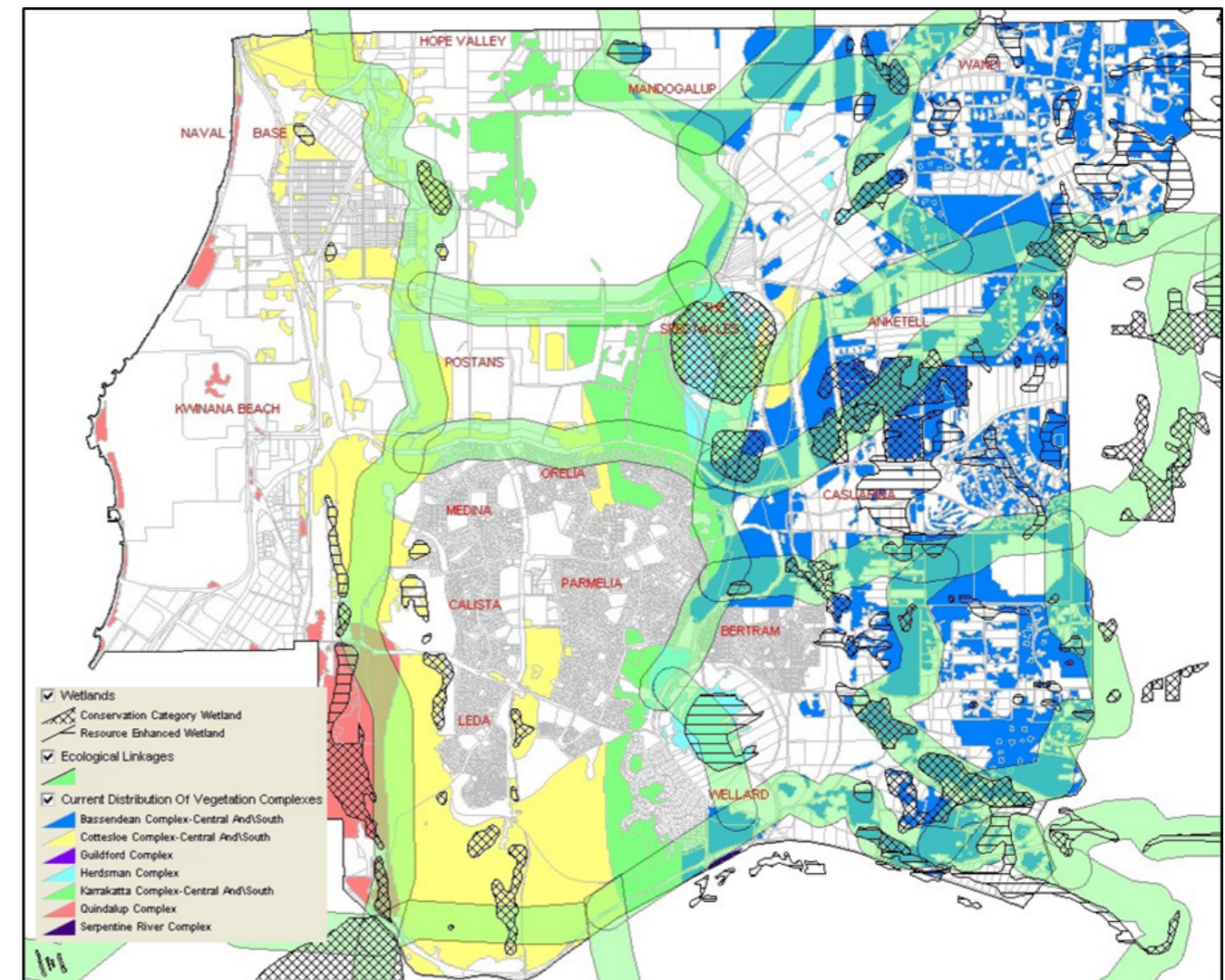


Figure 6: Vegetation complexes and Ecological linkages within Kwinana

### Re-alignment of the Fremantle - Rockingham Primary Regional Road

This proposed free-flow freight corridor is intended to provide efficient, high-capacity, free-flow connection from the Kwinana Freeway through the Kwinana Industrial Area, connecting to the Kwinana Outer Harbour.

The City will investigate options regarding the alignment of the Fremantle Rockingham Primary Regional Road to protect significant environmental values and maintain the green belt as a buffer between residential areas and industrial development.

Land-use and development within the City is influenced by water related issues. The Jandakot Water Mound, Peel Estuary Serpentine River Catchment, wetlands and seasonal expression of ground-water in areas with high ground-water tables have been considered as part of the preparation of the Local Planning Strategy. Increasingly, sea level rise and coastal recession will require consideration of planning conditions that limit time or particular events.



## Bushfire Management

The majority of rural land within Kwinana is classified as bushfire prone. Land use and development in rural areas therefore needs to respond to the threat of bushfire, balancing the potential impact of bushfire risk management regimes on the environment and natural resources. Specific planning and building requirements apply to developments within designated bushfire prone areas in accordance with the *Planning and Development (Local Planning Schemes) Regulations 2015* and *State Planning Policy 3.7 Planning in Bushfire Prone Areas*. The City has a responsibility to ensure sensitive or potentially hazardous land uses do not increase the risks on life and property.

### Strategic Direction



To identify, permanently protect and enhance Kwinana's natural environment which is critical to the maintenance of ecological processes and biodiversity

### Strategic Actions

45. Advocate for the inclusion of areas of significant environmental value within the Parks and Recreation reserve in the *Metropolitan Region Scheme* including, initiating an amendment to the *Metropolitan Region Scheme* to reserve Bollard Bulrush Wetland and Lot 9000 Treeby Road, Anketell as Parks and Recreation in the Metropolitan Region Scheme.
46. Advocate to the Federal and/or State Government for the protection, and where appropriate, management of nationally or regionally significant areas.
47. Adopt a *Local Biodiversity Strategy* which prioritises natural areas requiring conservation, and protects significant landscape features and ecological linkages.
48. Include provisions in the City of Kwinana's *Local Planning Scheme* to protect environmental values, significant landscape features and ecological linkages consistent with an adopted *Local Biodiversity Strategy*.
49. Regularly update the City of Kwinana's *Natural Areas Management Plan*.
50. Review all public open space under the management of the City of Kwinana to ensure vesting is consistent with conservation objectives (Local Reservation categorisation, Management Orders and title classification).
51. Implement the City of Kwinana's *Streetscape Upgrade Strategy* (2019 -2029).
52. Prepare and implement the City of Kwinana's *Urban Forest Strategy*.
53. Develop planning mechanisms to :
  - protect significant trees;
  - ensure high quality bushland remnants are prioritised for retention as part of any structure planning for new areas;
  - require all new industrial and commercial developments to retain existing trees and plant mature trees where appropriate; and
  - develop strategies to promote the retention of street trees by the community and raise community awareness and encourage community participation in biodiversity protection.
54. Explore programs that will assist landholders to enhance the condition and on-going conservation of Local Natural Areas.



55. Develop measures for ecological corridors, outlining revegetation, restoration and landscaping standards expected of developers, and requirements for other landscape elements to strengthen connections, such as wider verges and median strips to accommodate street trees.
56. Ensure that an emphasis is placed on the separation of land uses with off-site odour, noise, air-quality impacts to sensitive land uses.
57. Ensure future planning and development appropriately manages bushfire risk, which may include the development of local planning controls for bushfire management.
58. Apply *Water Sensitive Urban Design* principles where compatible with other design objectives.
59. Investigate the cumulative impact of native vegetation clearing and appropriate measures to reduce clearing and reinstate native planting.
60. Advocate to Main Roads Western Australia to realign the Fremantle-Rockingham Primary Regional Road to protect significant environmental values and maintain the green belt as a buffer between residential areas and industrial development.

## 7.2 Climate Change Mitigation and Adaptation

Western Australia, particularly the south-west, is vulnerable to the impacts of climate change which is causing:

- increases in average annual temperatures, leading to an increase in the number and severity of heatwaves and an increase in the number of high fire risk days and potential severe bushfires;
- reductions in average annual rainfall, causing environmental stress and potential death of native vegetation;
- sea level rise, with increased erosion along coastal areas and potential inundation of low lying coastal areas during severe storm events; and
- changes in the frequency and severity of storm events with a predicted trend for an increase in extreme events.

The City has recognised its responsibility to act on climate change and has adopted a *Climate Change Mitigation and Adaption Plan (2015-2020)*. The major climate change risks relevant to the City's operations identified in the Plan are temperature change, reduced rainfall, rising sea levels and extreme weather events which may affect the City's assets as follows:

- increased temperatures and temperature extremes,
- increased bushfires;
- erosion along coastal areas;
- increased pressure on drainage systems and inundation of low lying areas during severe storm events; and
- natural areas affected by drought conditions.



With an increasing population within Kwinana, climate change adaptation and improving community resilience will be important part of land use and infrastructure planning. The City notes the risk of the increasing heat island effect, which is exacerbated by vegetation clearing for development and road expansion. Strategies to reduce the urban heat island effect can include increasing landscaping and tree canopy cover.

Reduced water availability is being addressed through the City of Kwinana's *Sustainable Water Management Plan* which was developed at the same time as this Local Planning Strategy. The following goals for water use reductions are set out in the plan:

- Reduce scheme water consumption by 5% on 2016/17 levels by 2023; and
- Maintain groundwater abstraction at 2016/17 levels until 2023.

Significant climate modelling work has been undertaken by agencies such as CSIRO and robust trend data has been established to support the case to adapt settlements to cope with expected changes in sea level, temperature, rainfall, bushfire and storm events and to determine suitable responses to enable this. This has implications for the City as, within its boundaries, the coast will be subject to rising sea levels and there are areas that are prone to bushfire.

### Strategic Direction



Promote planning measures that encourage climate change adaptation and mitigation to ensure our communities are both resilient and liveable.

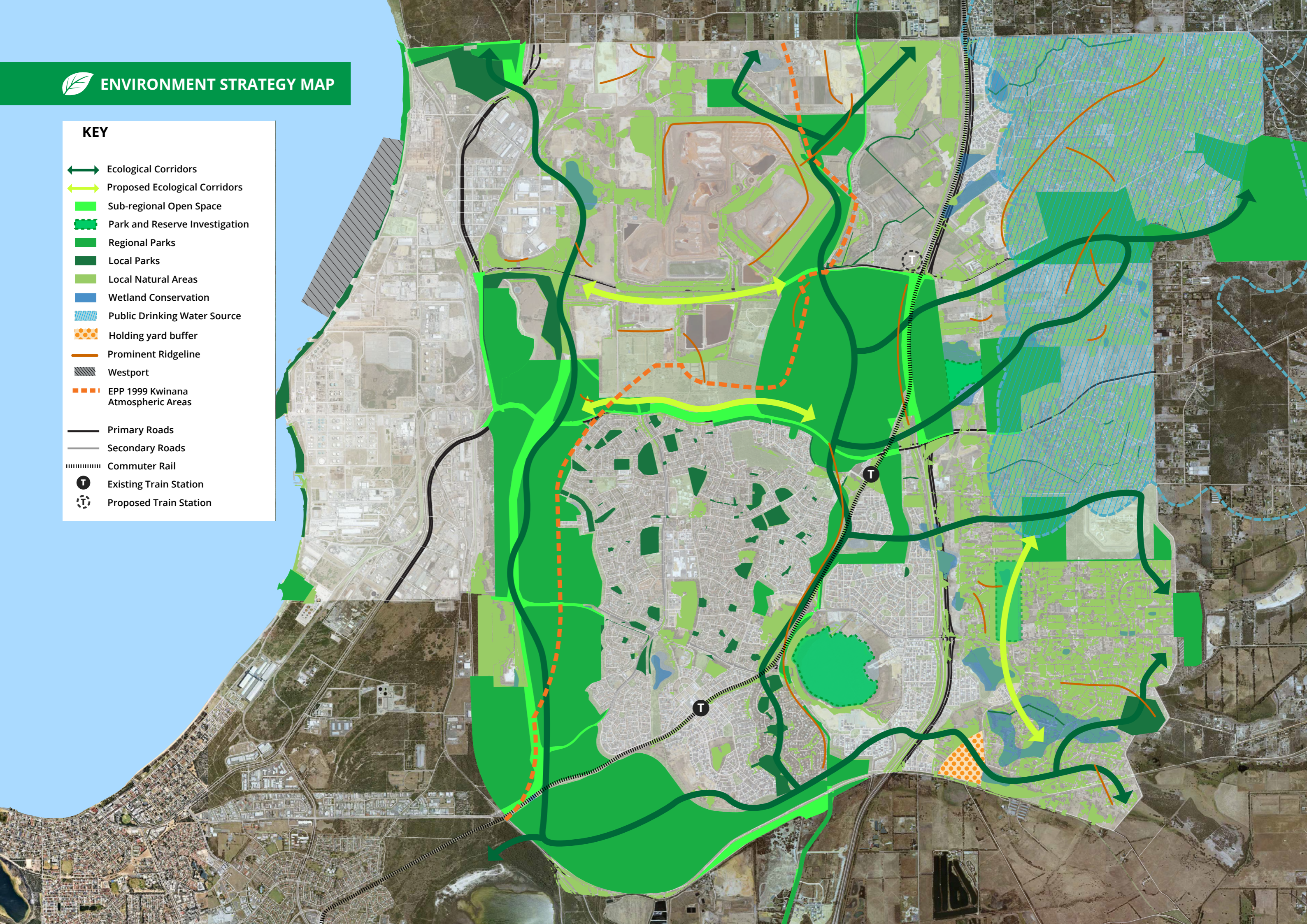
### Strategic Actions

61. Prepare a sea level rise policy based on the adaptation options for vulnerable areas outlined in the *Cockburn Coastal Vulnerability Values and Risk Assessment* to enable land use planning to respond to coastal hazard risks.
62. Implement planning mechanisms that support the outcomes of the City of Kwinana's *Climate Change Plan*.
63. Manage and protect water resources in urban environments in accordance with the planning framework set out in the State Government's *Better Urban Water Management Guidelines*.


# ENVIRONMENT STRATEGY MAP

## KEY

- ↔ Ecological Corridors
- ↔ Proposed Ecological Corridors
- Sub-regional Open Space
- Park and Reserve Investigation
- Regional Parks
- Local Parks
- Local Natural Areas
- Wetland Conservation
- Public Drinking Water Source
- Holding yard buffer
- Prominent Ridgeline
- Westport
- EPP 1999 Kwinana Atmospheric Areas
- Primary Roads
- Secondary Roads
- Commuter Rail
- Existing Train Station
- Proposed Train Station



## Environment – Implementation Table

Strategic Direction	Actions	Time *	Lead
<b>ENVIRONMENT</b>			
<b>Protect Local Natural Areas, Landscapes and Ecological Linkages</b>			
To identify, permanently protect and enhance Kwinana's natural environment which is critical to the maintenance of ecological processes and biodiversity.	45 Advocate for the inclusion of areas of significant environmental value within the Parks and Recreation reserve in the Metropolitan Region Scheme including, initiating an amendment to the Metropolitan Region Scheme to reserve Bollard Bulrush Wetland and Lot 9000 Treeby Road, Anketell as Parks and Recreation in the Metropolitan Region Scheme.	Short	City Development and Sustainability
	46 Advocate to the Federal and/or State Government for the protection, and where appropriate, management of nationally or regionally significant areas.	Ongoing	City Development and Sustainability
	47 Adopt a Local Biodiversity Strategy which prioritises natural areas requiring conservation, and protects significant landscape features and ecological linkages.	Short	City Development and Sustainability
	48 Include provisions in the City of Kwinana's Local Planning Scheme to protect environmental values, significant landscape features and ecological linkages consistent with an adopted Local Biodiversity Strategy.	Short	City Development and Sustainability
	49 Regularly update the City of Kwinana's Natural Areas Management Plan.	Ongoing	City Development and Sustainability
	50 Review all public open space under the management of the City of Kwinana to ensure vesting is consistent with conservation objectives (Local Reservation categorisation, Management Orders and title classification).	Short	City Development and Sustainability
	51 Implement the City of Kwinana's Streetscape Upgrade Strategy (2019 -2029).	Ongoing	City Infrastructure
	52 Prepare and implement the City of Kwinana's Urban Forest Strategy.	Short	City Development and Sustainability

Short term	1-3yrs
* Medium term	4-10yrs
Long term	10+yrs

Strategic Direction	Actions	Time *	Lead	
<b>(Continued)</b>				
To identify, permanently protect and enhance Kwinana's natural environment which is critical to the maintenance of ecological processes and biodiversity.	53 Develop planning mechanisms to : <ul style="list-style-type: none"> <li>• protect significant trees;</li> <li>• ensure high quality bushland remnants are prioritised for retention as part of any structure planning for new areas;</li> <li>• require all new industrial and commercial developments to retain existing trees and plant mature trees where appropriate; and</li> <li>• develop strategies to promote the retention of street trees by the community and raise community awareness and encourage community participation in biodiversity protection.</li> </ul>	Short	City Development and Sustainability	
	54 Explore programs that will assist landholders to enhance the condition and on-going conservation of Local Natural Areas.	Ongoing	City Development and Sustainability	
	55 Develop measures for ecological corridors, outlining revegetation restoration and landscaping standards expected of developers and requirements for other landscape elements to strengthen connections, such as wider verges and median strips to accommodate street trees.	Short	City Development and Sustainability	
	56 Ensure that an emphasis is placed on the separation of land uses with off-site odour, noise, air-quality impacts and sensitive land uses.	Ongoing	City Development and Sustainability	
	57 Ensure future planning and development appropriately manages bushfire risk, which may include the development of local planning controls for bushfire management.	Ongoing	City Development and Sustainability	
	58 Apply Water Sensitive Urban Design principles where compatible with other design objectives.	Ongoing	City Development and Sustainability & City Infrastructure	
	59 Investigate the cumulative impact of native vegetation clearing and appropriate measures to reduce clearing and reinstate native planting.	Medium	City Development and Sustainability	
	<b>Climate Change Mitigation and Adaptation</b>			
	Promote planning measures that encourage climate change adaptation and mitigation to ensure our communities are both resilient and liveable.	60 Prepare a sea level rise policy based on the adaptation options for vulnerable areas outlined in the Cockburn Coastal Vulnerability Values and Risk Assessment to enable land use planning to respond to coastal hazard risks.	Short	City Development and Sustainability
		61 Implement planning mechanisms that support the outcomes of the City's Climate Change Plan.	Short	City Development and Sustainability
62 Manage and protect water resources in urban environments in accordance with the planning framework set out in the State Government's Better Urban Water Management Guidelines.		Ongoing	City Development and Sustainability	

# INFRASTRUCTURE



## 8. INFRASTRUCTURE

### 8.1 Transport

Typical of Perth's outer suburbs, residential and commercial development within Kwinana is low density and spread out and Kwinana's residents are dependent on private vehicles to travel to schools, shops, services, employment and recreation.

An integrated transport network to distribute traffic and to provide linkages to other parts of the metropolitan region is crucial in facilitating access for the residents of Kwinana.

The City aims to make it safer and more convenient for all residents of varying incomes, needs and abilities to walk, cycle, use public transport and drive within Kwinana on an integrated network of paths, roads and public transport routes.

Access to regional roads and the freight rail network also provides opportunities for industries, businesses and the future Kwinana Outer Harbour to be well-connected to markets and suppliers.

#### Cycling and Walking

The City adopted the *Bike and Walk Plan* (2018) to provide a safe network of cycle routes and walking paths that are convenient for people of all ages and abilities to ride or walk to local destinations such as schools, shops, parks, bushland, bus stops and train stations. The intention is to create communities where cycling and walking are the preferred choices for short trips and everyone has the opportunity to choose a healthy lifestyle.

The *Bike and Walk Plan* also integrates the local network of cycling routes and walking paths with the regional cycling and walking network to connect with areas beyond Kwinana.

The Local Planning Strategy draws upon the *Bike and Walk Plan* to prioritise pathways that support and link activity centres

#### Metronet and Transperth

The City of Kwinana is serviced by the Mandurah Train Line, stopping at Kwinana Train Station and Wellard Train Station.

The Kwinana Train Station is located well to the north of residential and commercial zones, reducing its local catchment by at least 50%. This creates a high demand for the park 'n' ride service even from the surrounding residential development. The Local Planning Strategy seeks to support access to the train stations by advocating for more frequent bus connections to the trains station as opposed to increased parking. Furthermore investigation into the potential land uses in and around the train station seeking to intensify development around the stations ensuring convenient access to train services.

There is also a site reserved for a train station in the *Metropolitan Region Scheme* in Mandogalup (near the Kwinana Freeway / Anketell Road interchange). This station is not currently under consideration by the State Government, however, a train station in this location would provide opportunities for local residents to readily commute to work and education facilities within the Metropolitan area. The Local Planning Strategy seeks to explore opportunities for a transit oriented development to be considered at Mandogalup to support a station precinct.

The *South Metropolitan Peel Sub-Regional Framework* identifies two strategic bus corridors through Kwinana as described below:

- A High-Frequency Transit Corridor running between Kwinana Train Station and Wellard Train Station via the Kwinana City Centre. This could support increased land use density as an Activity Corridor, or maintain consistent mobility goals as a Transit Corridor.
- A High-Priority Transit Corridor between Rockingham and Fremantle via the Kwinana City Centre. This would improve direct access to Kwinana, but is primarily intended to be an efficient commuter service delivering employees to destinations along the coast, including the Kwinana Outer Harbour and supporting industry. This corridor is likely to require bus priority in the form of queue facilities or partial bus lanes to maintain consistent times.

#### Freight Network

The movement of freight through Kwinana provides essential economic benefits to the overall Western Australian economy and is a key consideration in terms of the overall efficiency and safety of the Western Trade Coast and Kwinana Outer Harbour.

The efficiency of freight movement is reliant on the provision of designated road and rail corridors that are separated from local traffic and are not restricted due to their potential impact on the amenity of incompatible land uses.

The Local Planning Strategy aims to manage land use conflict in the vicinity of the freight corridors by designating adjoining land use appropriately and by also ensuring sensitive land uses are adequately buffered from vehicle noise and vibration impacts.



## Gilmore Avenue

Gilmore Avenue functions as an important regional route between Kwinana and Rockingham, but this role conflicts with its role as a local access route to the Kwinana City Centre. The high traffic volume reduces opportunities for pedestrians crossing making it difficult to walk or cycle to the City Centre.

As demand continues to grow, and particularly if Gilmore Avenue were to be extended, the use of Gilmore Avenue by regional traffic can be expected to increase. This will impinge upon its function as a key public transport corridor and reduces the potential for activity along the edge of the City Centre.

Local congestion along Gilmore Avenue that results from high traffic demands and restricted turning movements can be expected to get worse as regional traffic increases. As such, a strategic view of the function of this road in the context of the Kwinana City Centre is required, weighing up the various needs and prioritising the value of activity, local access and regional purpose, public, private and active transport.

## Autonomous, Electric vehicles and Ride Share Services

It is anticipated that there will be an increasing transition towards electric vehicles and automated vehicles within the next five to ten years. The uptake of electric vehicles in the future will have significant implications for future transport planning. Considerations for Kwinana include requirements for electric vehicle charging facilities in new developments, and decreased requirements for public and private car parking spaces as the car share economy grows.

In the long term, driverless shuttles are likely to be able to support residential connections to train stations, maybe replacing residential coverage bus services to train stations. This is true even where low density residential development is retained.

## Undergrounding Overhead Power Lines

The City will liaise with Western Power to prioritise the undergrounding of overhead power lines to ensure Kwinana's valued tree lined streets are protected and to improve the character of Kwinana's suburbs.

## Strategic Direction

To create a well-connected community with a safe, high-quality, integrated, multi-modal transport network that makes it easy, safe and convenient for our community to access activity centres, schools, services, recreation and entertainment facilities.

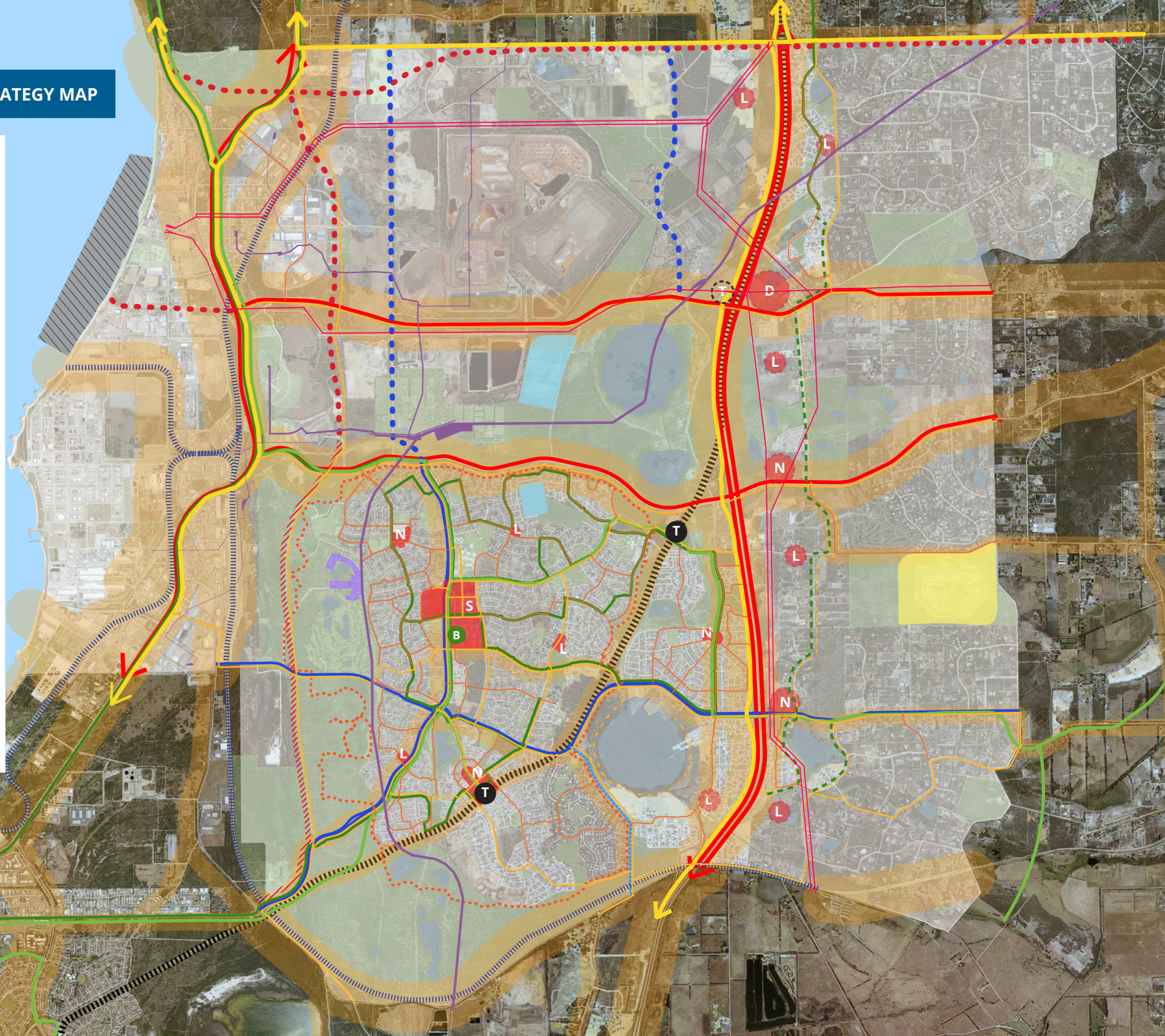
## Strategic Actions

64. Create a safe network of shaded cycle routes, walking paths and recreational trails in accordance with the City of Kwinana's *Bike and Walk Plan* that connect people with activity centres, schools, services, recreation and entertainment facilities.
65. Facilitate the integration of the *Bike and Walk Plan* into precinct structure plans recommended.
66. Investigate the potential for a high frequency bus route between Kwinana Train Station and Wellard Train Station via the Kwinana City Centre.
67. Advocate for the Public Transport Authority to improve public transport services including bus sizes, scheduling (late night) and route planning.
68. Review planning requirements within the City of Kwinana's *Local Planning Scheme* for onsite car parking and end of trip facilities for the purpose of:
  - supporting a transition to more sustainable forms of transport; and
  - ensuring new commercial and mixed use developments make alternative private (electric vehicles) and active travel modes (cycling/walking) a more attractive travel option.
69. Implement the *Parking Strategy* for the Kwinana City Centre which prioritises active travel modes and supports economic activity and/or development.
70. Protect Strategic Freight Routes from sensitive and incompatible land uses whilst mitigating the impacts of freight movements on the community, local roads and the environment.
71. Explore opportunities to provide a train station at the existing 'Railway' reserve in Mandogalup as shown in the Metropolitan Region Scheme.
72. The City of Kwinana to consider including requirements for electric vehicle charging facilities in new developments, and decreased requirements for public and private car parking spaces as the car share economy grows.
73. Continue the cooperative arrangements between the State Government, Western Power and the City of Kwinana to progressively replace the overhead electricity distribution network in residential areas with underground power and support the development of a new approach to identifying and prioritising areas for investment in underground power, particularly in areas with a high electricity network need and lower economic capacity. The approach should also ensure Kwinana's valued tree lined streets are protected and improve the character of the Kwinana's residential areas.
74. Manage land use and development through local planning controls to minimise conflict between infrastructure and sensitive land uses.
75. Maintain efficient functioning of Kwinana's freight routes by ensuring there is effective access control via the use of service roads or other measures.

# INFRASTRUCTURE STRATEGY MAP

## KEY

- Proposed Primary Roads
- //// Fremantle-Rockingham Primary Regional Road Investigation
- Proposed Secondary Roads
- Primary Roads
- Secondary Roads
- Kwinana Loop Trail
- Primary Cycle Routes
- Secondary Cycle Routes
- Local Cycle Routes
- ▬▬▬▬▬▬ Commuter Rail
- T Train Station
- B Bus Station
- Local Bus Routes
- ▬▬▬▬▬▬ Freight rail
- Water Treatment
- Casuarina Prison
- Regional Bus Route
- Westport
- DBNGP Easements
- High Voltage Overhead Transmission Lines
- SPP 5.4 Quiet House Design





## Infrastructure – Implementation Table

Strategic Direction	Actions	Time *	Lead	
<b>INFRASTRUCTURE</b>				
<b>Transport</b>				
To create a well-connected community with a safe, high-quality, integrated, multi-modal transport network that makes it easy, safe and convenient for our community to access activity centres, schools, services, recreation and entertainment facilities.	63	Create a safe network of shaded cycle routes, walking paths and recreational trails in accordance with the City of Kwinana's Bike and Walk Plan that connect people with activity centres, schools, services, recreation and entertainment facilities.	Ongoing	City Development and Sustainability & City Infrastructure
	64	Facilitate the integration of the City of Kwinana's Bike and Walk Plan into precinct structure plans.	Ongoing	City Development and Sustainability
	65	Investigate the potential for a high frequency bus route between Kwinana Train Station and Wellard Train Station via the Kwinana City Centre.	Medium	City Development and Sustainability
	66	Advocate for the Public Transport Authority to improve public transport services including bus sizes, scheduling (late night) and route planning.	Ongoing	City Development and Sustainability
	67	Review planning requirements within the City of Kwinana's Local Planning Scheme for onsite car parking and end of trip facilities for the purpose of: <ul style="list-style-type: none"> <li>supporting a transition to more sustainable forms of transport; and</li> <li>ensuring new commercial and mixed use developments make alternative private (e.g. sustainable / electric) and active travel modes (cycling / walking) a more attractive travel option.</li> </ul>	Medium	City Development and Sustainability
	68	Implement the Parking Strategy for the Kwinana City Centre which prioritises active travel modes and supports economic activity and/or development.	Medium	City Development and Sustainability
	69	Protect Strategic Freight Routes from sensitive and incompatible land uses whilst mitigating the impacts of freight movements on the community, local roads and the environment.	Ongoing	City Development and Sustainability
	70	Explore opportunities to provide a train station at the existing 'Railway' reserve in Mandogalup shown in the Metropolitan Region Scheme.	Medium	City Development and Sustainability

### (Continued)

To create a well-connected community with a safe, high-quality, integrated, multi-modal transport network that makes it easy, safe and convenient for our community to access activity centres, schools, services, recreation and entertainment facilities.

71	Advocate to Main Roads Western Australia to realign the Fremantle Rockingham Primary Regional Road to protect significant environmental values and maintain the green belt as a buffer between residential areas and industrial development.	Short	
72	The City of Kwinana to consider including requirements for electric vehicle charging facilities in new developments, and decreased requirements for public and private car parking spaces as the car share economy grows.	Medium	City Development and Sustainability
73	Continue the cooperative arrangements between the State Government, Western Power and the City of Kwinana to progressively replace the overhead electricity distribution network in residential areas with underground power and support the development of a new approach to identifying and prioritising areas for investment in underground power, particularly in areas with a high electricity network need and lower economic capacity. The approach should also ensure Kwinana's valued tree lined streets are protected and improve the character of the Kwinana's residential areas.	Medium	City Development and Sustainability & City Infrastructure
74	Manage land use and development through local planning controls to minimise conflict between infrastructure and sensitive land uses.	Ongoing	City Development and Sustainability
75	Maintain efficient functioning of Kwinana's freight routes by ensuring there is effective access control via the use of service roads or other measures.	Ongoing	City Development and Sustainability

## 9. IMPLEMENTATION

The Local Planning Strategy will need to respond to future changes in State and regional policy as well as any changes to local circumstances and priorities. This may be done through occasional updates as well as periodic comprehensive review.

A comprehensive review of the Local Planning Strategy should be undertaken at least every five years in conjunction with a scheme review. It is recommended that background information be updated on a more frequent basis in response to the availability of information or changes which may not have been foreseen at the time of formulating the original Strategy.

Modifications to the Local Planning Strategy should be undertaken in response to significant changes in circumstances in the period between comprehensive reviews, for example, in the event of major economic development or major infrastructure projects which were not addressed in the endorsed Local Planning Strategy.

The procedure for review or amendment of the Local Planning Strategy is generally the same as that for the preparation of the Local Planning Strategy except that, in the case of an amendment, it is only the changes that are subject to advertisement, assessment and endorsement by the Western Australian Planning Commission.

The focus of monitoring and review should be on the extent to which the Local Planning Strategy has been successful in:

- articulating and achieving the planning objectives of state and local government; and
- effectively guiding land use and development decisions to achieve the outcomes of the Local Planning Strategy.

The City will also report to Elected Members annually on the progress of implementing the actions listed in the Local Planning Strategy.



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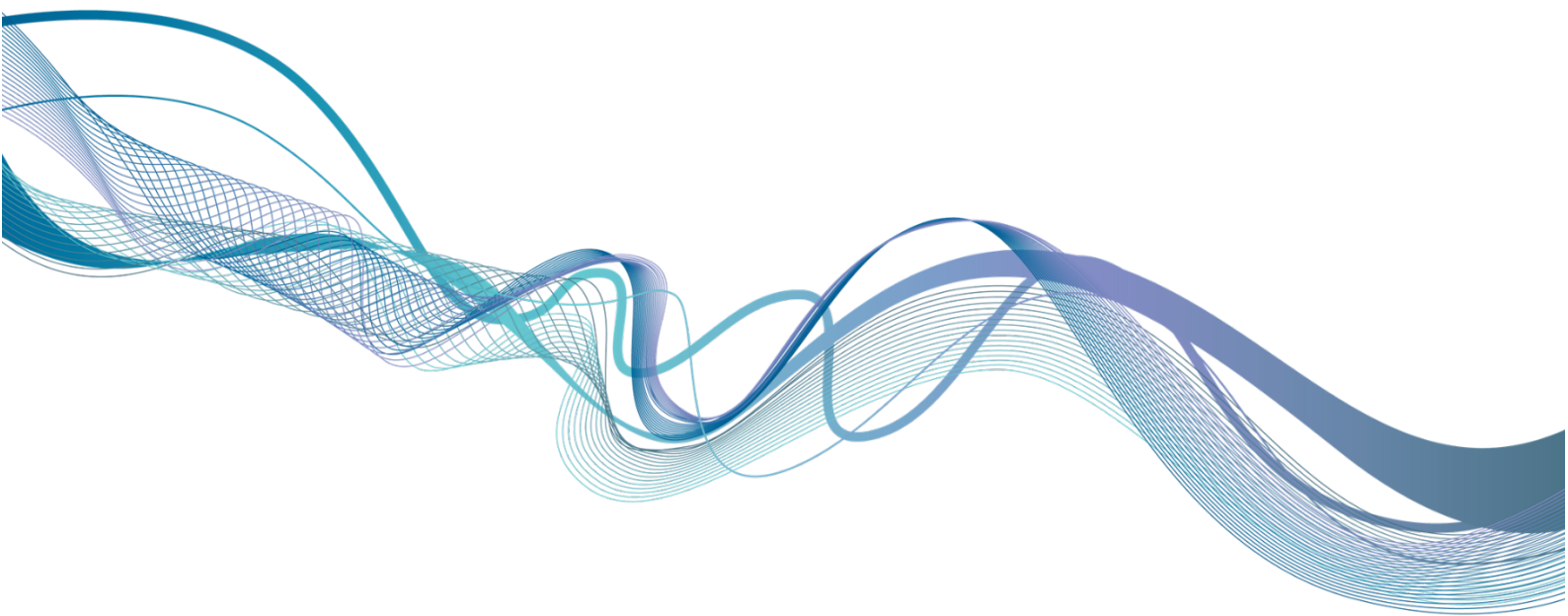
**ATTACHMENT B -  
DRAFT LOCAL PLANNING STRATEGY -  
PART 1 - STRATEGY**



**Our sustainable City**

**DRAFT  
LOCAL PLANNING STRATEGY**

**PART 2 - BACKGROUND AND ANALYSIS**



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## 1.0 Introduction



The City of Kwinana is required by the Planning and Development (Local Planning Schemes) Regulations 2015 (the Regulations) to prepare a Local Planning Strategy for each local planning scheme within its district.

The Regulations state that a Local Planning Strategy must:

- a) set out the long-term planning directions for the local government; and
- b) apply any State or regional planning policy that is relevant to the strategy; and
- c) provide the rationale for any zoning or classification of land under the local planning scheme.

The Local Planning Strategy provides strategic planning direction for the City's development and provides the framework for decisions on future proposals for rezoning, subdivision and development in the City through to 2036 and beyond.

The Local Planning Strategy provides the strategic basis for the City's local planning framework, primarily through the implementation of a Local Planning Scheme. A Scheme provides the basis for decision



making about development and the use of land and is the principle mechanism for implementing the Local Planning Strategy.

Whilst primarily a spatial planning strategy, the Local Planning Strategy addresses social, environmental economic, and infrastructure factors that affect, and are affected by, land use and development. For this reason, the Local Planning Strategy will also provide a reference for Council in relation to many of its other corporate functions, helping to identify priorities for investment in infrastructure such as community facilities and services, economic development initiatives, environmental management activities and community building. Similarly, other organisations will refer to the Local Planning Strategy for guidance on their own activities within the City.

The Local Planning Strategy will be reviewed every five years, as required by the Regulations. However, should changes in circumstances necessitate it, the Local Planning Strategy may be reviewed or amended sooner.

## 2.0 Community engagement



The views of the City's community have helped inform the development of the City's response to the key issues outlined in this Strategy.

As part of the preparation of this Strategy, the City prepared Planning Studies on each key theme of the Local Planning Strategy, being:

- **Kwinana Housing Study (Housing and Population):** The focus of this study was recognising the land-use impacts of the City's evolving demographics and population growth. This study identified changing residential and lifestyle requirements in the community.
- **Integrated Land use and Transport Study (Transport):** This examined the City's connectivity, focussing on integrated transport accessibility (all modes), vehicle traffic, community travel routes and destinations and parking requirements of the community now and in the future.
- **Employment and Economic Development Analysis (Economy):** This study focused on analysing the local economy and identified the relationship between population and industry, employment deficits and opportunities for economic growth and additional employment.

- Local Biodiversity Study (Environment): This study examined the extent and protection of the City's natural bushland and wetland biodiversity.
- Rural Lands Study (Rural): The purpose of this study was to examine the stock of rural lands and identify competing contemporary land-use and environmental challenges.

Short, concise Discussion Papers were prepared for each of the above topics with key issues highlighted for consideration by the community which was then presented for community feedback through various engagement exercises.

Engagement with the community and stakeholder groups included:

1. Making available draft Planning Studies and its related Discussion Papers on the City's LPS webpage, including printed copies of the Discussion Papers.
2. A Survey Questionnaire prepared and made available at the City's Administration Office, Library, and electronically on the City's webpage and Facebook page. The Survey Questionnaire focused on the key topics raised in the above studies as well as raising some broader questions about the communities planning vision for Kwinana. The survey was available to be completed by stakeholders in two forms, 'online' via survey monkey or as a hard copy.
3. Community Workshops and Focused Sessions
  - Four workshops for the general community held at a variety of locations across the City at differing times so as to allow interested persons with an opportunity to attend;
  - Two information sessions for the Stakeholder Groups;
  - A Focus session with an Aboriginal Community Group;
  - A Focus Session with a Youth Group; and
  - A Focus Session with the City's Senior Management Team.
  - A "Getting To Know Your Community Event" held at the Market Place on 22 May 2019.

## 2.1 Community engagement findings

A summary of findings from the engagement program is provided below and these have been used to inform the City's response to key issues throughout the Local Planning Strategy.

### 2.1.1 Housing

A common value that emerged from the engagement program was a general concern for the natural environment and aversion to the mass clearing of bushland to accommodate housing. The development of new estates have been criticised for the demolition of natural habitats, lacking innovative design and sustainable outcomes, contributing to the sprawl of Perth. The retention of significant trees as part of any new development, in older and newer suburbs a majority view.

The community reflected that they would support increased densities in locations that would generate and allow for increased activity, for example, around activity centres and major transport destinations. Many comments were conditionally supportive of high-density dwellings but that such development should be well designed.

The following design related comments were raised; encouraging safer environments, ensuring surrounding amenity is improved and not reduced, sustainable design, high quality and aesthetic design.

The requirement for a greater diversity of housing stock was also a common theme. Larger lots are valued in the City, it is important to retain large houses for multi-cultural, multi-generational and larger families. However, it was commonly identified that the City does not have enough housing stock to accommodate an ageing population. Sustainable, affordable, efficient housing options are valued.

### 2.1.2 Transport

The availability of public transport was the most common transport concern of participants, particularly east of the freeway.

The results also identify that public transport frequency, availability, routes and destinations are currently not meeting desires of the community.

A majority of respondents stated that they would like to use more integrated (public and active) transport modes to travel but they did not currently and that the lack of infrastructure and road design was the biggest barrier to users, along with a perception of safety and cleanliness of paths.

### 2.1.3 Economy

A strong theme that emerged was the desire within the community for a greater variety of local, small businesses in the City. The majority of participants viewed this as their highest priority for this topic. Generally, the community feel that there is a greater need for retail, commercial, entertainment and 'white collar' jobs, with a lesser focus on increasing industrial land based employment.

A significant concern was raised regarding the need for more activity such as restaurant's, businesses, cafes and entertainment in the City Centre. It was considered that the Kwinana City Centre should be more attractive and appealing to local residents. There were suggestions for night markets during summer and pop up food business in the City Centre and at parks and reserves.

In the Youth and Aboriginal Focused Sessions, there was seen to be the need for stronger links between local employers and local people. At both the workshops and focused group sessions, the need to create close synergies between job creation and education/training facilities was raised.

At the workshops, there was support for bulky goods precinct at strategic locations, like the freeway interchanges and higher density, mixed use development in Activity Centres. Suggestions were also made about attracting secondary industry based initiatives and jobs to support current/existing industries.

#### 2.1.4 Biodiversity

The engagement program revealed overwhelming support for environmental assets and initiatives within the community. It was clear that the community feels very passionately about the protection of remaining bush. Many community members believe that the City should prioritise environmental biodiversity and sustainability. Many residents stated that they chose to live in Kwinana because of the amount of untouched bushland that was available.

Overall, the community consensus is that the City should take a firmer regulatory approach to protecting the City's natural environment. There was a popular view that the City should also engage and educate the community more about the significance of the biodiversity in the region and provide incentives to community members to improve the environmental biodiversity of the City.

A common idea raised amongst residents was to not clear any more bushland to accommodate development. Many residents identified that there was 'enough cleared land' in the Metropolitan Region to accommodate residential growth. Many residents felt that they would rather increase infill and support denser development in appropriate locations to accommodate residential growth and reduce clearing of bush.

#### 2.1.5 Rural Land

There was concern expressed about land use and management, particularly over grazing and illegal dumping within rural areas.

Support was provided to retention of the City's rural living areas and the protection of bushland. There was a view that the City should avoid over regulation of rural areas and keep flexibility around home business.

Buffers between residential areas and rural land uses were seen as important. In respect to bush fire risk, the City could offer a service but also apply enforcement effectively.

It was also noted that a number of those attendees at the Community Workshops had a view that the proposed Planning Investigation Area identified by the Western Australian Planning Commission, in the Sub-regional Framework, should be retained as rural living.

### 3.0 State Planning Framework



The State and Regional Planning framework sets out the hierarchy of state and regional planning strategies, schemes, plans and policies which apply to development and land use in Western Australia.

Figure 1: State Planning Framework shows the hierarchy of the various planning mechanisms in operation and this Local Planning Strategy has been prepared in context of the state planning framework.

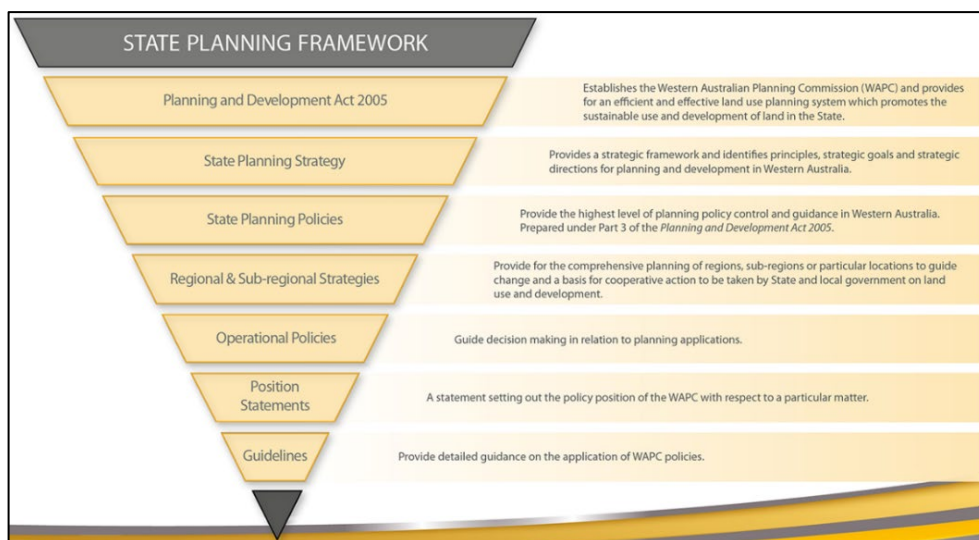


Figure 1: State Planning Framework<sup>1</sup>

<sup>1</sup> Western Australian Planning Commission, 2017, State Planning Framework

## 3.1 Legislation

### 3.1.1 Planning and Development Act 2005

The *Planning and Development Act 2005* (the Planning Act) is the primary piece of legislation governing development and subdivision in Western Australia. The purpose of the Planning Act is to provide for an efficient and effective land use planning system and to promote sustainable use and development of land. The Planning Act came into force in April 2006, consolidating three pieces of legislation:

- Town Planning and Development Act 1928;
- Metropolitan Region Town Planning Scheme Act 1959; and
- Western Australian Planning Commission Act 1985.

The Planning Act provides the head of power to a local authority to prepare a local planning scheme with the general objective of making suitable provision for the improvement, development and use of land in the local planning scheme area.

### 3.1.2 Planning and Development (Local Planning Schemes) Regulations 2015

The Regulations 2015 is a major part of Western Australia's planning reform agenda, affecting arrangements for local planning strategies, schemes and amendments. In addition to a new Model Scheme Text, the Regulations 2015 introduced a set of deemed provisions that now form part of every local planning scheme in the State. The Regulations state that a Local Planning Strategy must:

- a) set out the long-term planning directions for the local government;
- b) apply any State or regional planning policy that is relevant to the strategy; and
- c) provide the rationale for any zoning or classification of land under the local planning scheme.

Given the significant modifications to the State planning system there is a need to comprehensively review the City's local planning framework to align it with the current legislation.

### 3.1.3 Hope Valley-Wattleup Redevelopment Act 2000

The *Hope Valley-Wattleup Redevelopment Act 2000* (HVWR Act) functions to plan, undertake, promote and coordinate the development and redevelopment of land in the Latitude 32 area. As required by the HVWR Act, a Master Plan (Hope Valley Wattleup Redevelopment Project Master Plan) was approved by the Western Australian Planning Commission (WAPC) in 2004.

Under the Act and Master Plan the WAPC is the determining authority for planning. In early 2012, the WAPC delegated the authority back to the City of Kwinana and City of Cockburn. The Local Government responsibility in Latitude 32 is therefore planning and compliance and the WAPC is responsible for structure plan and subdivision approval.

## 3.2 State and Regional Strategies, Schemes & Plans

### 3.2.1 State Planning Strategy 2050

The State Planning Strategy 2050, aims to guide the sustainable development of Western Australia (WA) for the next four decades, is a collaborative whole-of-government approach to land use planning and development. It considers that the South West Sector, in which the City is a part, will continue to house the majority of the State's population and have the highest level and greatest range of health and education services, cultural activities and employment.

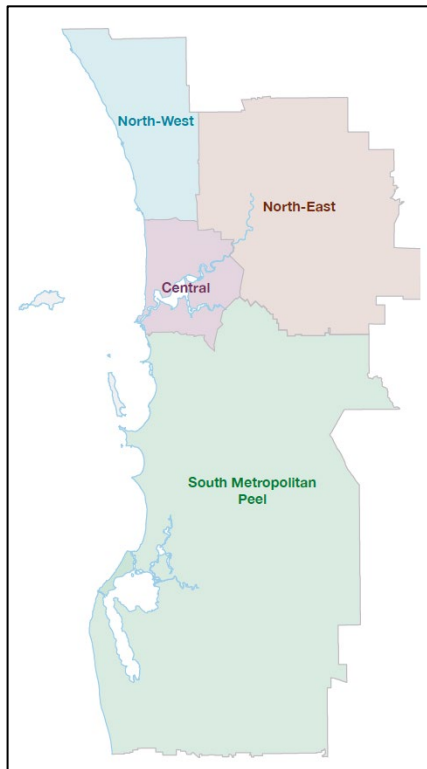
The strategy articulates a vision for 'sustained growth and prosperity' for WA, and envisages a future for its people where the present and future generations enjoy high standards of living, improved public health and an excellent quality of life.

### 3.2.2 Perth and Peel @3.5 Million

The overall spatial framework for Perth, the Perth and Peel@3.5 million suite of documents, is guided by the 'Connected City' principles, involving a balance between urban infill and fringe development. As a connected city, Perth will have:

- a strong central business district that is the business, finance, service, retail, cultural and entertainment centre of the State;
- a network of connected activity centres which deliver employment, entertainment and higher-density lifestyle choices. These centres will be designed to be attractive, accessible, compact, vibrant, pedestrian and cycling-friendly environments that have high-quality public transport and road linkages;
- connected, specialised hubs of innovation, education, health, research and technology which are designed and located to enable access to skilled labour and appropriate infrastructure;
- urban areas that deliver a range of contemporary lifestyle choices from low density suburban, to medium-density urban and high-density inner-city lifestyles in strategically-located areas;
- high-quality global and local infrastructure networks including roads, public transport, energy, water, communication and globally competitive, highly accessible airport and sea port infrastructure;
- urban form that maximises the use of existing infrastructure assets in parallel with extending infrastructure into the development areas of the outer sub-regions identified in the relevant draft sub-regional planning frameworks; and
- integrated land and water management that provides social amenity, environmental protection and resilience to climate change for a more liveable city into the future, through better urban water management, water sensitive urban design and green infrastructure.





Perth and Peel@3.5million outlines the key planning principles and influencing factors (growth patterns, people and society, economy, the urban environment and environment and landscape) that underpin the spatial plan for Perth and Peel.

The spatial plan for Perth has been developed to deliver a more consolidated urban form and achieve a more connected urban structure that minimises environmental impacts. The overarching document provides a link across four sub-regional planning frameworks, Metropolitan North-West, North-East, Central and South metropolitan sectors, of which the City is within the South Metropolitan Peel sub-region.

A key implementation tool developed from Perth and Peel@3.5million is the introduction of a target that 47% of development occurs as infill development rather than greenfield development.

The City will need to respond to the directions set in Perth and Peel@3.5million which are specified under the applicable sub-regional frameworks.

Figure 2: Sub-regional boundaries<sup>2</sup>

### 3.2.3 The South Metropolitan Peel Sub-regional Planning Framework

The South Metropolitan Peel Sub-regional Planning Framework (the Sub-regional Framework) is a subset of the over-arching Perth and Peel @3.5million regional strategy, the sub-regional areas being shown in Figure 2: Sub-regional boundaries, and provides strategic guidance for the future development of the south metropolitan and peel areas.

The Sub-regional Framework considers where future homes and jobs will need to be located; what community and social infrastructure will be required; better integrated use of existing infrastructure; protection of important environmental assets and critical services; and staging and sequencing of future development.

Specific to the City, the Sub-regional Framework identifies, as shown on Figure 3: Perth & Peel@3.5 million sub-regional framework spatial plan, the following matters that will need to be addressed:

- a port installation development for a future port facility at Kwinana;
- a planning Investigation area over land in the south-east of the City (effecting the Rural zone in Casuarina);
- industrial expansion areas through Hope-Valley, Mandogalup and Postans;
- an industrial investigation area in Mandogalup (now IP47);
- an urban infill dwelling target of 1,370 by 2050;
- Kwinana City Centre as a Secondary Centre;
- a High Priority Transit Corridor “Fremantle-Cockburn Coast-Kwinana-Rockingham” and
- proposed regional transport links at both primary and secondary levels.

<sup>2</sup> WAPC, 2018, Perth and Peel@3.5million

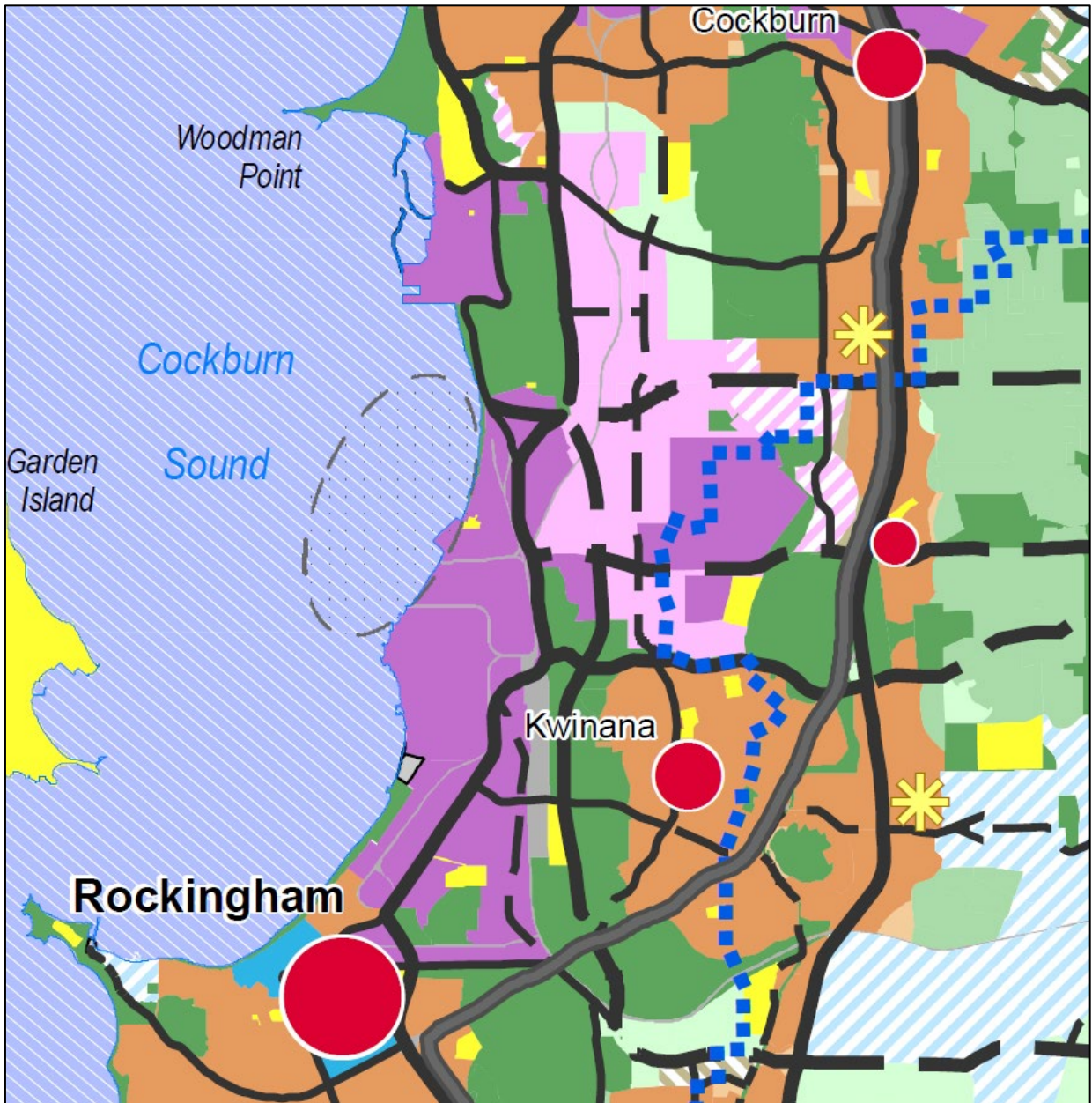


Figure 3: Perth & Peel@3.5 million sub-regional framework spatial plan

3.2.4 Metropolitan Region Scheme

The Metropolitan Region Scheme (MRS) is an overarching planning scheme for land use in the Perth metropolitan area. MRS defines the future use of land and provides the legal basis for planning in the Perth metropolitan region, dividing it into broad zones and reservations. Local Planning Schemes must be consistent with the MRS and are required to provide detailed plans for their part of the region.

It is noted that the proposed Fremantle-Rockingham Highway Primary Regional Road runs through Bush forever site 349 as shown on Figure 4: Map of Kwinana as shown in the Metropolitan Region Scheme.

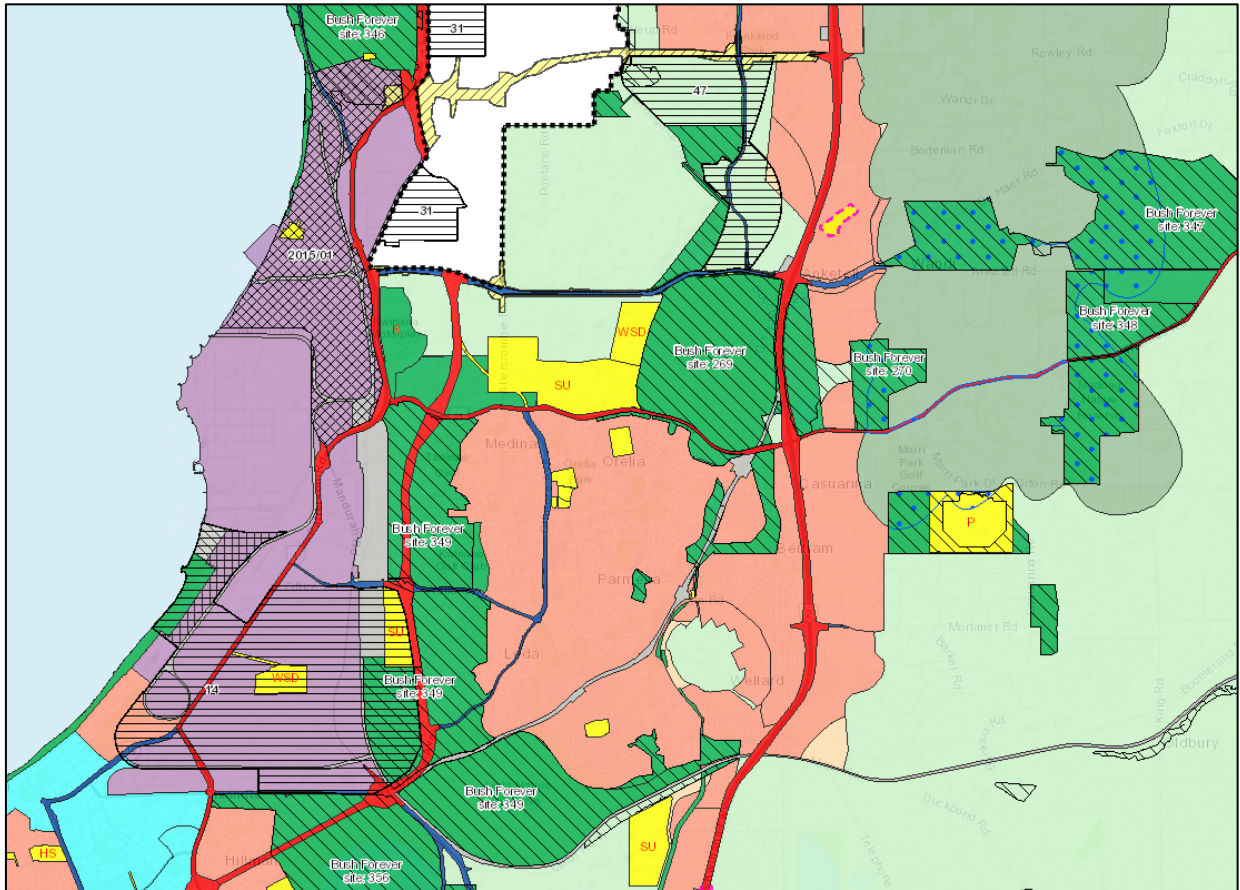


Figure 4: Map of Kwinana as shown in the Metropolitan Region Scheme

3.2.5 Westport Port and Environs Strategy

The Westport Port and Environs Strategy relates to the provision of infrastructure for the freight and logistics needs of Perth and the South West over the next 50-100 years. The Strategy will relate to the future of the Outer Harbour in Kwinana and its connection to the broader freight network, providing for efficient connections between port facilities.

Based on the combined results of a second multi-criteria analysis (MCA-2) and the rapid cost-benefit analysis, the port options that best meet WA’s future needs are a shared Fremantle/Kwinana operating model, eventually transitioning to Kwinana over time (Option D2) and moving all freight to Kwinana in one step (Option B).<sup>3</sup>

<sup>3</sup> Westport Taskforce Project Office, 2020, WESTPORT Future Port Recommendations - Stage 2 Report - May 2020



Figure 5: Artist's impression of the future container port in Kwinana

The announcement of a land backed port facility at Kwinana has significant opportunities and planning implications for the City, in particular:

- the impacts on existing properties and industries in Kwinana;
- the availability of land in Kwinana to support the operations of the port and port servicing industries (including customs, truck marshalling areas and empty container parks);
- the road and rail freight supply chain;
- potential for new intermodal facilities within Latitude 32; and
- the impact on environmental assets.

### 3.2.6 Hope Valley-Wattleup Redevelopment Project Master Plan

The Hope Valley-Wattleup Redevelopment Project Master Plan was prepared under the HVWR Act and was approved by the Western Australian Planning Commission (WAPC) in 2004 and continues to operate whilst the Latitude 32 development area remains under development.

The Master Plan functions in a manner similar to a local planning scheme. The Master Plan provides the procedures for establishing statutory documents including structure plans, planning policies and design guidelines, along with the requirements for planning approvals in order to control land use and development.

Under the HVWR Act and Master Plan, WAPC is the planning authority. In 2012, WAPC delegated the authority to the City with respect to planning and compliance, and the WAPC is responsible for structure plan and subdivision approval.

### 3.2.7 Improvement Plan 47 – Mandogalup

Improvement Plan 47 – Mandogalup (IP47) came into effect when it was gazetted on 12 April 2019. Specifically, the purpose of IP47 is to:

- establish the strategic planning and development intent with the improvement plan area;
- provide for a planning framework;
- authorise the preparation of an improvement scheme; and

- provide objectives to guide the preparation of the improvement scheme, statutory plans, statutory referral documentation and policy instruments.

An improvement scheme makes provision for the improvement, development and use of land for a range of matters, in the same way as a local planning scheme is required to by the Planning Act. The improvement scheme is still in development and is not yet in effect. A number of land use scenarios are currently being proposed each of which will greatly impact on the adjoining land use context.

Once gazetted, the improvement scheme will replace the Metropolitan Region Scheme and the City of Kwinana s Local Planning Scheme No. 2. The WAPC will be responsible for determining land use planning matters in the improvement scheme area.

### 3.2.8 Jandakot District Structure Plan 2007

The Jandakot Structure Plan provided a guide to the future development and management of key environmental issues, primarily by providing key strategic planning direction for the rezoning of rural to urban deferred land for future subdivision and development within the wider Jandakot area. It has been relied upon for planning within this area since 2007, but is now largely superseded by the Perth and Peel Sub-regional Frameworks.

### 3.2.9 Key issues

- A new outer harbour will be located at Kwinana which will impact on the configuration of the Kwinana industrial area.
- Anketell Road will become the primary freight route for the Port, with Rowley Road and Thomas Road also becoming increasingly critical to the Port’s long term operations.
- New industrial and service commercial land uses may seek to locate near Port activities and require suitable land for their operations.
- Existing land uses within Kwinana may be displaced due to the development of a outer harbour and require alternative suitable locations.
- Hope Valley-Wattleup Redevelopment Act area is anticipated to continue to provide industrial development land.
- Planning for the IP47 area continues and its role to buffer future industrial activities from the urban development front is increased.
- Planning investigation area, in the south-east of the City (Casuarina), may change the land use and development intentions for that locality.
- The City has been allocated an urban infill dwelling target of 1,370, to be achieved by 2050.
- A High Priority Transit Corridor “Fremantle-Cockburn Coast-Kwinana-Rockingham” provides increased connectivity throughout the region.

### 3.2.10 Response to issues

The City supports the development of outer harbour and the associated infrastructure required for its operations but is keen to ensure the Kwinana community can benefit from this major infrastructure development.

The City will seek to ensure compatible development near and along freight routes so that future land use conflicts between road noise and sensitive uses are minimised.

Additional industrial land for existing businesses who are impacted by the outer harbour and future businesses seeking to locate in the area will require additional appropriately zoned land and this should occur throughout the already identified industrial expansion areas in Hope Valley-Wattleup, Mandogalup. It is important to note that the industrial expansion will need to be protected from sensitive land uses and the urban front through a transition and buffer.

The City, and the community as identified through engagement exercises, values its rural living environment. Any planning investigation for the Casuarina area will need to take into account the need to continue to provide this housing option.

The City will focus on delivering its infill target through zoning and land use controls that provides quality diverse housing in areas that are close to activity centres and transport nodes.

### 3.3 State Planning Policies

#### 3.3.1 SPP 1.0 State Planning Framework

SPP 1.0 is the overarching State Planning Policy that specifies general principles for land use planning and development and identifies the primary aim of planning to provide for the sustainable use and development of land. It unites existing State and regional policies, strategies and guidelines within a central framework, which provides a context for decision-making on land use and development in WA. SPP 1.0 identifies and expands on six general principles to guide land use and development: community; economy; environment; infrastructure; regional development and governance.

#### 3.3.2 SPP 2.0 Environment and Natural Resources

SPP 2.0 defines the principles and considerations that represent good and responsible planning in terms of environment and natural resource issues, which include water resources, air quality, soil and land quality, biodiversity, agricultural land and rangelands, minerals, petroleum and basic raw materials, marine resources and aquaculture, landscapes, and greenhouse gas emissions and energy efficiency.

Some of the major policy measures include:

- Avoiding development that has the potential to cause unacceptable environmental damage;
- Protecting significant natural, indigenous and cultural features; and
- Supporting conservation, protection and management of native remnant vegetation where possible, to enhance soil and land quality, water quality, biodiversity, fauna habitat, landscape, amenity values and ecosystem function.

This policy informs the Local Planning Strategy by supporting sustainable development that minimises the impact on the environment. The main implications for the City are the protection of wetland and significant natural bushland environments through the recognition of reserves, open space and limiting intensification of development within identified natural asset areas.

#### 3.3.3 SPP 2.1 The Peel-Harvey Coastal Plain Catchment

The objectives of this Policy are to improve the social, economic, ecological, aesthetic, and recreational potential of the Peel-Harvey Coastal Plain Catchment, ensure that changes to land use are controlled to avoid and minimise environmental damage to the Peel-Harvey Estuarine system and increase high water-using vegetation cover. The retention and rehabilitation of existing remnant vegetation is also encouraged, with a target of 50% of land area to be established with deep rooted perennial plants, preferably local indigenous species, and prevent land uses likely to result in excessive nutrient export into the drainage system.

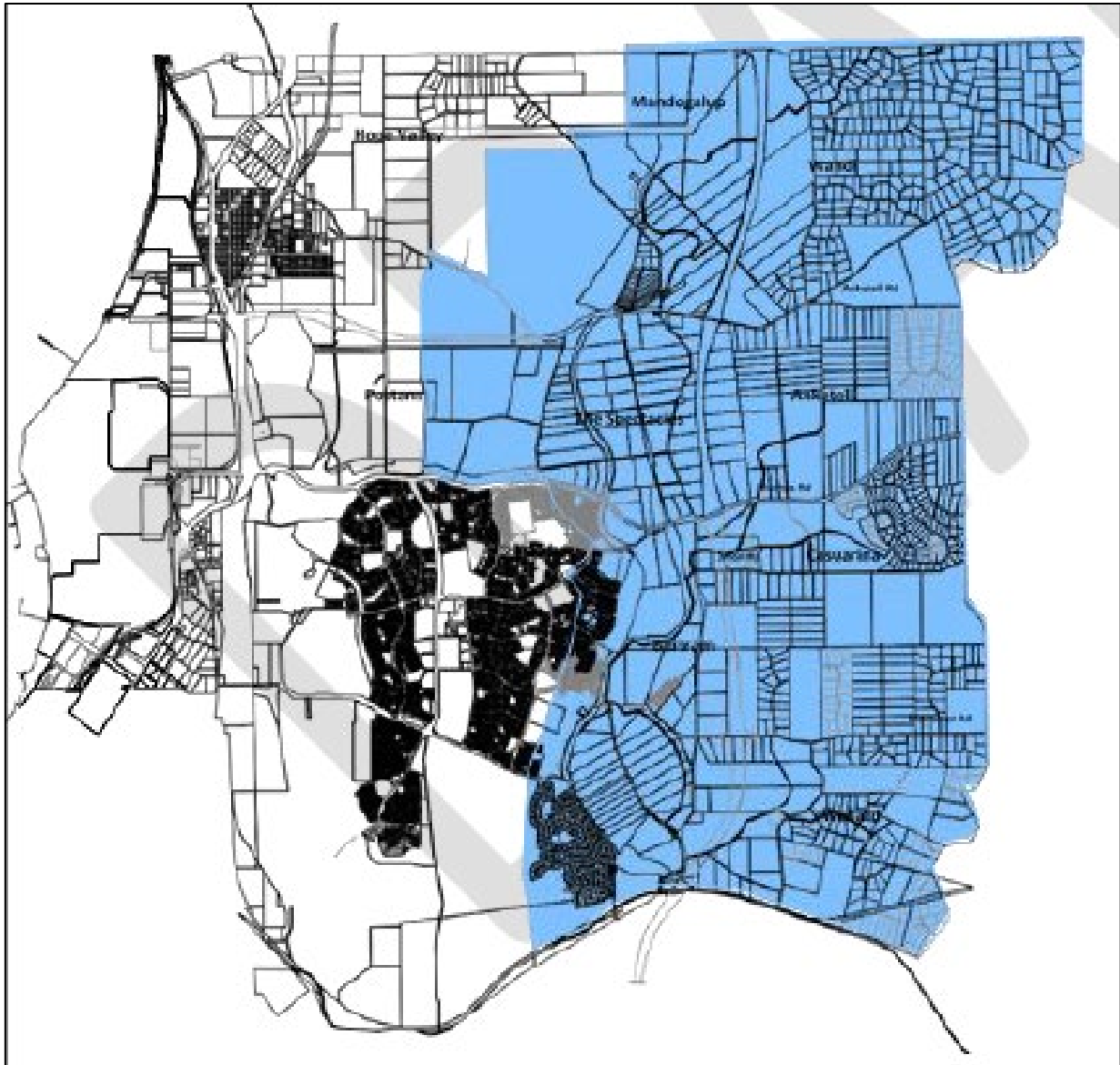


Figure 6: Map of Peel Harvey Catchment

All of the eastern half of the City and significant central areas are within the Peel Harvey catchment. These areas are largely comprised of Bassendean Sands and therefore are not suitable for intensive agriculture. Rehabilitation of the area with deep rooted plants should be encouraged throughout these areas.



### 3.3.4 SPP 2.3 Jandakot Groundwater Protection

A key objective of this policy is to ensure that all development and changes to land use within the policy area are compatible with maximising the long-term protection and management of groundwater, in particular for public drinking water supply. Another objective aims to prevent, minimise, and manage development and land uses that may result in contamination of groundwater.



Figure 7: Map of Jandakot Water Mound

The policy limits lots sizes within the policy area to a minimum of 2 hectares, and outlines development control provisions applicable within the policy area, including when advice from the Department of Water is required.

A large portion of the City's rural living areas, including Wandi and Anketell are within the groundwater protection mound and subject to the above requirements.

### 3.3.5 SPP 2.4 Basic Raw Materials

A key objective of SPP 2.4 is to enable the responsible extraction of basic raw materials used in building and development while ensuring the protection of people and the environment.

This policy sets out the matters which are to be taken into account and given effect the WAPC and local governments in considering zoning, subdivision and development applications for extractive industries (for the extraction of basic raw materials) in the vicinity of identified basic raw material resource areas.

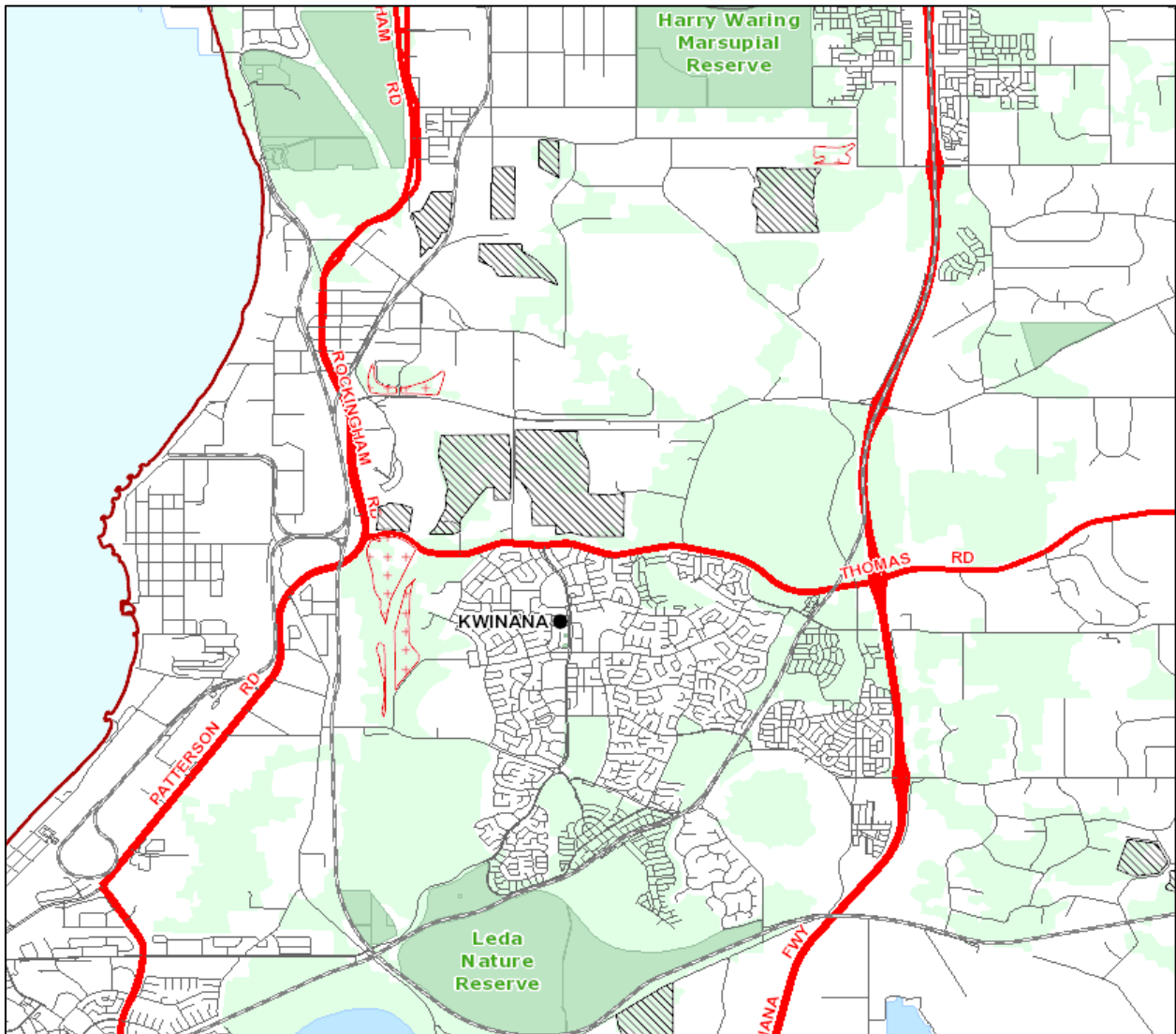


Figure 8: Mapped Basic Raw Material extraction and exclusion sites<sup>4</sup>

There are no significant geological supplies located with the City, however, there are nine extraction sites and four exclusion sites as shown in Figure 8: Mapped Basic Raw Material extraction and exclusion sites. These sites are on either Crown land, Reserved under the MRS, or within existing rural or industrial areas. Any future zoning of land for these sites will need to ensure sensitive land uses do not encroach these locations.

### 3.3.6 SPP 2.5 Rural Planning

The primary objective of this policy is to protect and preserve Western Australia’s rural land assets due to the importance of their economic, natural resource, food production, environmental and landscape values. Ensuring broad compatibility between land uses is essential to delivering this outcome.

There are 3 locations within the City shown as rural land on Figure 3: Perth & Peel@3.5 million sub-regional framework spatial plan:

1. 34 Marri Park Drive - Marri Park golf course
2. Lot 8022 Lyon Road, Wandi – Wetland Reserve 51580
3. 24 McDowell Lane, The Spectacles – Land directly adjacent The Spectacles wetlands

<sup>4</sup> Department of Mines, Industry, Regulation and Safety, Geoview mapping system

None of the three locations are used for primary production purposes and are instead more appropriate to be reserved for parks and recreation under the MRS. However, the location known as 24 McDowell Lane is within a planning investigation area and as such any rezoning of that site should await the results of any future planning.

### 3.3.7 SPP 2.6 State Coastal Planning

SPP 2.6 is a higher order policy for all coastal matters. One of the main objectives of this policy is to protect, conserve and enhance coastal zone values, particularly in areas of landscape, biodiversity and ecosystem integrity, indigenous and cultural significance.

The Policy prescribes that any rezoning, structure planning, subdivision, strata subdivision or development should ensure that a coastal planning strategy or coastal foreshore management plan is prepared and implemented by the local government and/or proponent for the coastal foreshore reserve and that adequate coastal hazard risk management and adaptation planning occurs.

The City has a coastal hazard risk management and adaptation plan in place, developed in partnership with the Cockburn Sound Coastal Alliance. This strategy should have regard to this plan in considering built form along the coastline.

### 3.3.8 SPP 2.7 Public Drinking Water Source

This Policy aims to protect and manage public drinking water source areas from incompatible land uses and pollution in order to maintain the quality of the drinking water. Under this Policy, land uses that are detrimental to the quality and quantity of the water supply will not be permitted, unless it can be demonstrated that such impact can be managed. The WAPC classifies drinking water resource areas as follows:

1. Priority 1 (P1): These areas should be managed to ensure that there is no degradation of the water resource in these areas. P1 is the highest level of protection for the water source and will normally apply to land owned by the state, and is characterized by low intensity and low-risk land use, such as forestry.
2. Priority 2 (P2): These areas should be managed to ensure that there is no increased risk of pollution to the water source. P2 areas are declared over land where low-risk development already exists that may be privately owned.
3. Priority 3 (P3): These areas should be managed to reduce the risk of polluting the water source. P3 areas are declared over land where water supply sources need to co-exist with other land uses such as residential, commercial, and light industrial developments, although there is some restriction on potentially highly polluting land uses.

All land mapped as part of the Jandakot public drinking water source under Figure 7: Map of Jandakot Water Mound, is priority 1 or priority 2. The majority of land within the Jandakot Water Mound is zoned for rural living and this Strategy should seek to retain, and potentially expand, that land use.

### 3.3.9 SPP 2.8 Bushland Policy for the Perth Metropolitan Region

SPP 2.8 seeks to address the protection and management of significant bushland areas, particularly those identified for protection through strategies and processes at the regional and local level in the

Perth Metropolitan Region. Regionally significant bushland areas are known as ‘Bush Forever sites’ and are identified in this Policy.

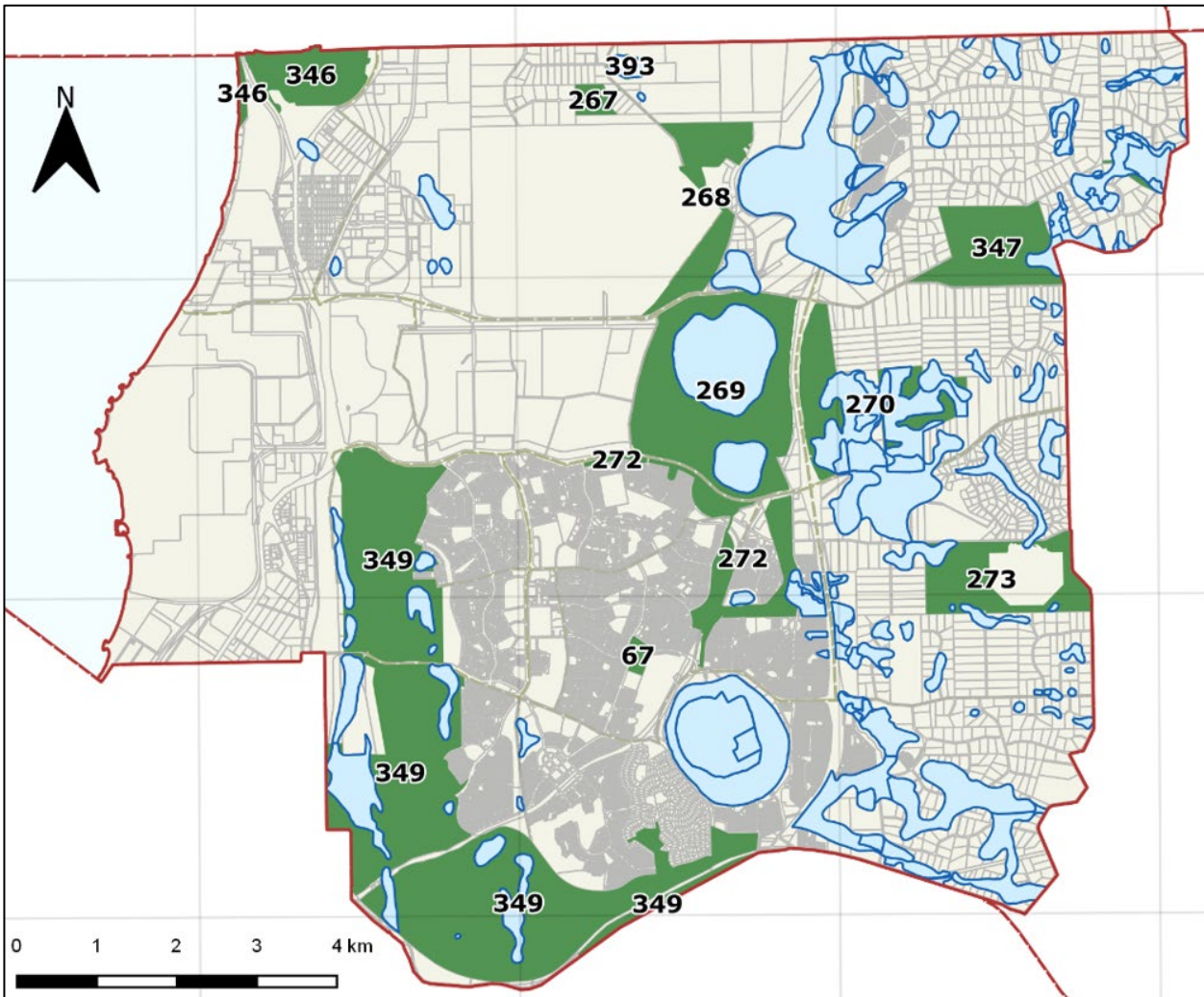


Figure 9: Map of Bush forever sites within the City of Kwinana

Bush Forever Sites	Location	Ownership	Management
67	Parmelia Avenue Bushland	WAPC	Crown Reserve vested with the City
267	Mandogalup Road Bushland, Hope Valley	WAPC	WAPC
268	Mandogalup Road bushland, Mandogalup	Commercial, Private, State Government	Part no management authority, and part Crown reserve vested with the City
269	The Spectacles Bushland	Commercial, Private, State Government	Part WAPC, part Department of Water and Environmental Regulation (DWER), part no management agency
270	Sandy Lake and adjacent Bush land, Anketell	WAPC, Commercial	Part WAPC, part DWER, part no management agency

272	Sicklemore Road Bushland, Parmelia / Casuarina	Commercial, Private, State Government	Part unallocated crown land, part drain reserve, part Crown reserve vested with the City, part no management agency
273	Casuarina Prison Bush land	State Government	Crown reserve vested in Department of Corrective Services
346 (partial)	Brownman Swamp, Mt Brown Lake and adjacent Bushland, Henderson / Naval Base	State Government, Water Corporation, WAPC	Part DBAC conservation Park, part Crown Reserve vested in Local Government, part unallocated Crown land, part no management agency (Beeliar Regional Park)
347 (partial)	Wandi Nature Reserve and adjacent Bush Land, Wandii / Oakford	WAPC, State Government	Part DBAC Nature Reserve, part WAPC (Jandakot Regional Park)
349	Leda and adjacent Bushland	Commonwealth, WAPC, State Government	Part DWER nature reserve, part Vacant Crown Land, part WAPC part Crown Reserve vested with the City, part no management agency
393	Wattleup Lake and adjacent Bush Land, Wattleup / Mandogalup	Commercial, Private, State Government	No Management Agency

Table 1: Listing of Bush forever sites within the City of Kwinana

This Strategy should ensure the future planning framework appropriately reserves significant bushland areas and limits any development occurring on or nearby to those sites.

SPP 2.8 also contains policy measures for ‘local bushland’ and also provides guidance to local government to prepare a local bushland protection strategy or local biodiversity strategy, in accordance with the Local Government Biodiversity Planning Guidelines for the Perth Metropolitan Region (WALGA 2004). A draft local biodiversity study was prepared by the City and has been used to inform this Local Planning Strategy.

### 3.3.10 SPP 2.9 Water Resources

The main objectives of this policy are:

- To protect, conserve and enhance water resources that are identified as having significant economic, social, cultural and/or environmental values;
- To assist in ensuring the availability of suitable water resources to maintain essential requirements for human and all other biological life with attention to maintaining or improving the quality and quantity of water resources; and
- To promote and assist in the management and sustainable use of water resources.

The policy seeks to ensure that local and regional planning strategies, structure plans, schemes, subdivisions and development applications adopt measures relating to the identification, protection, management and/or enhancement of water resources including wetlands, waterways (such as rivers,

streams and creeks), estuaries, groundwater and surface water catchments, dams, floodplains, foreshores and existing and future surface and groundwater drinking water catchments and sources.

A number of significant water resources are located within the City, as discussed in 5.14 Water Resources and appropriate planning measures to protect the quality of these resources is crucial.

#### 3.3.11 SPP 3.0 Urban Growth and Settlement

This is a broad policy, which sets out the principles and considerations pertaining to planning for urban growth and settlement. This policy facilitates sustainable patterns of urban growth and settlement by setting out requirements for settlements and communities, including scope for growth and change. The broad objectives of this policy are:

- to provide a wide variety of housing, employment, recreation facilities, and open space;
- to meet the social and economic needs of the community;
- to enhance the quality of life of the community;
- to promote the development of a sustainable and liveable neighbourhood form; and
- to coordinate the timely provision of infrastructure and services to the new development.

This strategy is underpinned by these objectives as reflected in Part 1.

#### 3.3.12 SPP 3.4 Natural Hazards and Disasters

This policy encourages local governments to adopt a systematic approach to the consideration of natural hazards and disasters when performing statutory or advisory functions. Natural hazards and disasters include bush fires, floods, severe storms and storm surges, all of which are possible within the City.

Bushfire management is more specifically controlled through SPP 3.7 which the City applies at all levels of the planning process. Floodplain mapping is considered as part of the development process, with floodplain mapping being based on the 1 in 100 year flood, and development and subdivision design responding accordingly.

#### 3.3.13 SPP 3.5 Historic Heritage Conservation

SPP 3.5 sets out the principles of sound and responsible planning on the identification, planning and management of places of historic heritage significance. The policy applies principally to historic cultural heritage other than Aboriginal heritage (which is the subject of separate legislation), including heritage areas, buildings and structures, historic cemeteries and gardens, manmade landscapes and historic or archaeological sites with or without built features.

The City has a rich settlement history that is reflected in a number of places, buildings and the design and character of established suburbs. This Strategy will need to consider the significance of the City's character, identity and history and give direction on appropriate mechanisms by which to respond to the historic heritage significance of the locality.

#### 3.3.14 SPP 3.6 Development contributions for infrastructure

This policy sets out the principles and considerations that apply to development contributions for the provision of infrastructure in new and established urban areas. The objectives of this policy are:

- to promote the efficient and effective provision of public infrastructure and facilities to meet the demands arising from new growth and development;

- to ensure that development contributions are necessary and relevant to the development to be permitted and are charged equitably among those benefiting from the infrastructure and facilities to be provided;
- to ensure consistency and transparency in the system for apportioning, collecting and spending development contributions; and
- to ensure the social well-being of communities arising from, or affected by, development.

A draft SPP 3.6 was published for comment in July 2019 and a second revised draft SPP is expected to be released imminently. The City currently operates 15 development contribution schemes, 7 of which are still provisional. Therefore any new provisions resulting from a revised SPP will need to be considered.

### 3.3.15 SPP 3.7 Planning in Bushfire Prone Areas

The intent of this policy is to implement effective, risk-based land use planning and development to preserve life and reduce the impact of bushfire on property and infrastructure.

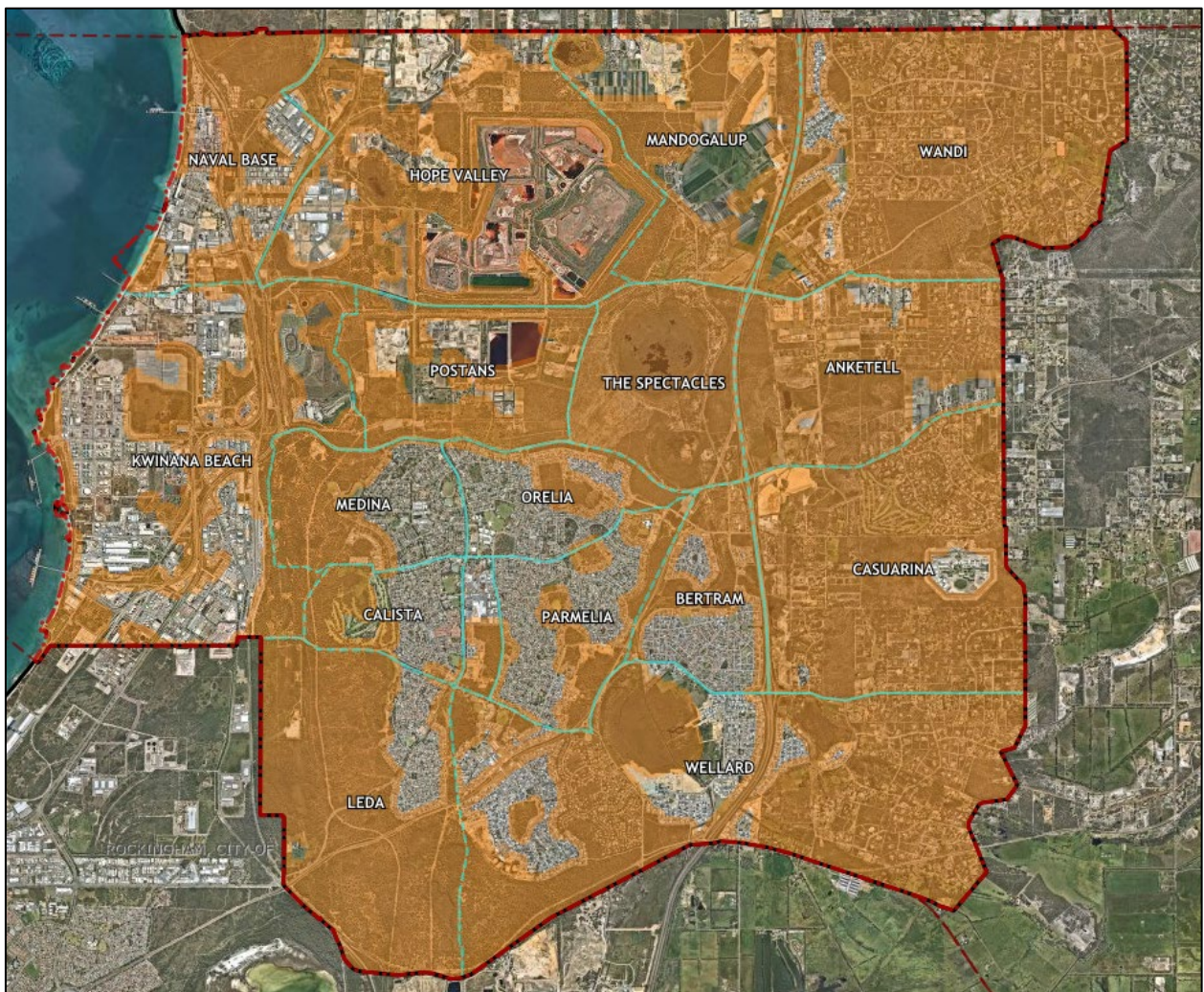


Figure 10: Map of Bushfire Prone Areas

The City has significant amount of bushfire prone areas as designated under the Fire and Emergency Services Act 1998. SPP 3.7 requires bushfire mitigation and protection measures to be integrated into structure planning, subdivision and development stages.

The State Government is currently reviewing WA's bushfire planning and building frameworks. This is to ensure they are based on scientific evidence and adapted to the State's landscapes and bushfire risks.

A working group representing DFES, DPLH, DMIRS and the Commonwealth Scientific and Industrial Research Organisation (CSIRO) is undertaking the review. It's anticipated an updated Map of Bushfire Prone Areas, draft SPP 3.7 and the Guidelines will be released for public discussion before being finalised in 2021.

Whilst bushfire planning is a necessity, certain mitigation measures introduce challenges in relation to conservation of bushland and amenity of rural areas and public open space. As such, the City is keen to ensure that bushfire mapping can be updated appropriately as the growth of the City's urban areas may impact upon the bushfire prone areas. The City may seek to undertake further detailed bushfire mapping to assist in updating the bushfire prone areas map.

### 3.3.16 SPP 4.1 State Industrial Buffer Policy

The purpose of the policy is to provide a consistent state-wide approach for the protection and long-term security of industrial zones, transport terminals (including ports) other utilities and special uses. It will also provide for the safety and amenity of surrounding land uses while having regard to the rights of landowners who may be affected by residual emissions and risk. The objectives of this policy are:

- To provide a consistent state-wide approach for the definition and securing of buffer areas around industry, infrastructure and some special uses;
- To protect industry, infrastructure and special uses from the encroachment of incompatible land uses;
- To provide for the safety and amenity of land uses surrounding industry, infrastructure and special uses; and
- To recognise the interests of existing landowners within buffer areas who may be affected by residual emissions and risks, as well as the interests, needs and economic benefits of existing industry and infrastructure which may be affected by encroaching incompatible land uses.

A draft SPP 4.1 has been released by the WAPC and it provides further guidance on how to protect industrial facilities. The key policy measures require that statutory buffers should be provided around strategic sites and facilities of State significance.

There are significant industrial land uses, within the City, which warrant protection under this SPP. The recent announcement of the outer harbour at Kwinana, also signifies the importance of the Western Trade Coast as being of State significance which pursuant to this SPP would require a statutory buffer within the MRS.

### 3.3.17 SPP 4.2 Activity Centres for Perth and Peel

The main purpose of this policy is to specify broad planning requirements for the planning and development of new activity centres and the redevelopment and renewal of existing centres in Perth and Peel. It is mainly concerned with the distribution, function, broad land use and urban design criteria of activity centres, and with coordinating their land use and infrastructure planning.

Other purposes of the policy include the integration of activity centres with public transport; ensuring they contain a range of activities to promote community benefits through infrastructure efficiency and economic benefits of business clusters; and lower transport energy use and associated carbon emissions. The policy proposes to incorporate higher-density housing within and immediately adjacent to these activity centres and stipulates that local planning strategies and schemes should optimise housing potential in walkable catchments and meet density targets.

Kwinana City Centre is listed as a Secondary Centre and Wandi a District Centre with all other centres within the City being Neighbourhood or Local centres. This Strategy will need to ascertain if the various centres within the City are fulfilling their role for the community, both existing and future and align the function of centres with SPP 4.2 objectives.



### 3.3.18 SPP 5.2 Telecommunications Infrastructure

This planning policy aims to balance the need for effective telecommunications services and effective roll-out of networks, with the community interest in protecting the visual character of local areas. Using a set of land use planning policy measures, the policy intends to provide clear guidance pertaining to the siting, location and design of telecommunications infrastructure. The objectives of this policy are:

- to facilitate the provision of telecommunications infrastructure in an efficient and environmentally responsible manner to meet community needs;
- to manage the environmental, cultural heritage, visual and social impacts of telecommunications infrastructure;
- to ensure that telecommunications infrastructure is included in relevant planning processes as essential infrastructure for business, personal and emergency reasons; and
- to promote a consistent approach in the preparation, assessment and determination of planning decisions for telecommunications infrastructure.

The SPP indicates that local governments may include any relevant provision of the SPP in a planning scheme or planning policy. The City has a LPP on Telecommunications and reviewed its policy in 2019.

### 3.3.19 SPP 5.4 Road and Rail Noise

This policy seeks to minimise the adverse impact of transport noise, without placing unreasonable restrictions on noise-sensitive residential development, or adding unduly to the cost of transport infrastructure.

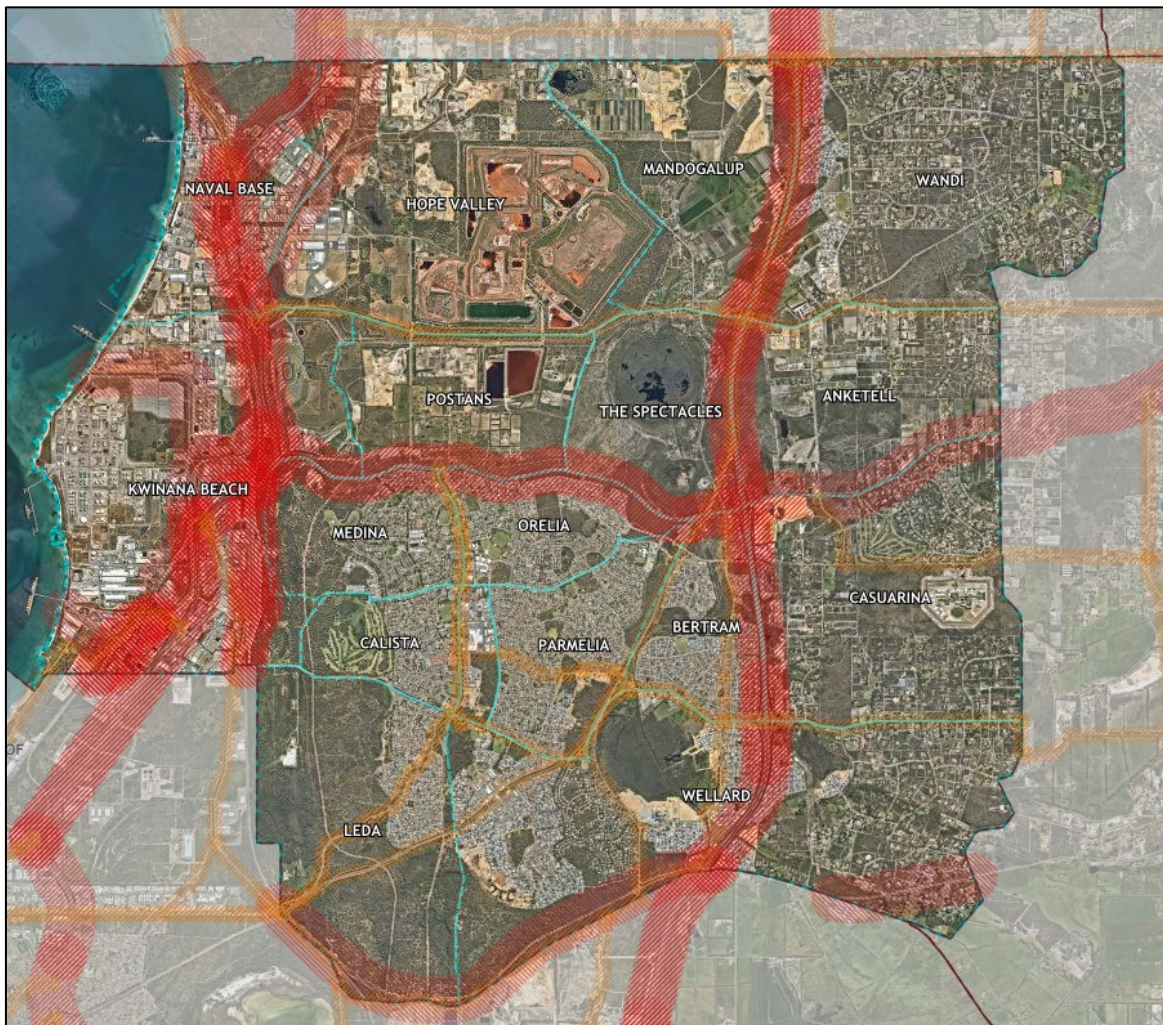


Figure 11: Map of transport noise corridors

SPP 5.4 also provides a standardised and consistent framework for the consideration and management of the impacts of transport noise and freight operations on new noise-sensitive development in the vicinity of existing or future major transport corridors or freight handling facilities, and new major road or rail infrastructure projects in the vicinity of existing or future noise-sensitive residential development.

There are a number of major and minor road and rail corridors throughout the City that require consideration in planning of urban areas and appropriate land uses. The City will need to ensure it manages the location of sensitive land uses in proximity to noise corridors and conversely, consider and advise relevant agencies of noise constraints when seeking to upgrade transport corridors.

It should also be noted that due to the recent announcement regarding the Outer Harbour, Rowley Road and Anketell Road will become major sources of road noise and the City should factor this into any future planning along or near these corridors now.

### 3.3.20 SPP 7.0 Design of the Built Environment

This policy was introduced to provide a broad framework for design of the built environment and achieve good design outcomes. It applies across all planning and development types such as structure planning, subdivision, and development applications across WA. It aims to provide consistency across all jurisdictions through a coordinated strategy of design quality mechanisms, which include:

- Design Principles – Performance-based approach to policy
- Design Review – Skilled evaluation expertise
- Design Skills – Skilled design expertise.

### 3.3.21 SPP 7.3 Residential Design Codes Volume 1

The purpose of the Residential Design Codes (R-Codes) is to provide a comprehensive basis for the control of residential development throughout Western Australia, primarily for single dwellings on single lots.

Local governments are responsible for administering the R-Codes and can vary certain requirements, through local planning mechanisms including local planning schemes and local planning policies.

### 3.3.22 SPP 7.3 Residential Design Codes Volume 2

The purpose of this policy is to provide planning and design standards exclusively for residential apartments (multiple dwellings) in areas coded R40 and above, within mixed use development and activity centres. This includes guidance for siting and orienting buildings into existing and emerging neighbourhoods, and offering support to design practitioners and decision makers who propose alternative and better design solutions.

This is a performance-based policy, which means an application for development approval needs to demonstrate that the design achieves the objectives of each design element. As with Volume 1 of the R-Codes, a Local Government may vary parts of Volume 2 where it is considered appropriate to modify standards for localised conditions.

### 3.3.23 Key issues

- Any existing inappropriate land uses within the Peel Harvey Catchment area may need to be relocated.
- Within the Peel Harvey Catchment area, remnant vegetation is required to be retained and rehabilitated.
- Lots sizes, within the Jandakot Water Mound area, are limited to a minimum of 2 hectares.

- Raw Material extraction sites exist within the City and appropriate zoning around these sites is essential to manage their impact.
- Within the Jandakot water mound area, the retention of rural living zones is encouraged, and further intensification is not permitted.
- MRS Rural zoned areas do not meet SPP 2.5 Rural planning objectives.
- Significant regional bushland areas require appropriate reservation under the MRS for formal protection.
- A Local Biodiversity Strategy may be required to protect significant local bushland areas.
- Significant water resources require protection from inappropriate development.
- The character, identity and history of the City needs to be acknowledged and protected.
- The operation of development contribution schemes may alter the City's approach to the delivery of key community and development infrastructure.
- Determine mitigation measures for bushfire planning conflicts with the conservation of bushland and amenity of rural areas and public open space.
- Assessment and update of bushfire mapping in line with urban development is required.
- Significant industrial land uses warrant statutory buffer protection under the Metropolitan Region Scheme.
- Activity Centres need to fulfil their role for the community, both existing and future, as multi-purpose community hubs that support their catchments.
- Sensitive land uses should not be located in proximity to noise corridors.
- Relevant agencies need to be advised of noise constraints when seeking to upgrade transport corridors that will impact upon existing and pre-planned urban areas.
- Residential built form outcomes, delivered by the R-Codes, may not meet the urban design objectives of the City.
- Investigation areas being considered by the State Government need to ensure there is due regard given to the increase in demand for industrial land arising from the outer harbor at Kwinana.

#### 3.3.24 Response to issues

The City shall review land use permissibility within the Peel Harvey and Jandakot catchment areas to determine appropriate uses for the area. Additional development controls may be necessary to ensure appropriate lot sizes are maintained and vegetation retention and rehabilitation occurs.

MRS zones and reserves should be reviewed and more land appropriately zoned based on its intended use. This may require rural zones to be modified and the potential for further MRS reserves to be introduced to ensure areas of significant environmental value and wetland and water resource areas are protected.

Local environmental values will need to be protected through scheme and policy controls and may require a Local Biodiversity Strategy to guide what assets require protection.

The City may seek to introduce scheme or policy controls to achieve local character and identity objectives, particularly where the R-Codes does not deliver the City's intended outcome.

The City may seek to undertake further detailed bushfire mapping to assist in updating the bushfire prone areas map.

Given the significance of the Kwinana Industrial Area and the nature of the land uses in the Western Trade Coast, there is a need for a statutory buffer to be developed under SPP 4.1.

The City will seek to ensure compatible development near and along freight routes so that future land use conflicts between road noise and sensitive uses are minimised.

### 3.4 Development Control and Operational Policies

There are multiple Development Control and Operational Policies affecting land use planning, development and subdivision. A large proportion are applicable within the City as it performs its statutory planning requirements, however there are only a few that have specific relevance to the preparation of a Local Planning Strategy.

#### 3.4.1 Liveable Neighbourhoods

Liveable Neighbourhoods is a Western Australian government sustainable cities initiative which addresses both strategic and operational aspects of structure planning and subdivision development.

The purpose of the policy is to better structure new urban development on greenfield and large urban infill sites. The policy aims to achieve well-defined, sustainable and self-sufficient communities through site-responsive design based on safe, sustainable, attractive neighbourhoods interconnected by a street layout that promotes walking, cycling and public transport and reduced dependency on private vehicles. Community needs, employment opportunities and economic sustainability are facilitated through a coherent hierarchy of mixed used main street activity centres.

A Draft Liveable Neighbourhoods policy was released for comment in 2015 and it has been indicated by the WAPC that the draft and any feedback received will be taken into account as part of the Design WA suite of policies.

The City has significant tracts of development zoned land where structure planning process will need to occur, the Liveable Neighbourhoods policy, will be a key instrument for the City in ensuring sustainable communities are developed.

#### 3.4.2 Development Control Policy 1.6 Planning to Support Transit Oriented Development

This policy seeks to maximise the benefits to the community of an effective and well used public transit system by promoting planning and development outcomes that will support and sustain public transport use.

DC policy 1.6 applies to transit-oriented precincts outlined as those within 800 metres of high frequency heavy rail or major bus transfer stations and within 400 metres of high frequency bus stops. DC policy 1.6 specifies that development within these areas should achieve a density of at least 25 dwellings per gross hectare in order to support the public transport infrastructure.

Kwinana and Wellard Train stations and Gilmore Avenue are identified as indicative transit-oriented precincts. The City must have regard to the policy position outlined in DC policy 1.6 and this Strategy will further investigate the potential for transit-oriented precincts.

#### 3.4.3 Development Control Policy 2.3 Public Open Space in Residential Areas

DC policy 2.3 provides the broad framework for the provision, use and development of public open space areas. The policy is based on the principles of the Stephenson-Hepburn plan for Perth developed in 1956 which recommended that approximately 3.36 hectares of public open space per 1,000 people is provided. This principle is imbedded in the Planning and Development Act 2005 which allows the WAPC to impose a requirement that land as part of a subdivision be set aside for public open space. This is generally provided at a rate of 10% of the subdivisible area. The policy also considers principles for the use of foreshore reserves, regional open space reservations and community facility provision.

The provisions of DC policy 2.3 should guide the City's approach towards open space planning in ensuring that sufficient land is allocated towards public open space and also to ensure that new development contributes to the provision, and/or upgrading, of public open space.

#### 3.4.4 Development Control Policy 4.1 Industrial Subdivision

This policy statement provides guidance for industrial subdivision throughout the State on matters such as the design and shape of industrial lots, road layout, servicing and open space requirements. The objectives of this policy are:

- To encourage the development of well-designed industrial areas serving the full range of general and special industrial needs throughout the State.
- To provide for the safe and efficient movement of traffic to and from each site within the industrial area.
- To provide for infrastructure services and public open space consistent with the operational needs of industrial users and the workforce.
- To protect the amenity of adjacent land uses, where necessary, from the effects of industrial development.

The City has significant areas of industrial zoned and industry expansion land identified under the Sub-regional Framework and industrial subdivision design will need to take into account this policy's requirements.

#### 3.4.5 Development Control Policy 4.2 Planning for Hazards and Safety

This policy seeks to provide guidance in planning to prevent the occurrence of potentially hazardous events and to mitigate the effects of any such events, should they occur and in ensuring that the appropriate procedures are followed in the consideration of proposals.

The objectives of the policy are:

- To ensure that developments are reviewed with a view to maintaining appropriate public safety.
- To maintain acceptably low risk exposure through appropriate planning procedures.
- To provide guidelines for the consideration of the influence of existing risk levels in the planning process.
- To provide for the development of industries and activities which are hazardous but which are desirable for the benefit of the wider community

This has implications for the City as the policy seeks to control land use permissibility to mitigate risk for hazardous industrial areas and seeks to apply buffer zones around hazardous industrial areas with reference to EPA guidance. This Strategy will need to ensure appropriate land uses are located within hazardous industrial areas and also within established buffer areas.

#### 3.4.6 Development Control Policy 5.1 Regional Roads (Vehicular Access)

This policy sets out the principles to be applied when considering proposals for vehicle access to or from developments abutting regional roads. The access control requirements of this policy apply to Primary and District Distributors, which includes all categories of regional roads designated in the Metropolitan Region Scheme.

This is significant for the City with an increased importance being placed on Anketell Road, Rowley Road and Thomas Road as the main freight routes for the Outer Port.

#### 3.4.7 Key issues

- Structure planning processes must respond to Liveable Neighbourhoods sustainable design principles. Any revision to the Liveable Neighbourhoods policy may have implications on the City's ability to deliver sustainable communities.

- Kwinana and Wellard Train stations and Gilmore Avenue are identified as indicative transit-oriented precincts and this Strategy will need to consider appropriate development within their walkable catchments.
- Ensure adequate allocation and design of POS, for emerging and established areas.
- The design of new industrial areas will need to follow policy guidance.
- Hazardous industrial areas require appropriate land use controls and buffer areas.
- Access onto major east-west routes through the City will be limited.

#### 3.4.8 Response to issues

Design will become a key factor in the assessment of all future proposals put before the City. In particular, the City will pursue sustainable design outcomes through its assessment of structure plans and any infill precinct plans and seek to improve upon the design of POS and industrial developments.

The City will focus on delivering quality diverse housing outcomes within transit based walkable catchments.

Given the significance of the Kwinana Industrial Area and the nature of the land uses in the Western Trade Coast, there is a need for a statutory buffer.

The City will seek to ensure compatible development near and along freight routes so that future land use conflicts between road noise and sensitive uses are minimised.

### 3.5 Other relevant strategies, plans and policies

#### 3.5.1 Perth & Peel @3.5 million - The Transport Network

The Transport Network document summarises the transport components of the Perth and Peel sub-regional land use planning and infrastructure frameworks.

Specific transport projects that affect the City include:

- A new north–south route connecting Spearwood and Gilmore Avenues with opportunities for a lateral connection to the potential north–south route is proposed.
- Nairn Drive will connect to Wellard Road — via Kerosene Lane and Baldivis Road – with a direct grade-separated crossing over the railway.

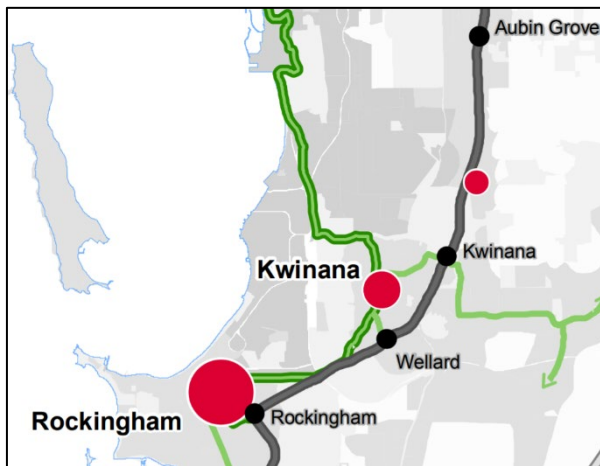


Figure 12: 2050 Public transport network

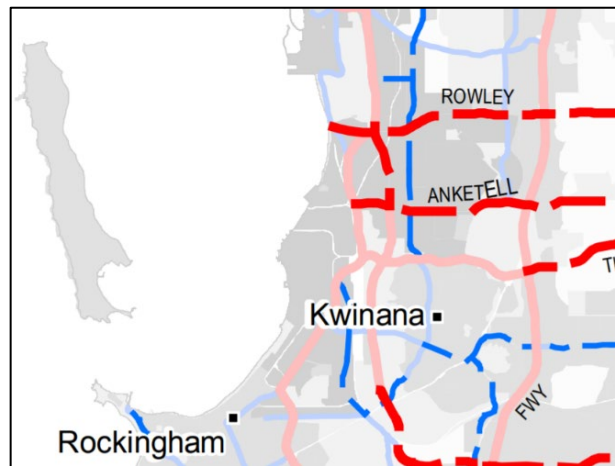


Figure 13: 2050 Road network

In addition to the above, Gilmore Avenue is also indicated as a ‘high-priority transit corridor’ with connections to both train stations.

The City will need to ensure it acknowledges the importance of this sub-regional connections and seeks to capitalise on the opportunities they afford.

#### 3.5.2 WA Housing Strategy

The purpose of the WA Housing Strategy is to create an agile housing system that underpins individual, family and community wellbeing and is based on four main principles:

- A housing system that provides choice for every Western Australian.
- Support equity and accessibility to create an inclusive WA.
- Maintain and grow the social housing safety net.
- Person-centred, place-based and community focused.

Local Government is listed as a key stakeholder for the following targets:

- Ensure a minimum of 20 per cent social and affordable homes in Government residential developments. By 2025, develop a framework and baseline standards to make approval systems consistent and efficient.
- Grow the number of transit-oriented homes by 45 per cent, providing 150,000 new homes close to public transport.
- Catalyse 40 per cent of new homes built to liveable design standards by 2030.
- Inform consumers and incentivise industry to support 30 per cent of new WA homes built from non-conventional building materials by 2030.

- By 2025, develop a system to enable planners, developers, local government authorities and consumers to easily access housing information.

The City's role will be to collaborate with the Department of Communities to implement, where possible and appropriate, the above targets.

### 3.5.3 Guideline for the Determination of Wetland Buffer Requirements

This guideline has been developed to assist landowners, developers, planners and architects to identify an appropriate buffer between wetlands and land uses that will enhance or maintain the significant attributes and values of the wetland.

The City currently uses this guide to inform the structure planning design and the appropriate management of development around important environmental assets.

It is of interest to the City as there are significant areas of wetlands within the City and this document guides the planning processes that protect the wetland areas. It is a planning concern that the guideline is still listed as a draft, and has been since 2005. Its status should be resolved and preferably elevated.

### 3.5.4 Planning Bulletin 87 High Pressure Gas Transmission Pipelines in the Perth Metropolitan Region

This planning bulletin provides guidance when considering planning proposals in the vicinity of the Dampier to Bunbury Natural Gas Pipeline and the Parmelia Gas Pipeline, in the Perth metropolitan region.

Both the Dampier to Bunbury natural gas pipeline and the Parmelia pipeline traverse the City and any potential development within their vicinity will need to adhere to the separation distances required by this policy.

### 3.5.5 Visual Landscape Planning in Western Australia

This manual provides advice to state agencies, local governments, developers and the community on techniques for incorporating visual landscape planning into the planning system.

Local planning strategies should identify regional and local landscapes and help set the objectives for the protection and enhancement of these landscapes. Local governments, particularly those with areas of significant landscape value, are encouraged to review and incorporate appropriate landscape policies in their local planning frameworks.

The City undertook a landscape analysis in 2013 which identifies elements of topography, vegetation, water and the built environment. Given the amount of change that has occurred in the City since the study was undertaken, it is considered appropriate that it be reviewed.

### 3.5.6 Key issues

- Additional secondary regional roads linking Gilmore Avenue north through the Postans and Hope Valley Wattleup areas will provide increased connectivity to Perth's southern suburbs.
- A high-priority transit corridor is proposed for Gilmore Ave, which includes a connection to both Kwinana and Wellard train stations which will elevate its status as a public transport corridor.
- Wetland classification, planning and management will need to be incorporated into planning functions to provide certainty in regard to wetland management.
- Future development near high pressure gas pipelines will need to take into account separation distances which could affect development layout and design.



- Landscape character requires identification and protection and the City should reassess its local landscape values and implement appropriate planning controls.

### 3.5.7 Response to issues

Development along key transport links will need to be planned to capitalise on increased connectivity. This may prove valuable for businesses seeking exposure to high volume routes throughout the area that is close to the outer harbor and other industry, whilst not being located along heavy freight routes.

The City will continue to ensure wetlands are identified and managed appropriately and will seek to be involved in any review of the Guidelines for the Determination of Wetland Buffer Requirements.

Development near critical infrastructure will need to take into account relevant setbacks and separation distances. Whilst not explicitly outlined in the planning framework, this should also be applied to development near high voltage power lines, where comment from Western Power will be necessary.



## 4.1 Strategies and Schemes

### 4.1.1 Strategic Community Plan 2019-2029

The Strategic Community Plan (SCP) outlines the community's long-term (10+ years) vision, values, aspirations and priorities with reference to other plans, information and resourcing capabilities of a local government. This plan is not static, a full review is currently underway and is expected to be adopted by Council in mid-2021. The purpose of the plan is to:

- establish the community's vision for the local government's future, including aspirations and service expectations;
- drive the development of local government area/place/regional plans, resourcing and other informing strategies, e.g. workforce, asset management and services; and
- ultimately drive all other planning.

The City's vision outlined in the Strategic Community Plan is:

**Rich in spirit, alive with opportunities, surrounded by nature – It's all here.**

The vision statement represents the City's Community, Economy, Environment, and Infrastructure respectively.

The following objectives listed in the plan are directly relevant to the preparation of the Local Planning Strategy:

- d) Create diverse places and spaces where people can enjoy a variety of lifestyles with high levels of amenity;
- e) The City Centre is home to a thriving range of specialty shops, restaurant and family entertainment venues and an active night-life while neighbourhood centres are revitalised;
- f) The Western Trade Coast Precinct is developed with maximum leverage being gained from investments in new infrastructure; and
- g) Improve conservation of biodiversity and protection of native vegetation whilst achieving high levels of environmental protection in new developments.

This Strategy will need to ensure that the objectives of the Strategic Community Plan are embedded in the strategic directions and actions listed in Part 1.

### 4.1.2 Local Commercial and Activity Centre Strategy 2014

The Local Commercial and Activity Centres Strategy (LCACS) is an important guiding document for the City's decision making for commercial and activity centres within the City. It has set the framework for the size, distribution and location of activity centres as well as commercial and office uses across the City.

The LCACS's main purpose is to allocate retail floor space for commercial purposes across the hierarchy of activity centres within the City. It assists City Officers and Council in its decision making about such land uses, and places strong importance to the prosperity and promotion of the Kwinana City Centre as the key activity centre for the broader wellbeing of the City and community.

LCAC's has been reviewed as part of the economic study undertaken for this Strategy and its strategies and actions are further discussed and reflected through section 5.7 Activity Centres.

### 4.1.3 City Centre Masterplan

The Kwinana City Centre Master Plan and Design Guidelines was developed as the framework for the City Centre and surrounding precinct in 2007 and updated in 2019.

As a document to guide the future vision and development of the city centre, the Masterplan concentrates on those aspects which are of particular importance - creating places for people, establishing the right built form response and establishing strong spatial relationships to integrate the city centre with surrounding neighbourhoods.

The Masterplan is referenced through LPS3 and contains design guidelines that provide urban structure, built form and public realm provisions. As LPS3 is reviewed as part of any new local planning framework the provisions of the Masterplan must be incorporated into a new planning mechanism. It is also worth noting that the Masterplan does not extend to land use control and there are opportunities for a future scheme to reflect the land use permissibility suggested through the masterplan.

#### 4.1.4 [Bike and Walk Plan 2018](#)

The City aims to create communities where cycling and walking are an integral part of daily life for all types of trips, enabling people to lead healthier lifestyles and stay more active and independent for longer.

The plan seeks to increase walking and cycling use within the City by making recommendations for improvements to existing infrastructure and developing new infrastructure where demand requires it.

The objectives of the plan are as follows:

- To have an interconnected, continuous and well maintained bicycle and pedestrian network that pedestrians and cyclists of all abilities feel comfortable using;
- To be a city where walking and cycling is the first choice for transport (for all ages) for short trips (1-2km);
- To have a network of safe roads designed to Safe Active Travel Street principles to encourage the short trip journeys;
- To improve the City's health issues by actively promoting new cycle and walking infrastructure as implemented; and
- To improve walking and cycling access to schools, train stations, parks and recreational facilities.

Neighbourhood plans that define long-term objectives and implementation priorities have been developed for individual communities within Kwinana with priority given to Bertram, Medina and Leda.

The City should continue to invest in implementation of the Bike and Walk Plan and consider relevant actions that will ensure the planning framework facilitates the implementation of this plan where possible.

#### 4.1.5 [Local Planning Scheme 2 and Local Planning Scheme 3](#)

The City currently operates two Local Planning Schemes being Local Planning Scheme 2 (LPS2), which was gazetted on 20 November 1992 and Local Planning Scheme 3 (LPS3) gazetted August 1998.

LPS3 applies to the Kwinana City Centre, providing a level of detail required to achieve the most appropriate development outcome within the City Centre whilst LPS2 applies to all other land within the City and contemplates both zoning and development control for the remainder of the City.

To date, there have been a total of 159 amendments made to LPS2 (as at 23 October 2019), signalling that a review of it is necessary to meet the challenges that have risen out of the various statutory and strategic documents released by the State.

The changes introduced in the Planning and Development (Local Planning Schemes) Regulations 2015 (Regulations 2015) have provided opportunities to simplify and streamline Local Planning Schemes. Any

new Local Planning Scheme will be required to be consistent with the Model Scheme Text under the Regulations 2015.

#### 4.1.6 Key issues

- The objectives of the Strategic Community Plan must be addressed through the strategic directions and actions listed in Part 1 of this Strategy.
- The City Centre remains the primary activity centre for Kwinana and recommendations from LCAC's will inform future actions for centres.
- Any new local planning framework must incorporate the City Centre Masterplan objectives and design guidelines.
- The City's planning framework must incorporate relevant implementation requirements of the Bike and Walk Plan.

#### 4.1.7 Response to issues

The Strategic Community Plan has been used to frame Part 1 of this Strategy and each key theme is designed to align with the Strategic Community Plan.

The City's local planning framework will reinforce the City Centre and will incorporate the Masterplan into any new planning tools that apply to the Centre.

There are key actions from the Bike and Walk Plan that are already being implemented and the City will continue to ensure these priorities are integrated into the planning framework where necessary.

## 4.2 Plans and Policies

### 4.2.1 Local Structure Plans

A structure plan is a plan that provides for the coordination of future subdivision and zoning of an area of land. In accordance with the Regulations, a decision maker for an application for development approval or subdivision approval in an area covered by a structure plan is required to have due regard to that structure plan.

A structure plan has effect for a period of 10 years, following which it is anticipated that the structure plan be normalised into a scheme via a scheme amendment or as part of a scheme review. Should a structure plan area not be significantly developed, and thus not appropriate for normalisation, the Regulations allow the WAPC to grant extensions to the operational period of a structure plan.

The City currently has the following structure plans that are in effect that will need review and potential normalisation into any new Scheme.

Local Structure Plan	Lot Number/s and Location
Anketell North	Anketell North LSP
Anketell South	Lots 1, 2, 3, & 17 Thomas Road and Portion Lot 13 Treeby Road
Mandogalup East	Lots 9002 & 9006 Hoffman Road and Lot 9019 Rowley Road
Mandogalup West	Lot 682 & 52, Rowley Road.
Wandi South	Lots 60 & 61 Kenby Close
Wandi North	Lot 313 (651) Anketell Road
Wellard	Lot 64 (78) Woolcoot Road
Wellard	Sunrise Estate - Part Lot 9001 (previously Lot 201) Mortimer Road and Part Lot 379 Millar Road
Wellard	Lot 661 Bertram Road
Wellard	Lot 601 Millar Road, Wellard
Wellard	Oakebella Estate - Lots 503-505 507 & 900 Johnson Road Wellard
Wellard	Lot 502 Tamblyn Place
Wellard	Lot 500 & 501 Bertram Road
Wellard	Lots 670 & 1338 Bertram Road and Reserve
Wellard	Lot 59 Mortimer Road, Wellard
Wellard	Lots 90 & 378 Millar Road Wellard
Wellard	Lot 900 Tamblyn Road
Wellard	Wellard Village

Wellard	Providence - Lots 167-170, 83, 85, 92, & 1278 Wellard Road, and Lots 1, 10, & 2 Johnson Road in Wellard
Wellard	Emerald Park - Lots 1, 21, & 500 Mortimer Road and a portion of Lot 9000 Johnson Road, Wellard

#### 4.2.1.1 Postans Precinct Structure Plan

The Postans Precinct Structure Plan was a study undertaken on behalf of the Department of Planning (now DPLH), LandCorp (now Development WA) and the City of Kwinana to identify additional general and light industrial land in the Postans Precinct as a transition to Latitude 32. The study considered the Fremantle/Rockingham Industrial Area Regional Strategy, Western Trade Coast Development Strategy, National Ports Strategy, and Port Optimum Planning Study.

The outcomes of the study indicated that the Postans Precinct is a strategic land parcel that should be recognised for its longer term development potential for emerging industry.

It has been recommended that the land is retained and released progressively in response to strategic industries' specific supply chain, land requirements and business collaboration or for emerging industries.

The ultimate status of the Postans Precinct Structure Plan is unclear, therefore it may be appropriate to review the plan. Nonetheless, any future recommendations made by this Strategy will need to consider the direction given by the Postans Precinct Structure Plan.

#### 4.2.2 Local Planning Policies

In accordance with the Regulations 2015 a local government may prepare a local planning policy (LPP) in respect of any matter related to the planning and development of the Scheme area.

The City has prepared 32 LPP's on a range of planning matters to help guide planning decisions. These policies are prepared under LPS2 and whilst decision-makers must have due regard to the provisions of a policy before making a decision, they are not bound by the requirement of a policy.

Following the finalisation of this Strategy the City will need to review all existing LPP's to ensure they align with the strategic direction set by the Strategy. Any review of LPP's will also need to ensure they are complimentary to a future local planning scheme. It is therefore prudent that the LPP review occurs alongside the drafting of a new scheme.

#### 4.2.3 Local Development Plans

Local development plans (previously known as detailed area plans) are used for introducing specific design elements for defined local areas and include aspects such as the location of garages, vehicle access, parking, solar active/passive design, street orientation and setbacks. Local development plans have been developed for most new development areas and will continue to apply under a new local planning framework.

The development of new local development plans will need to align with the objectives of the City as outlined by this Strategy.

#### 4.2.4 Key issues

- Local structure plans must be reviewed, and where appropriate normalised, as part of any new scheme development process.
- Consider the Postans Precinct Structure Plan recommendations in any land use planning for that locality.

- Review LPP's as part of aligning with the Strategy's direction and new Scheme provisions.
- Ensure any new LDP's are developed according to the strategic direction given by this Strategy.

#### 4.2.5 [Response to key issues](#)

The City will undertake a review of all structure plans as part of developing a new Scheme and will review its policy framework to supplement, and align with, the new scheme in accordance with the direction given by this Strategy.



## 4.3 Other relevant strategies, plans and policies

### 4.3.1 Community Infrastructure Plan

The Community Infrastructure Plan (CIP) provides the City with a strategic framework for providing community infrastructure for the period 2011 to 2031 and focuses on community facilities typically provided by Local Government. The CIP has also been prepared to meet the requirements of the WAPC'S, State Planning Policy 3.6 'Development Contributions for Infrastructure' and establishes a framework upon which the policy can be applied and implemented.

While a standalone plan in its own right, the CIP should be considered a critical part of the City's Development Contribution Plans contained in Local Planning Scheme No. 2.

### 4.3.2 Municipal Heritage Inventory / Local Heritage Survey

The City of Kwinana engaged Palassis Architects to carry out a review of the existing 2007 Municipal Heritage Inventory in 2015. This involved a review of all the places currently on the Inventory, but no new nominations.

This revised heritage inventory is used to inform this Strategy, particularly section 5.3.2.1 Local Heritage Survey and Heritage List and provides direction on the character and identity of the City. Furthermore, the City is also currently engaging the services of a heritage consultant to review the 2015 Municipal Heritage Inventory in order to bring it into alignment with recent changes to legislation.

### 4.3.3 Key issues

- This Strategy must reflect the CIP requirements for public open space and community facilities and any planning framework must incorporate the relevant development contribution plans to ensure their implementation.
- This Strategy must reflect the heritage and character elements outlined in the Municipal Heritage Inventory.

### 4.3.4 Response to issues

The CIP has been used to inform this Strategy, particularly section 5.4 Public Open Space & Community Infrastructure

Section 5.3.2.1 Local Heritage Survey and Heritage List and provides direction on the character and identity of the City. A review is underway of the City's heritage places and that review will inform future planning controls such as the development of a heritage list and/or any local planning policy controls that may be appropriate for retaining character and identity.

5.0 Local Government Profile



# Community



## 5.1 Population and Housing

### 5.1.1 Population growth

The City's population is projected by forecast i.d. to grow from 40,305 (2016) to 85,158 (2036). This is generally consistent with WAPC WA Tomorrow<sup>5</sup> projections, which show population increasing to between 68,290 and 75,270 by 2031, with a mid-range (Band C) almost identical to the forecast id projection at 75,260 in 2031. The Sub-regional Framework shows the population of the City being at 80,000 in 2050. This implies either slower growth than the current trajectory of the .id forecast and WA Tomorrow series or no growth after 2036.

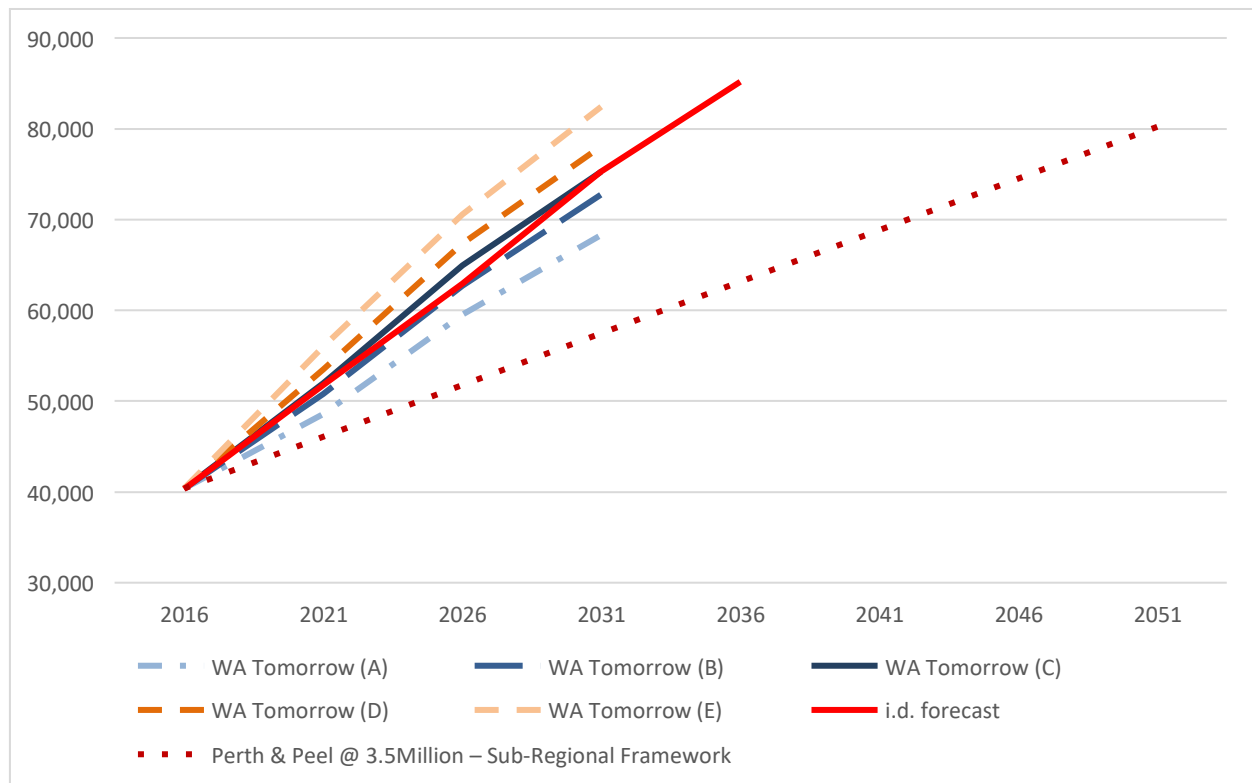


Figure 14: Population Projections, WA Tomorrow, i.d. forecast, Perth & Peel @ 3.5 Million

For the purpose of this Local Planning Strategy i.d. forecast projections have been used and further analysis is provided in Table 2: Forecast Population, Households and Dwellings.

In 2016, the Estimated Resident Population of the City was 40,305 people. It is expected to increase by over 44,800 people to 85,158 by 2036, at an average annual growth rate of 6.45%. This is based on an increase of over 15,245 dwellings to the City during the same period, with the average number of persons per household rising from 2.68 to 2.81 by 2036, as shown in Table 2: Forecast Population, Households and Dwellings.

<sup>5</sup> Western Australia Tomorrow, Population Report No. 11 - sub-State ASGS (SA2), Medium-Term Population Forecasts 2016 to 2031

Forecast population, households and dwellings - Summary	Forecast year				
	2016	2021	2026	2031	2036
Population	40,305	51,746	62,902	75,270	85,158
Change in population (5yrs)	-	11,441	11,155	12,368	9,888
Average annual change	-	5.12	3.98	3.66	2.50
Households	14,740	18,559	22,443	26,480	29,985
Average household size	2.68	2.74	2.76	2.81	2.81
Population in non-private dwellings	769	855	913	971	1,029
Dwellings	15,480	19,521	23,524	27,530	30,989
Dwelling occupancy rate	95.22	95.07	95.40	96.19	96.76

Table 2: Forecast Population, Households and Dwellings

Accordingly, it is necessary to prepare for Kwinana's community to grow beyond an ultimate population of over 85,000. Whether this significant growth is realised by 2050 does not remove the need to ensure that the population can be accommodated when required. A key challenge will be to ensure the delivery of the required housing stock for this population in a manner that is appropriate to Kwinana.

### 5.1.2 Age structure

Analysis of the City's age structure, as shown in Table 3: Service Age Groups, compared to the South West Group, shows that there was a higher proportion of people in the younger age groups (0 to 11 years) and middle aged groups (18 to 34 years). Conversely, there was a lower proportion of people in the secondary schoolers (12 to 17 years) and older age groups (50+ years).

2016 Service age group (years)	CoK %	SW Group %
Babies and pre-schoolers (0 to 4)	8.7	6.7
Primary schoolers (5 to 11)	10.1	9.4
Secondary schoolers (12 to 17)	6.9	7.4
Tertiary education and independence (18 to 24)	9.9	9.2
Young workforce (25 to 34)	19.5	14.7
Parents and homebuilders (35 to 49)	21.9	21.2
Older workers and pre-retirees (50 to 59)	10.0	12.5
Empty nesters and retirees (60 to 69)	7.1	9.5
Seniors (70 to 84)	5.2	7.5
Elderly aged (85 and over)	0.8	1.9
Total	100	100

Table 3: Service Age Groups

Forecast increase in Service Age groups, shown in Figure 15: Forecast population in Age Structure, indicates that for the next two decades there will be an increase of 12,619 in the children (0 to 17 years) categories, 25,251 in the working age (18 to 59) categories, and 5,157 persons in the older (60 plus) categories.

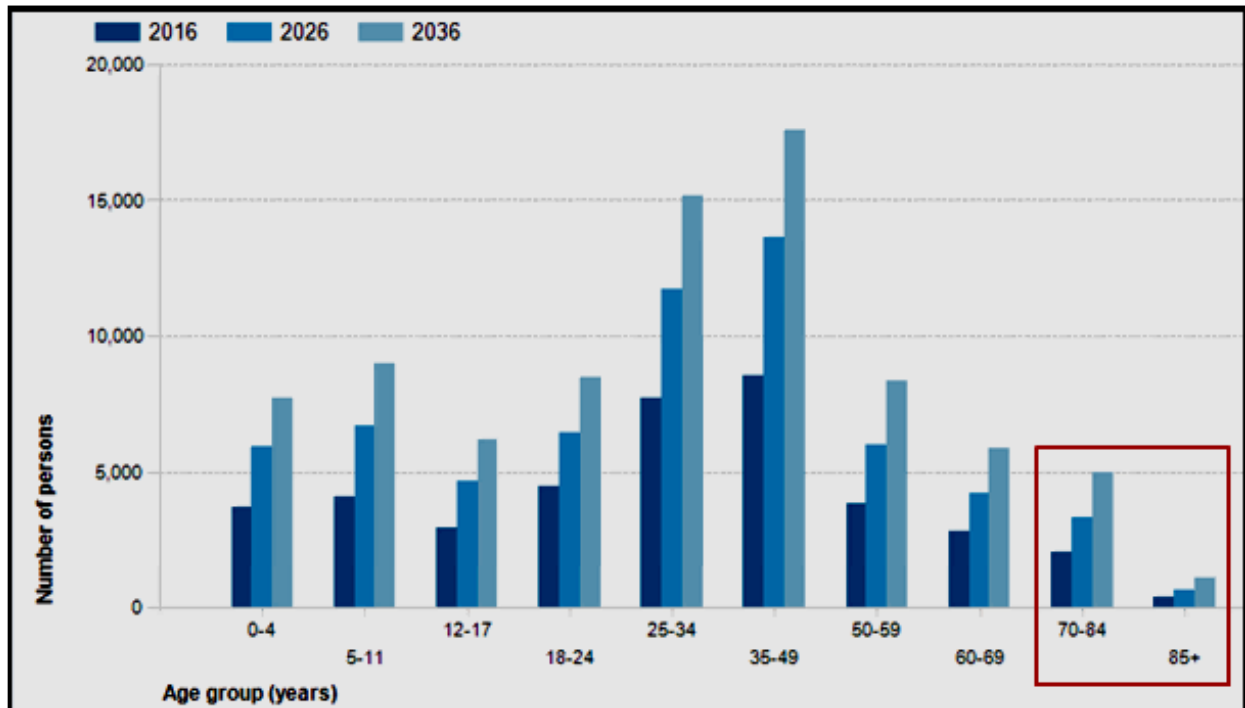


Figure 15: Forecast population in Age Structure

Whilst the population forecasts indicate the most significant increases occurring within the working age group categories, as a percentage of the total population across the City, the greatest change, as shown in Figure 16: Forecast change in Age Structure actually occurs in the 60+ age categories. These groups are expected to increase steadily in proportion to the general population by 14.3% (12,177 out of 85,158) by 2036.

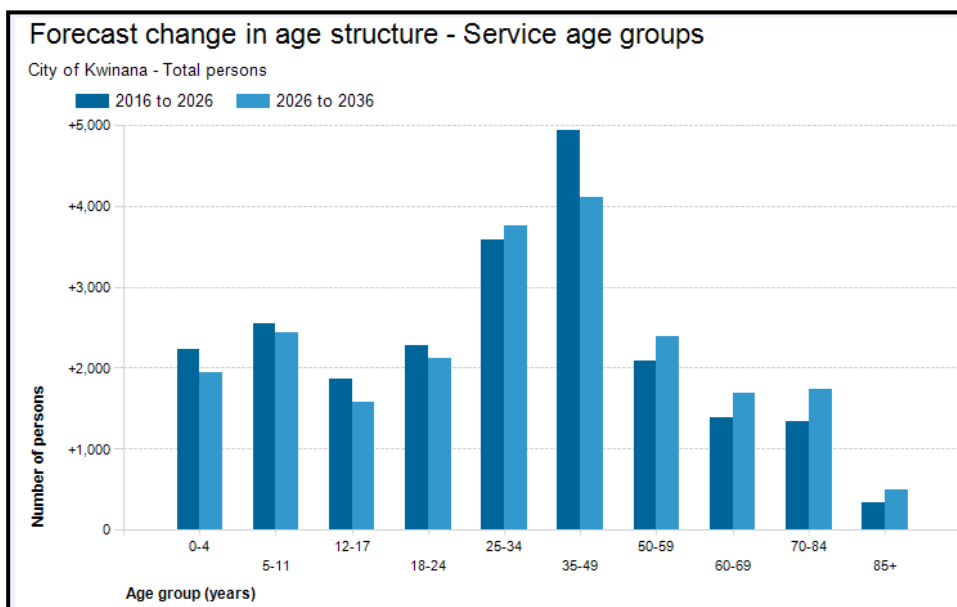


Figure 16: Forecast change in Age Structure

The service needs of these age groups are different; for example, persons in the 60+ age categories may need smaller, one and two bedroom dwellings that are close to activity centres in order to access daily needs, visit medical centres, and to have an active life, whereas the children categories are likely to need access to schools and recreation facilities.

### 5.1.3 Cultural background

Country of Birth data identifies where people were born and is indicative of the level of cultural diversity in the City of Kwinana.

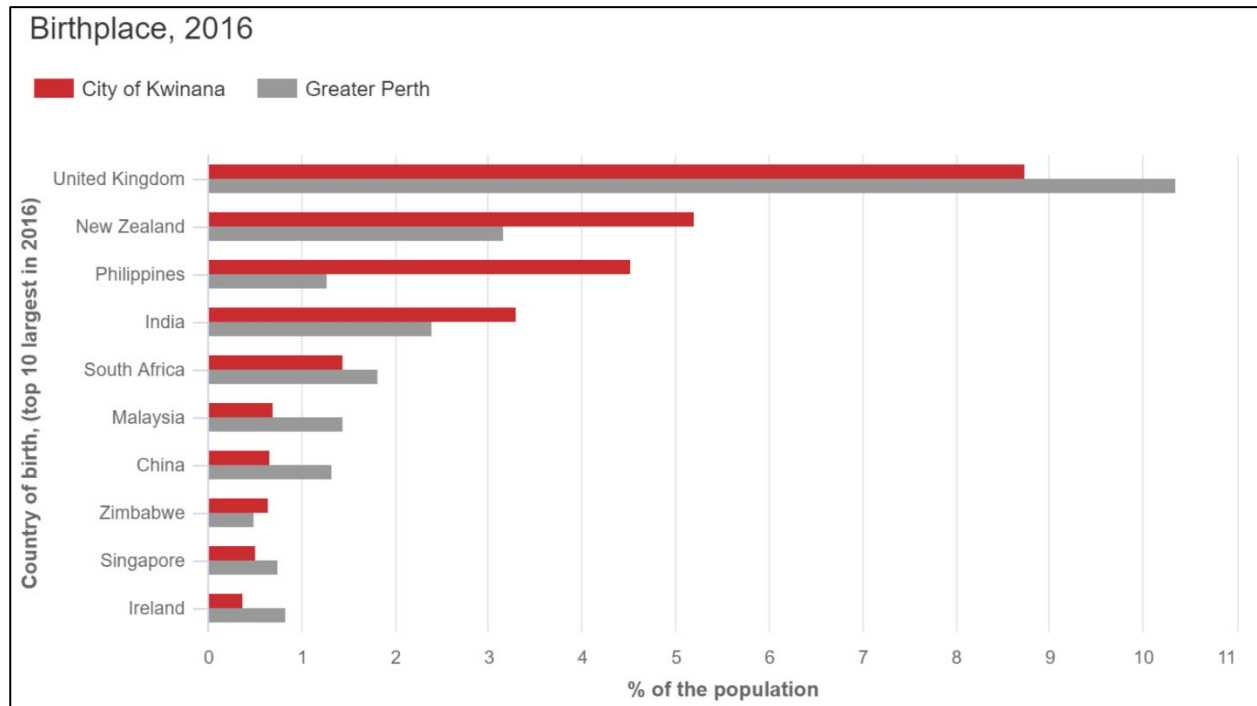


Figure 17: Country of birth 2016

Analysis of the country of birth of the population in the City of Kwinana in 2016 compared to Greater Perth shows that there was a similar proportion of people born overseas with 34.3% of the population was born overseas, compared with 36.1% for Greater Perth.

However, there is a significant difference in the number of people from New Zealand, the Philippines and India and between 2011 and 2016, the number of people born overseas increased by 5249 or 64.7%.

The largest changes in birthplace countries of the population in this area between 2011 and 2016 were for those born in:

- Philippines (+1,310 persons)
- India (+1,060 persons)
- New Zealand (+654 persons)
- South Africa (+211 persons)

This demonstrates the significant diversity in cultural background within the City and can be a driver for the need for affordable housing and adaptable housing types.

### 5.1.4 Household type

The City's profile for household types, as shown in Figure 18: Households by type, is similar to that of the broader South West Group with the City having proportionally more one parent families and slightly less lone person and couples without children households.

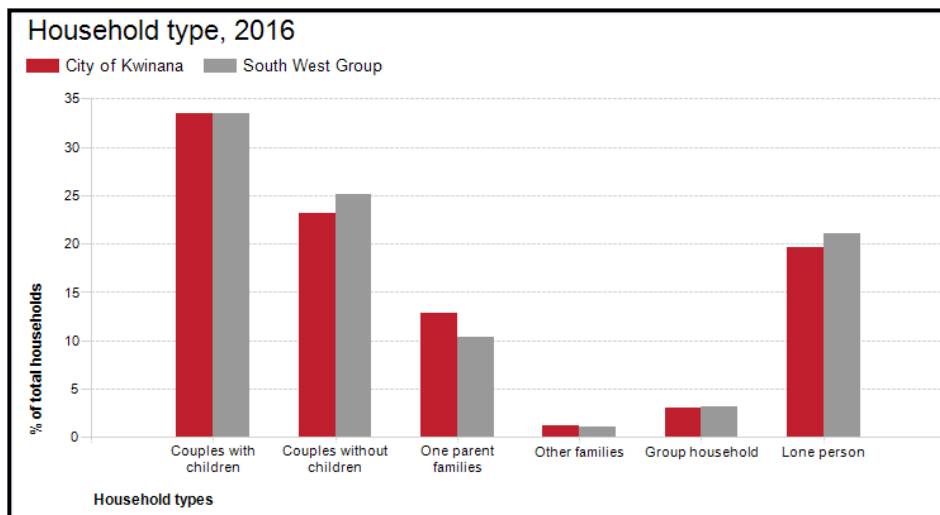


Figure 18: Households by type

Within the City, the majority of household types are couples with children, making up 46% of the total households.

23.1% of the City’s total families are couples without children. As shown in Figure 19: Couples without children, the newer suburbs of Wandi - Mandogalup (33.5%), Wellard East (28.9%), Anketell-Casuarina (28%), and Wellard West (26.5%) have a predominantly higher percentage of couples without children, while established areas show a lower proportion of couples without children.

This may indicate younger couples buying and building in those areas in anticipation of setting up a young family household.

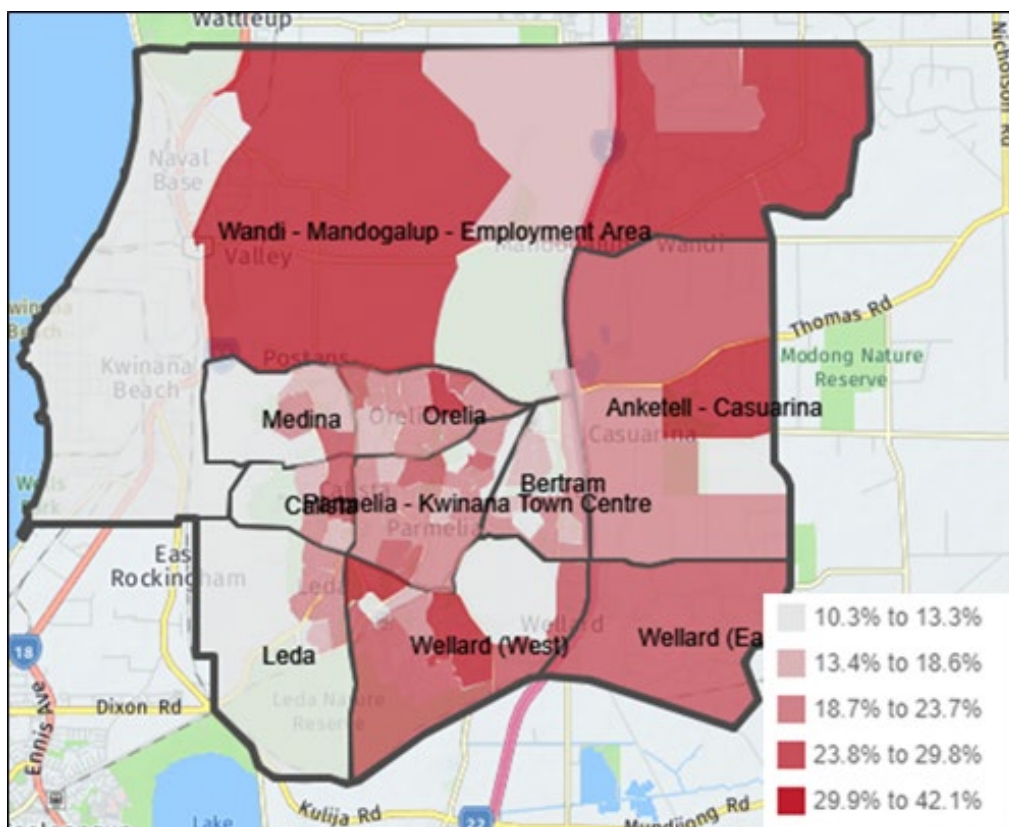


Figure 19: Couples without children



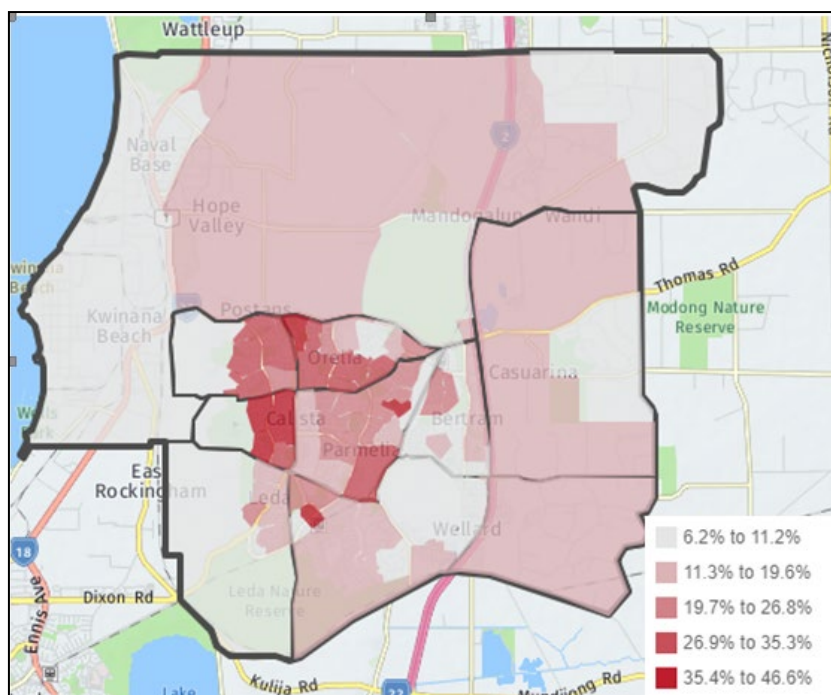


Figure 20: Lone person households

As depicted in Figure 20: Lone person households, 19.6% of the City's total households were lone person households of which the highest proportions were in the City's established suburbs of Calista (39.5%), Medina (31.8%), Orelia (24.8%) and Parmelia (including Kwinana Town Centre) (23.7%).

This shows that the City's established suburbs have a higher number of lone person households which may reflect the settlement history of those suburbs and the life-cycle stage of these residents potentially being empty nesters who now live alone. It may also indicate one parent families, who on census night, did not have dependents living at the home.

Table 4: Forecast household types, shows that by 2036 each household type is expected to increase and generally maintain its household share throughout the City. The largest increase in overall numbers will be couples with children, however the largest percentage change will be couples without children. Further analysis of these household types alongside dwelling structure is provided under 5.1.9 Dwelling structure by household.

Forecast household types	2016		2026		2036		Change between 2016 and 2036	
	Number	%	Number	%	Number	%	Number	%
Couples with children	5,019	34.1	7,844	35.0	10,704	35.7	+5,685	1.6
Couples without children	3,795	25.8	6,050	27.0	8,322	27.8	+4,527	2.0
Group households	471	3.2	665	3.0	786	2.6	+315	-0.6
Lone person households	3,195	21.7	4,513	20.1	5,794	19.3	+2,599	-2.4
One parent family	1,798	12.2	2,627	11.7	3,273	10.9	+1,475	-1.3
Other families	456	3.1	740	3.3	1,106	3.7	+650	0.6

Table 4: Forecast household types

### 5.1.5 Household size

The number of persons per household in the City compared with the South West Group, as shown in Figure 21: Household Size, shows a lower proportion of single, two person and four person households and a higher proportion of households with 3 persons and 5 persons or more which is possibly a reflection of the one parent family and other family household types that are represented in the household types.

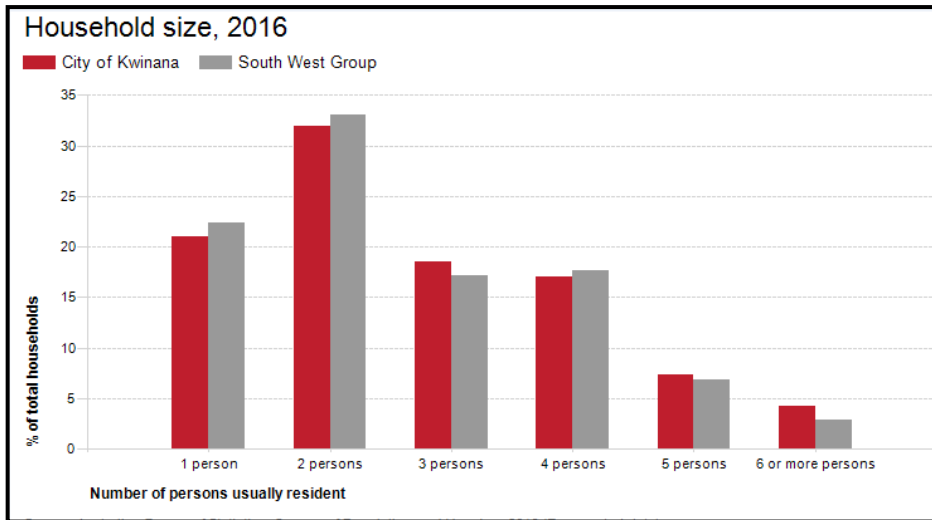


Figure 21: Household Size

Over half the City’s households comprise of 1 or 2 person households. Consistent with the household type discussed earlier, smaller households are typical in the City’s established areas. Figure 22: Average persons per dwelling by suburb shows 2.16 in Calista and 2.20 in Medina with larger average persons per dwelling in newer areas of Wellard and Wandi.

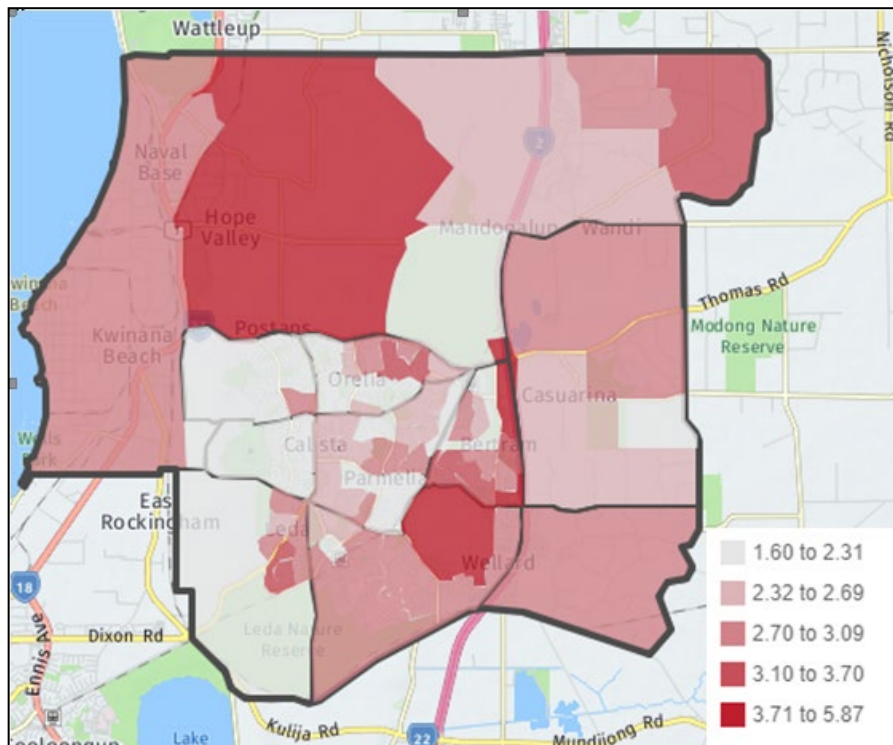


Figure 22: Average persons per dwelling by suburb

Figure 22: Average persons per dwelling by suburb shows the City had an average household size of 2.67 persons per dwelling in 2016 with variation in household size across the City ranging from a low of 2.16 in Calista to a high of 3.11 in Bertram.

A higher average household size can indicate households with more children, larger group households or multi-family households. Smaller household sizes indicate lone person households, less families.

#### 5.1.6 Housing tenure

In 2016, 65.7% of households in the City were either purchasing or fully owned their home, while 20.7% were renting privately. Four percent of the households were in social housing as shown in Table 5: 2016 Tenure type.

2016 Tenure type	Number	Kwinana %	South West Group %
Fully owned	2,106	15.2	27.2
Mortgage	7,009	50.7	40.6
Renting	3,455	25.0	24.2
Renting - Social housing	555	4.0	3.2
Renting - Private	2,860	20.7	20.6
Renting - Not stated	40	0.3	0.4
Other tenure type	69	0.5	0.9
Not stated	1,173	8.5	7.1
Total households	13,812	100.0	100.0

Table 5: 2016 Tenure type

Analysis of the housing tenure of the residents of the City in 2016 shows that rental properties comprised about 25%, similar to the South West Group. Whilst there was a smaller proportion of households who owned their dwelling and a larger proportion purchasing their dwelling, as compared to the South West Group, which is reflective of the emerging suburbs and new development occurring throughout the City.

#### 5.1.7 Housing affordability

Table 6: Median house price presents a comparison of the median market values of houses and units of the City as compared to Greater Perth and Western Australia. At June 2017, the median house valuation in the City was \$161,028 lower than compared to Greater Perth. In percentage terms, the City's average prices are 31 per cent less than that across Perth.

Noticeable also is the decline in values from 2015 to 2017 consistent with that for the Greater Perth and State.

Houses				Units		
Year (at June)	City of Kwinana	Greater Perth	Western Australia	City of Kwinana	Greater Perth	Western Australia
2011	330,515	524,554	497,396	197,286	389,826	385,007
2012	324,248	505,025	478,871	185,757	380,671	372,390
2013	335,932	526,786	496,421	188,829	403,098	392,444
2014	382,453	570,796	537,452	203,535	438,230	427,041
2015	398,081	573,329	534,155	211,453	431,343	422,375
2016	381,180	536,845	500,225	193,090	407,830	392,052
2017	355,263	516,291	479,243	184,864	379,904	367,211

Table 6: Median house price

There is a more significant difference between the average value of units with the City’s average value being less than 50% (48.7%) of the average value of units in Greater Perth in 2017. This percentage appears to have stayed relatively constant since 2011.

The value of houses and apartments in the City of Kwinana are significantly lower than the Greater Perth area. This suggests that Kwinana is, on average, more affordable than many other parts of Perth.

5.1.8 Dwelling structure

In 2016, there were 13,607 (88.9%) separate houses in the City, 1,293 medium density dwellings, and 222 high density dwellings as shown in Figure 23: 2016 Dwelling Structure Types. The total number of dwellings in the City increased by 3,812 between 2011 and 2016 this included 3,219 separate houses and 470 medium density dwellings.

In the City in 2016, 9.9% of the dwellings were medium or high density, compared to 19.6% in the South West Group.

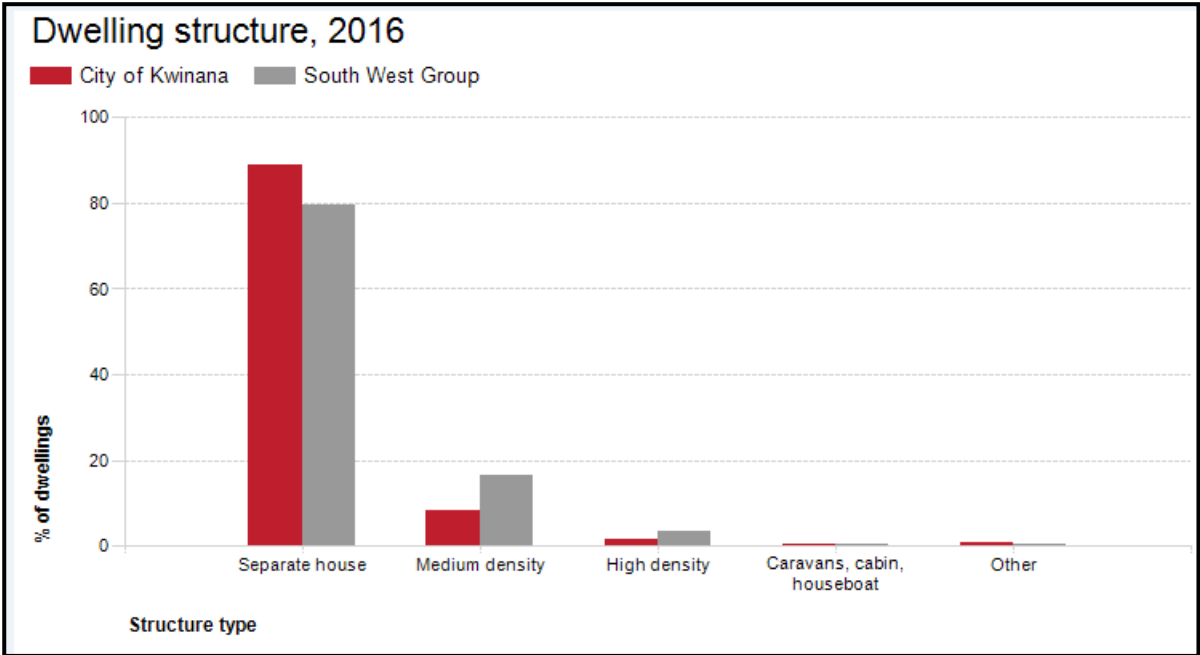


Figure 23: 2016 Dwelling Structure Types

The City has almost 50% less medium and higher density dwellings than the South West Group and a higher proportion of separate dwellings. This may reflect the growth in new suburbs and settlement history but does show a distinct lack of housing diversity within the City.

5.1.8.1 Number of bedrooms

Analysis of the number of bedrooms in dwellings in 2016 as compared to the South West Group shows that there was a lower proportion of dwellings with 2 bedrooms or less, as well as a lower proportion of dwellings with 4 or more bedrooms as shown in Table 7: 2016 Number of bedrooms per dwelling.

2016 Number of bedrooms per dwelling	Number	%	South West Group %
0 or 1 bedrooms	239	1.7	2.7
2 bedrooms	1,034	7.5	10.4
3 bedrooms	5,001	36.2	32.5
4 bedrooms	5,811	42.0	41.6
5 bedrooms or more	594	4.3	5.9

Not stated	1,145	8.3	6.8
Total Dwellings	13,824	100.0	100.0

Table 7: 2016 Number of bedrooms per dwelling

The largest changes in the number of bedrooms per dwelling in the City between 2011 and 2016 were 4 bedrooms (+1,502 dwellings) and 3 bedrooms (+1,068 dwellings) reinforcing the continued construction of this dwelling type within Kwinana.

In 2016, 46.3% of all the dwellings in the City were having four bedrooms or more with Wellard (E) having 69.1%, Bertram 67.3%, Anketell-Casuarina 63.4%, Wellard (W) 60.3%, Wandl - Mandogalup 57.1%, Leda 55.9%, Orelia 34.9%, Parmelia-Kwinana Town Centre 33.8%, Medina 12.8%, and Calista 12.8% as shown in Figure 24: Dwellings with four or more bedrooms 2016.

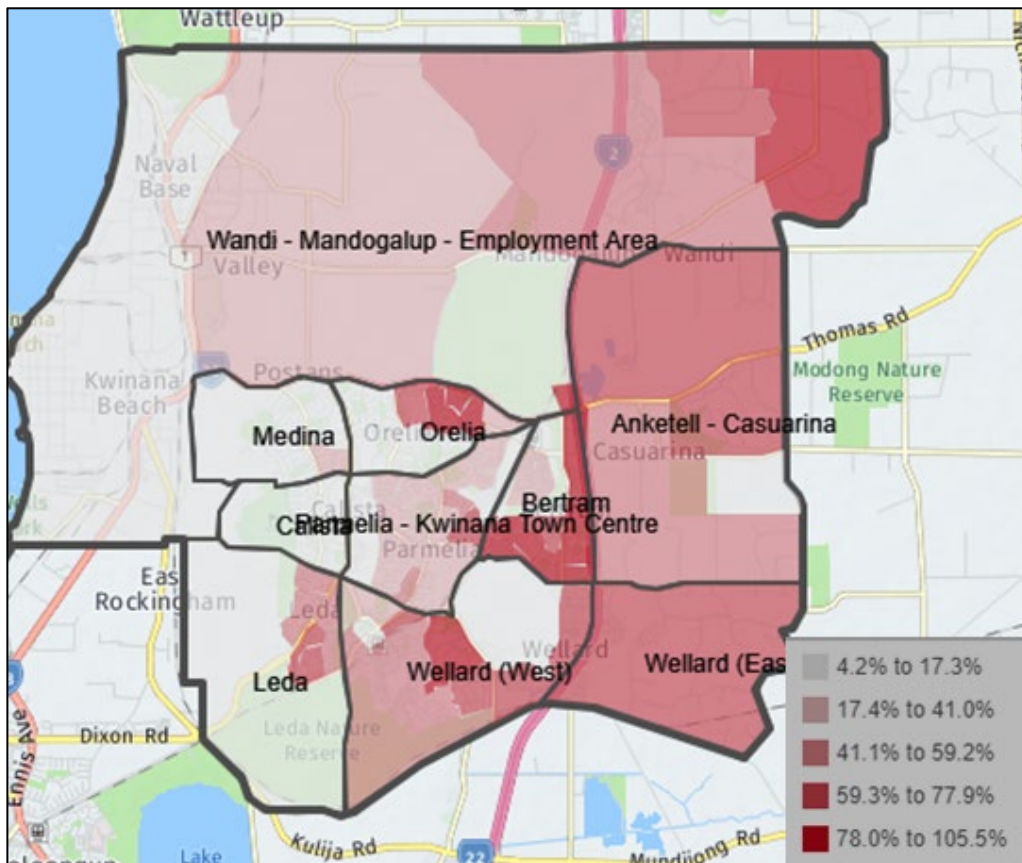


Figure 24: Dwellings with four or more bedrooms 2016

Generally, large dwellings are often occupied by families. A large number of four plus bedroom dwellings may also indicate more recently built housing, as these multiple bedroom dwellings have been more popular in recent decades, which is evident from the data of newer suburbs.

In 2016, 9.2% of the City's total dwellings were classed as two bedrooms or less with Medina having 27.4%, Calista 24.7%, Orelia 16.3%, Parmelia-Kwinana Town Centre 6.5%, Bertram 4.6%, Wellard (E) 3.6%, Wellard (W) 3.4%, Wandl - Mandogalup 3.1%, Leda 1.3%, and Anketell-Casuarina 0% as shown Figure 25: Dwellings with two or less bedrooms 2016.

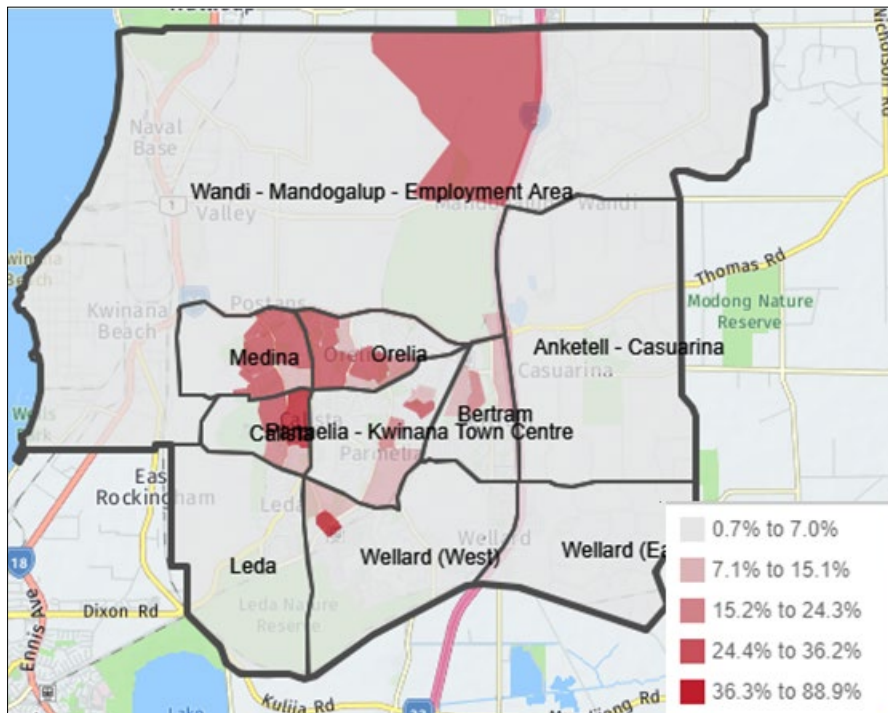


Figure 25: Dwellings with two or less bedrooms 2016

Generally, small dwellings are often occupied by single persons and couples. The data shows older suburbs, excluding Leda, have more dwellings having two bedrooms or less, as compared to newer suburbs. This can also be attributed to the presence of retirement villages and aged care facilities in the older suburbs. It is also noted that the Housing Authority WA manages 228 retirement dwellings within the City, including in the older suburbs.

#### 5.1.9 Dwelling structure by household

Table 8: Dwelling structure by household shows a comprehensive comparison of the number of persons by dwelling structure and number of bedrooms in Kwinana.

The data also shows that single person households occupy more than 2000 dwellings having three or more bedrooms, and two-person family households and two-person group households occupy almost 1,700 four-bedroom dwellings.

This is particularly evident throughout the separate house category which would appear to indicate lone person households, couples without children or one parent families, living in large family homes. In conjunction with analysis of all housing data discussed, it would appear to indicate young couples looking to establish family homes in emerging suburbs, but also older couples without children (empty nesters) and single person households who remain in large dwellings in the established suburban areas, which might indicate the potential unavailability of smaller dwellings for people who are keen to downsize or rightsize.

The data also shows that 181 five-member and 71 six-member family households live in dwellings having only three bedrooms. Similarly, 12 five-member family households live in two bedrooms dwellings, and three five-member and four three-member family households live in one bedroom dwellings. This shows that there are some large family households (multi-generational / multi-cultural) that are living in dwellings that do not have a matching number of bedrooms which indicates a lack of housing choice for larger family households.

Household Type	Single Person	Family Member Households						Group Households					
Dwelling Structure (# Bedrooms)	One	Two	Three	Four	Five	Six +	Total	Two	Three	Four	Five	Six +	Total
<b>Separate house</b>													
One bedroom	64	12	4	3	3	0	20	0	0	0	0	0	0
Two bedrooms	240	201	54	25	12	0	284	27	0	0	0	0	27
Three bedrooms	<b>1,195</b>	1,609	849	549	<b>181</b>	<b>71</b>	3,261	147	16	3	0	0	163
Four or more bedrooms	<b>695</b>	<b>1,553</b>	1,286	1,485	707	432	5,459	<b>121</b>	21	15	3	3	158
Number of bedrooms not stated	101	64	21	24	10	10	128	11	4	0	0	0	13
Total	2,295	3,434	2,219	2,088	905	523	9,152	305	43	17	3	3	367
<b>Semi-detached, row or terrace house, townhouse etc.</b>													
One bedroom	129	9	0	0	0	0	9	3	0	0	0	0	3
Two bedrooms	236	67	10	5	0	0	81	5	0	0	0	0	5
Three bedrooms	139	106	46	31	10	10	198	6	0	0	0	0	11
Four or more bedrooms	4	22	23	14	4	0	64	0	0	0	0	0	0
Number of bedrooms not stated	28	4	0	0	0	0	6	0	0	0	0	0	0
Total	536	213	80	52	18	13	369	14	0	0	0	0	16
<b>Flat, unit or apartment</b>													
One bedroom	0	3	0	0	0	0	6	0	0	0	0	0	0
Two bedrooms	52	73	23	8	4	0	103	23	3	0	0	0	25
Three bedrooms	16	22	10	6	3	0	45	0	0	0	0	0	0
Four or more bedrooms	0	0	0	0	0	0	3	0	0	0	0	0	0
Number of bedrooms not stated	0	0	0	0	0	0	3	0	0	0	0	0	0
Total	68	105	28	20	7	0	156	23	3	0	0	0	25
<b>Other dwelling</b>													
One bedroom	0	0	0	0	0	0	0	0	0	0	0	0	0
Two bedrooms	0	0	0	0	0	0	0	0	0	0	0	0	0
Three bedrooms	5	15	3	4	0	0	18	0	0	0	0	0	0
Four or more bedrooms	9	4	13	9	3	3	29	0	0	0	0	0	0
Number of bedrooms not stated	0	0	0	0	0	0	3	3	0	0	0	0	3
Total	14	20	15	14	7	3	57	3	0	0	0	0	3
<b>Dwelling structure not stated</b>													
Total	9	16	8	0	3	4	28	0	0	0	0	0	4
Total	2,911	3,781	2,348	2,168	939	538	9,762	344	43	17	3	3	416

Table 8: Dwelling structure by household type

### 5.1.10 Residential growth

There has been strong growth in the residential sector of the City over a number of years, due largely to formerly Rural zoned land being rezoned to Urban. This is as a result of both the Kwinana Freeway and the Perth-Mandurah rail line being extended to the City and the City being designated a growth corridor under prior State metropolitan regional strategies.

The land designated for urban development in the City grew from 1713 hectares to 2826 hectares during the 1992 – 2012 period, equating to an average 55.65 hectares each year. In 2012, the City had 740 hectares of undeveloped land still zoned for urban development, this includes land that is zoned urban or urban deferred.

As of 2019, the residential land use in the City has grown to approximately 3,320 hectares.

Residential development is forecast to increase the number of dwellings in the City by an average of 775 dwellings per annum to 30,989 by 2036 as shown in Figure 26: Forecast development trend to 2036.

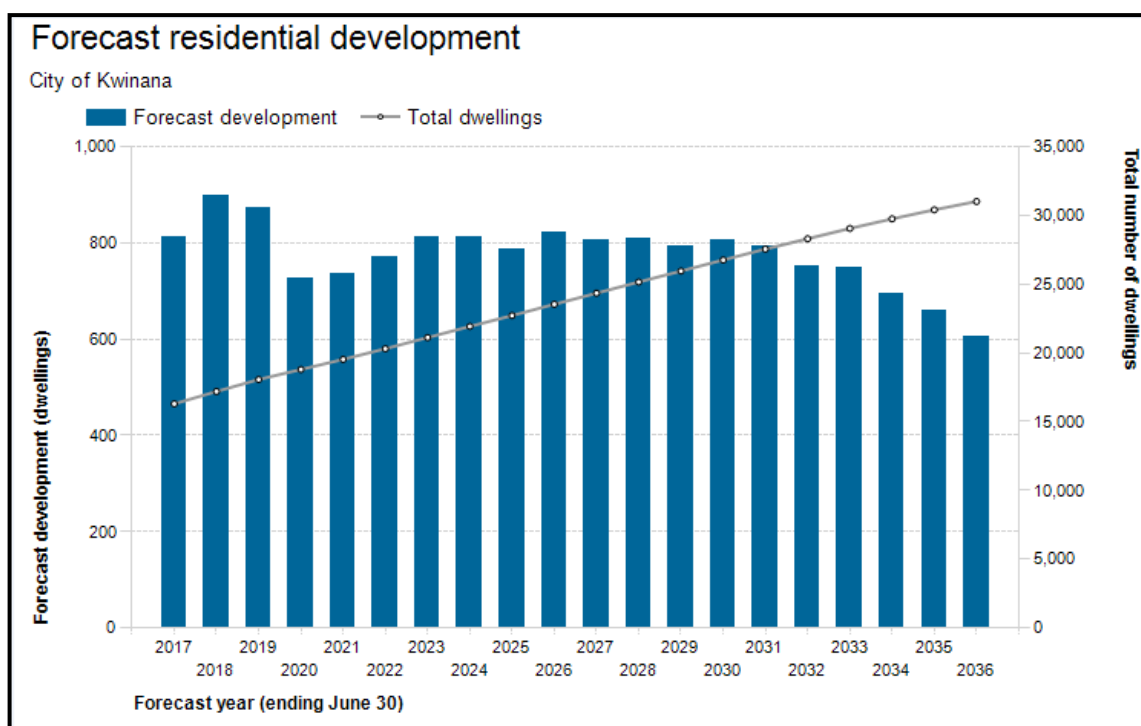


Figure 26: Forecast development trend to 2036

Table 9: Forecast residential development - 2016 to 2036 shows the residential development forecast for each suburb in the City.

Forecast residential development 2016 to 2036		
Area	Change in dwellings between 2016 and 2036	
	Number	%
City of Kwinana	+15,509	+100.2
Anketell	+1,619	+1740.9
Bertram	+275	+12.6
Calista	+106	+11.1



Casuarina	+2,268	+818.8
Kwinana Town Centre	+1,061	+684.5
Leda	+71	+6.0
Mandogalup	+2,408	+3391.5
Medina	+89	+7.3
Orelia	+728	+35.1
Parmelia	+726	+28.1
Wandi	+1,304	+112.9
Wellard (East)	+1,242	+249.9
Wellard (West)	+3,612	+119.4

Table 9: Forecast residential development - 2016 to 2036

Approved Local Structure Plans provide an indication of how many new lots can be created in the future. An analysis, as shown in Table 10: Approved Local Structure Plan lot yield, of the approved structure plans shows the potential to create approximately 11,600 lots in the developing suburbs.

Local Structure Plan	Lot Number/s and Location	Estimated Lot Yield
Anketell North	Anketell North LSP	1,180
Anketell South	Lots 1, 2, 3, & 17 Thomas Road and Portion Lot 13 Treeby Road	420
Mandogalup East	Lots 9002 & 9006 Hoffman Road and Lot 9019 Rowley Road	1,500
Mandogalup West	Lot 682 & 52, Rowley Road.	900
Wandi South	Lots 60 & 61 Kenby Close	56
Wandi North	Lot 313 (651) Anketell Road	1,004
Wellard	Lot 64 (78) Woolcoot Road	188
Wellard	Sunrise Estate - Part Lot 9001 (previously Lot 201) Mortimer Road and Part Lot 379 Millar Road	1,440
Wellard	Lot 661 Bertram Road	198
Wellard	Lot 601 Millar Road, Wellard	265
Wellard	Oakebella Estate - Lots 503-505 507 & 900 Johnson Road Wellard	415
Wellard	Lot 502 Tamblyn Place	65-70
Wellard	Lot 500 & 501 Bertram Road	126
Wellard	Lots 670 & 1338 Bertram Road and Reserve	106
Wellard	Lot 59 Mortimer Road, Wellard	55
Wellard	Lots 90 & 378 Millar Road Wellard	242

Wellard	Lot 900 Tamblyn Road	53
Wellard	Wellard Village	2,205
Wellard	Providence - Lots 167-170, 83, 85, 92, & 1278 Wellard Road, and Lots 1, 10, & 2 Johnson Road in Wellard	770-790
Wellard	Emerald Park - Lots 1, 21, & 500 Mortimer Road and a portion of Lot 9000 Johnson Road, Wellard	425

Table 10: Approved Local Structure Plan lot yield

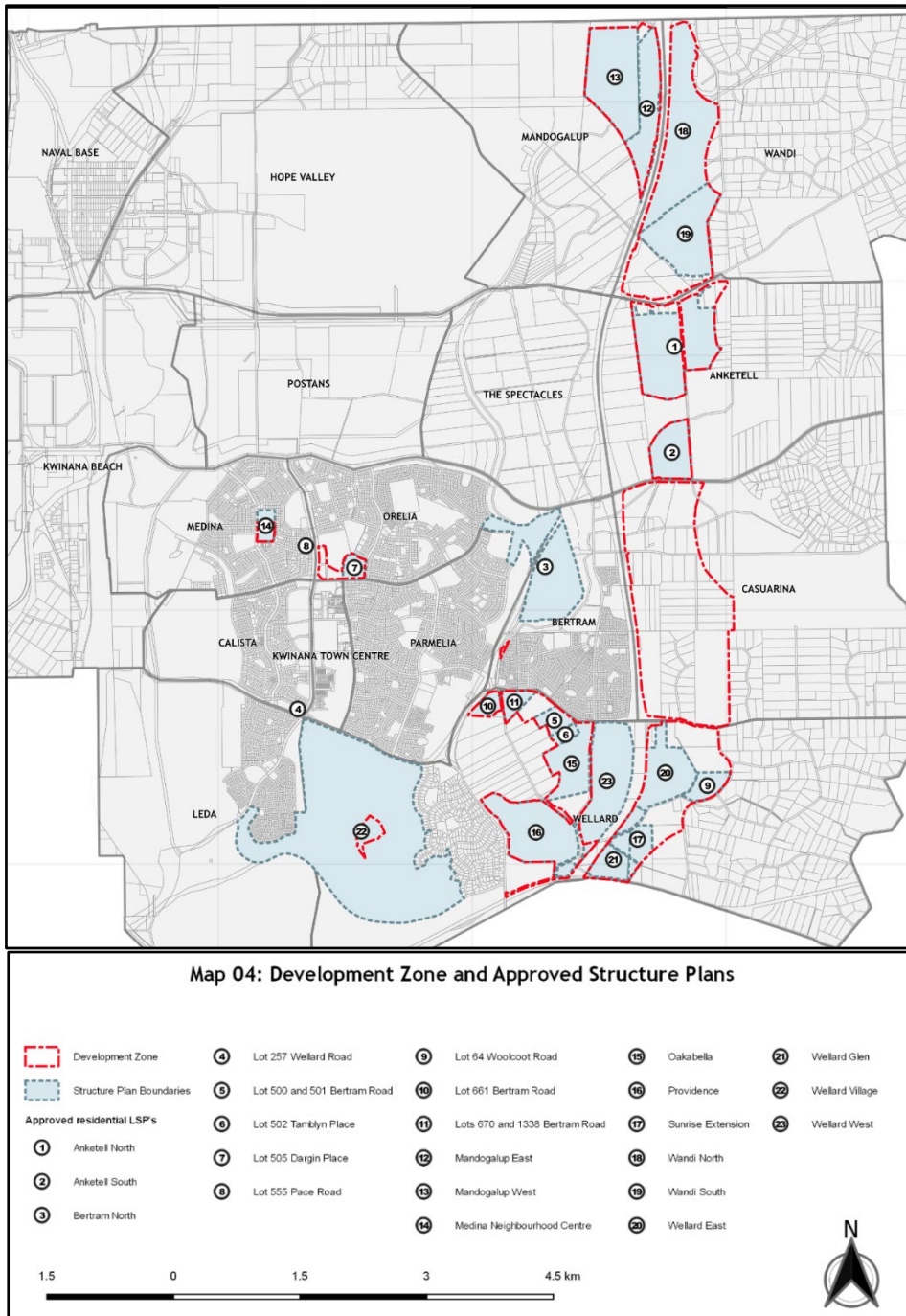


Figure 27: Location of approved Local Structure Plans

Further, a desktop analysis of the structure plans and subdivision plans approved by the City revealed that a dwelling requires an average 500m<sup>2</sup> area, which is roughly in line with the standards required by Liveable Neighbourhoods. The 'average area' includes the lot area, and supporting land area required for road, public open space/s, and school/s. Based on 500m<sup>2</sup> per lot, the City requires 330 hectares of urban land by 2036 to accommodate the forecast dwelling development.

A current analysis of the location of approved Local Structure Plans vs Urban and Urban Deferred zoned land showed that there is still approximately 410 hectares of unplanned and undeveloped land within the City.

This demonstrates that whilst the City will continue to experience a significant population increase and residential growth, its existing structure planned, urban and urban deferred areas are more than capable of providing the required land area to meet this need. It also shows that the majority of this growth will occur within the growing urban areas of the City located mostly east of the Kwinana Freeway and in Mandogalup.

#### 5.1.11 Residential infill

Whilst the City is capable of meeting its population growth needs through the existing expansion of growth areas, the Sub-regional Framework has also set out an infill dwelling target of 1,365 dwellings to be built within the City by 2050. It is expected that this infill target be met within the City's existing suburban areas, where in-line with sound planning principles.

In order to understand how this target can be met, it's necessary to know the current residential yield from existing zoning and density codes that apply within the established suburbs in the City. As shown in Table 11: Distribution of residential lots in the established suburbs there are almost exclusively R20 lots throughout the established suburbs. The exception is Orelia which does have some lots with R25, R30, and R40 codes, although this only accounts for 9.6% of lots within Orelia and 2.3% of all lots across the City's established suburbs.

This further demonstrates the lack of diversity within the City, particularly within the City's established suburban areas.

Suburbs	R20	R25	R30	R40
Medina	1,116	0	0	11
Calista	704	0	0	45
Leda	999	0	0	1
Orelia	1,536	107	15	42
Parmelia	2,559	0	0	8
Total	6,914	107	15	107

Table 11: Distribution of residential lots in the established suburbs

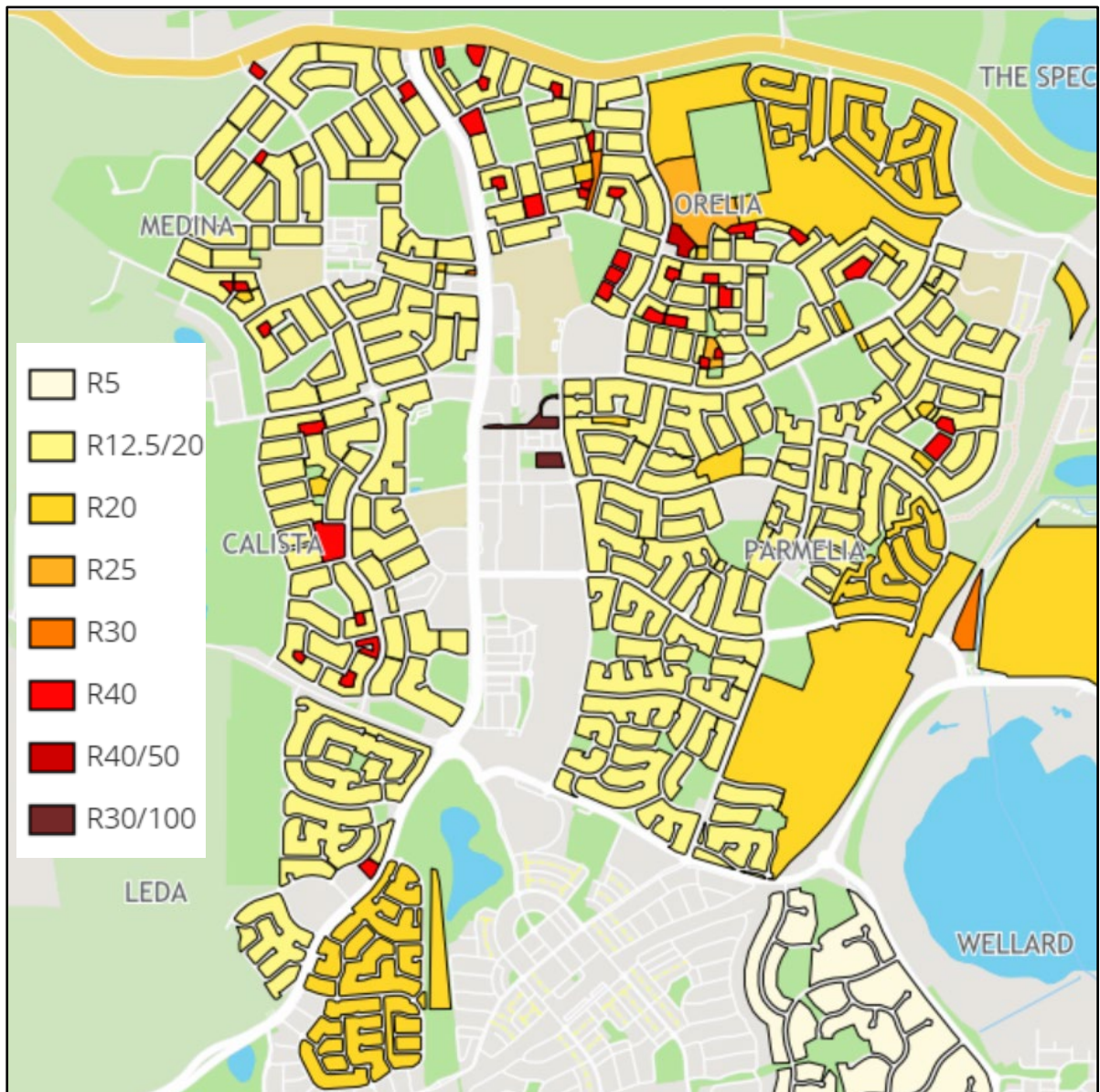


Figure 28: Location of density codes across the City

Analysis of R20 development potential, as shown in Table 12: Size and Distribution of R20 lots, indicates that more than 90% of the R20 lots are less than 900m<sup>2</sup> with only, 506 lots having the capacity for future subdivision.

R20 Lots Size (m <sup>2</sup> )	Medina	Calista	Leda	Orelia	Parmelia	Total
<900	944	548	955	1447	2394	6288
900-1350	140	132	37	61	136	506
1350-1800	15	10	1	4	8	38
1800-2250	9	5	1	5	6	26
>2250	8	9	5	19	15	56
Total	1116	704	999	1536	2559	6914

Table 12: Size and Distribution of R20 lots

However, the development capacity of all lots across all density codes throughout the older suburbs indicates that there are an additional 897 lots (including the 506 R20 sites) that could be created, based on the current local planning scheme zoning provisions.

If the City is to achieve its density target of 1,365 infill dwellings, the City will need to facilitate further infill development of existing suburban areas.

#### 5.1.12 Key issues

The City's population will double by 2050, potentially sooner, with marginal increases in household size. The majority of this population will be located in growth suburbs, and it is important the level of services provided to residents meet their needs and provide the best possible environment for residents to live in.

Associated with the forecast population growth:

- the largest component of household types are couples with children and couples without children;
- one parent families are higher in the City than surrounding areas and will continue to increase;
- lone person households will increase;
- population in the 60+ age category is expected to increase in proportion to the general population; and
- There also appears to be a mis-match between number of people living in a dwelling and number of bedrooms in a dwelling.

This demonstrates a need for the City to continue to provide family homes for a growing population, but to also acknowledge that there are significant portions of the City's community that require different housing needs. This includes the need to continue to provide an affordable housing option within the City and the City will require smaller housing, medium and higher density housing and alternative housing types to meet the needs of a changing community to be provided throughout all areas of the City.

The City also has almost 50% less medium and high density dwellings than the South West Group and a higher proportion of separate dwellings. There is a clear need to diversify the City's housing stock to provide dwellings that cater to the other ends of the housing spectrum rather than just 3-4 bedroom stand-alone dwellings, on single lots.

The Sub-regional Framework requires an additional 1,365 infill dwellings by 2050 and under the City's local planning scheme and current land use zones and densities, there is capacity for 897 dwellings. The City will need to provide additional dwellings but this consideration should not be in isolation to more specific community needs and trends.

In a local government with a high amount of separate dwellings, the challenge will be to identify the right locations for additional housing types and sizes to meet the housing needs of its growing community (i.e. the provision of appropriate, affordable, safe, secure, sustainable, and well located housing) in the context of other planning issues, such as the need to retain the urban character of established areas, maintain the City's high tree canopy cover, the need to support activity centres and to locate housing in areas with good access to public transport.

### 5.1.13 Response to issues

The City will ensure that the housing needs of new residents in growth suburbs are met over the next 30 years through sound structure planning processes that deliver suburbs that meet best practice urban design to ensure a quality living environment for residents not only now, but into the future.

There is a demonstrated need to provide a diversity of dwelling options that accommodate varying household types and family structures which are currently not provided within the City's housing stock. There is a need for more housing options (particularly for more one and two bedroom dwellings) and additional medium and high density development. Affordable housing will also need to continue to be provided within the City. Equally, the City will need to provide alternative housing options that cater for multi-generational families who may not fit traditional housing models.

It is important that the needs of these residents for services such as schools and day care, shopping for a range of goods, access to local and regional transport networks and to live in safe, attractive neighbourhoods is considered.

Accordingly, any changes to housing densities shall be located in areas that are close to existing Activity Centres and the City will seek to strengthen access between local amenities, shops and schools with newly created diverse housing to create liveable communities.

The City will:

- ensure best practice urban design is applied to emerging suburban development;
- increase housing diversity with the walkable catchments of Activity Centres, of Neighbourhood Centre or higher; and
- seek to increase housing diversity with the walkable catchments around Train Station precincts.

## 5.2 Rural living

The City of Kwinana comprises approximately 12,000 ha, of which the rural zoned areas make up approximately 3,520 ha. The City's rural areas are spread across the City facilitating various rural pursuits, however approximately 60% of rural land is located to the east of the Kwinana Freeway.

Under LPS2, rural land is zoned as Rural A (797 ha), Rural B (775 ha), Special Rural (1620 ha) or Rural Water Resource (328 ha), see Figure 30: Rural zones as per LPS2.

### 5.2.1 Rural A

To the east of Kwinana Freeway, along Born Road, Casuarina, there are 23 lots which comprise of an area of 82.14 hectares, with 17 lots being over 4 hectares in area. While there are some businesses such as a market garden, kennels, nursery, machinery workshop, many are largely residential uses. These lots are located within the Peel/Harvey Estuarine System and as such the City will need to manage and minimise water quality impacts on the drainage catchment of the Peel/Harvey Estuarine System.

In addition, these lots are located within a planning investigation area shown in a shaded circle on Figure 31: Sub-regional Framework map extract. Planning investigation areas have been set aside as part of the strategic reconsideration of land use and will be subject to assessment by the WAPC to determine whether any possible change to current zoning is appropriate. These investigations are being progressed by the WAPC and are required to be undertaken prior to any related Metropolitan Region Scheme (MRS) amendment. The findings of this investigation have yet to be made available to the City for consideration.

There are additional Rural A zoned lots in Mandogalup, used for market gardens, quarrying and residential land uses however the eastern portion of Mandogalup is demarcated as Industrial Expansion, as shown in Figure 31: Sub-regional Framework map extract, with the remainder shown Industrial Investigation which is subject to IP47. The City has an adopted Local Planning Policy that seeks to guide development within this locality based on the City's priorities and needs of the community.

The policy identifies a station precinct that advocates for the development of a future train station and complementary commercial uses at Anketell Road along with residential areas and industrial areas separated by a transitional area that could incorporate composite lots that serve multiple purposes.

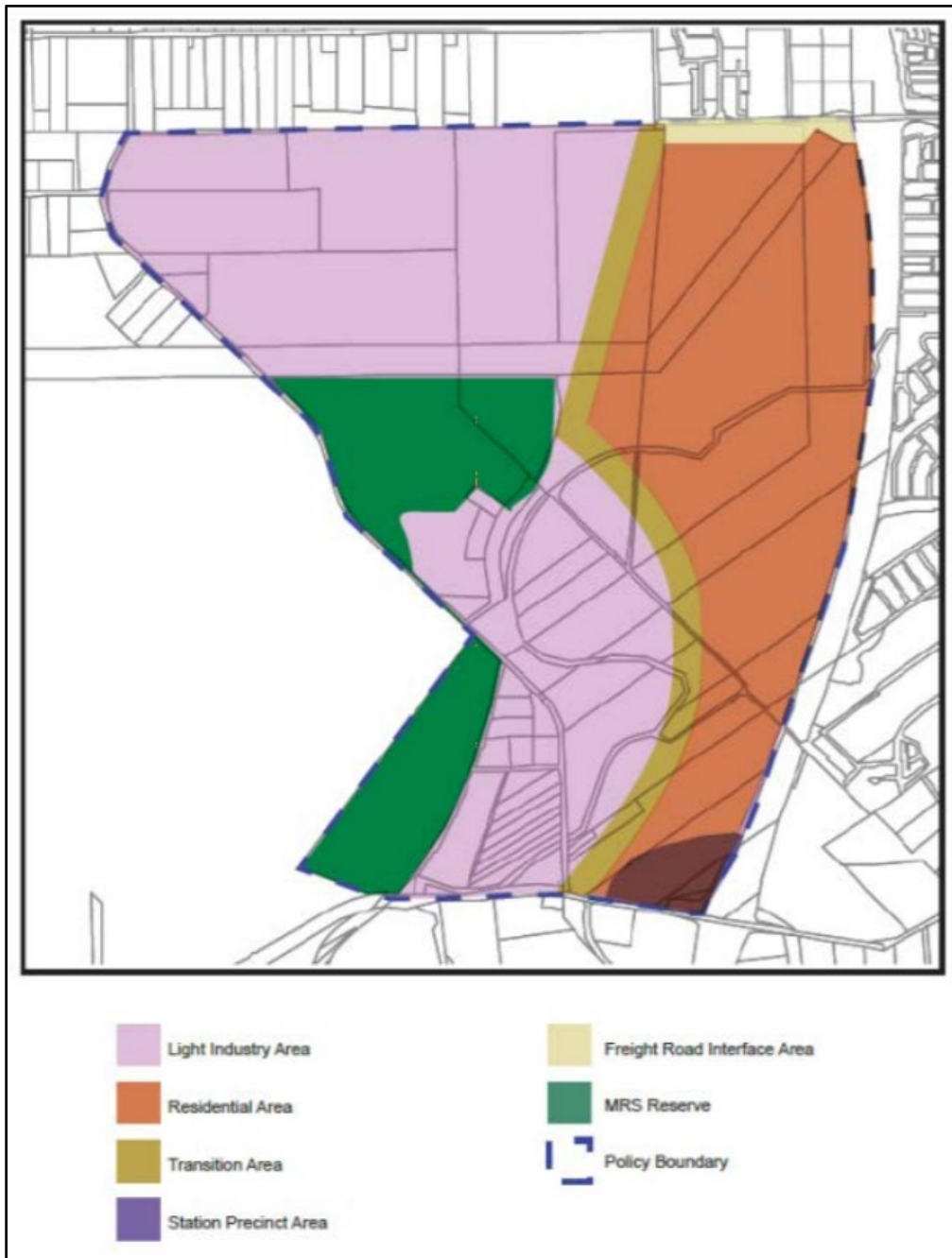


Figure 29: LPP 12 - Mandogalup future development land uses

Whilst the IP47 process will follow its due course, the City’s adopted position is to advocate for the outcome set out in LPP 12 as illustrated in Figure 29: LPP 12 - Mandogalup future development land uses. It should be noted that the IP47 scenarios developed to date provide for varying locations for rural-industrial composite landuses.

The remaining Rural A zoned sites are environmental assets that remain incorrectly classified under the MRS or land near the Kwinana Motorplex that is also shown as Industry or Industrial Investigation.

The future of Rural A lots is unknown and largely dependent on the outcome of planning for the IP47, the long-term implementation of the Sub-regional Framework and associated industrial expansion and planning investigation area and/or the need to rezone land to protect the water quality of major drainage systems.



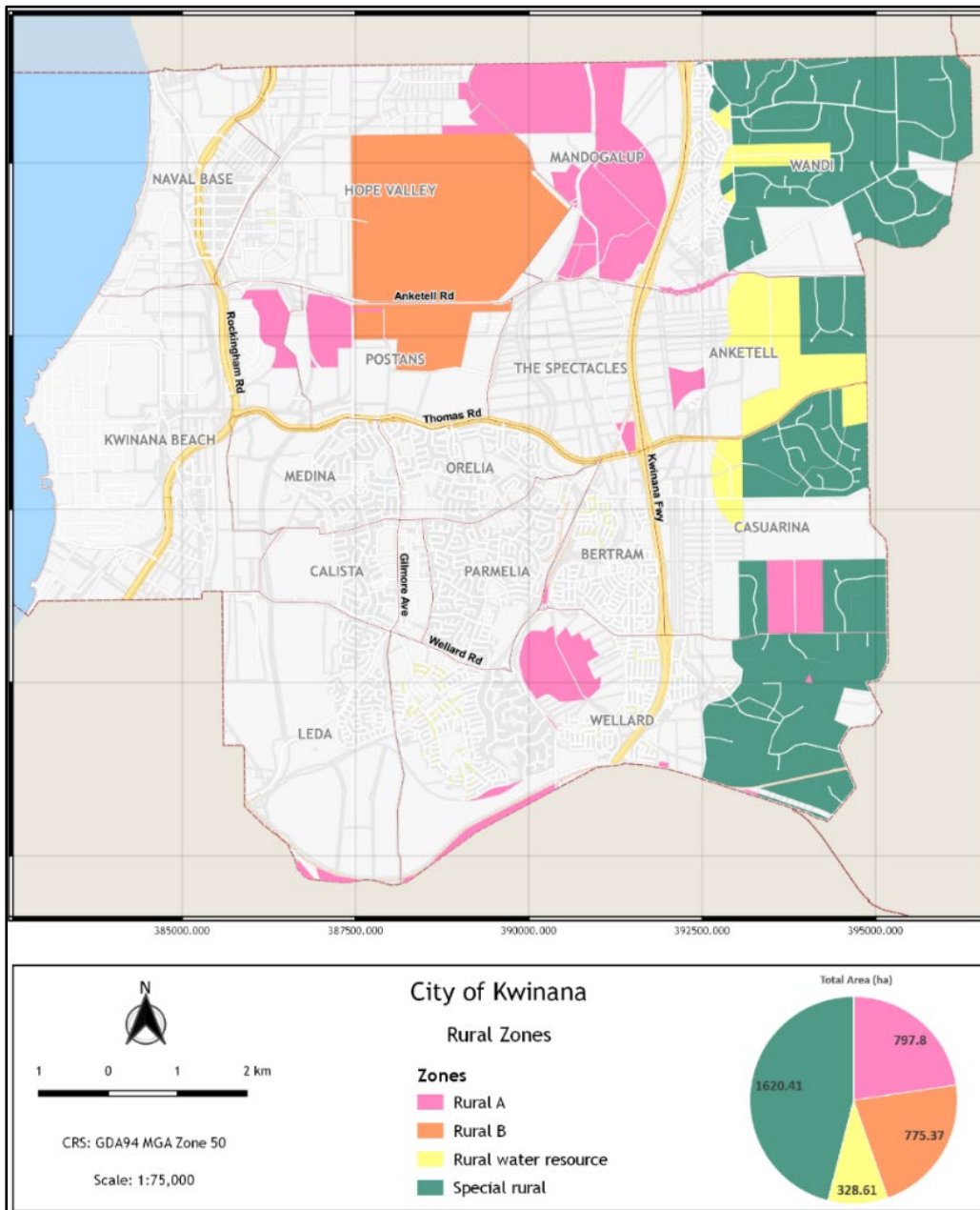


Figure 30: Rural zones as per LPS2

### 5.2.2 Rural B

Rural B zoned area is located to the west of Kwinana Freeway and is used for extractive industries, tailings ponds, processing and other purposes.

The land set aside as Rural B under LPS2 north of Anketell Road, is the Alcoa Residue Disposal Area which is identified in Figure 31: Sub-regional Framework map extract as Industrial, whilst the area south of Anketell Road, the Postans area, is shown as partially Industrial and partially Industrial Expansion.

The application of a rural zone to these land uses and areas may not be appropriate and the City will need to determine how best to manage these land uses through appropriate zoning.

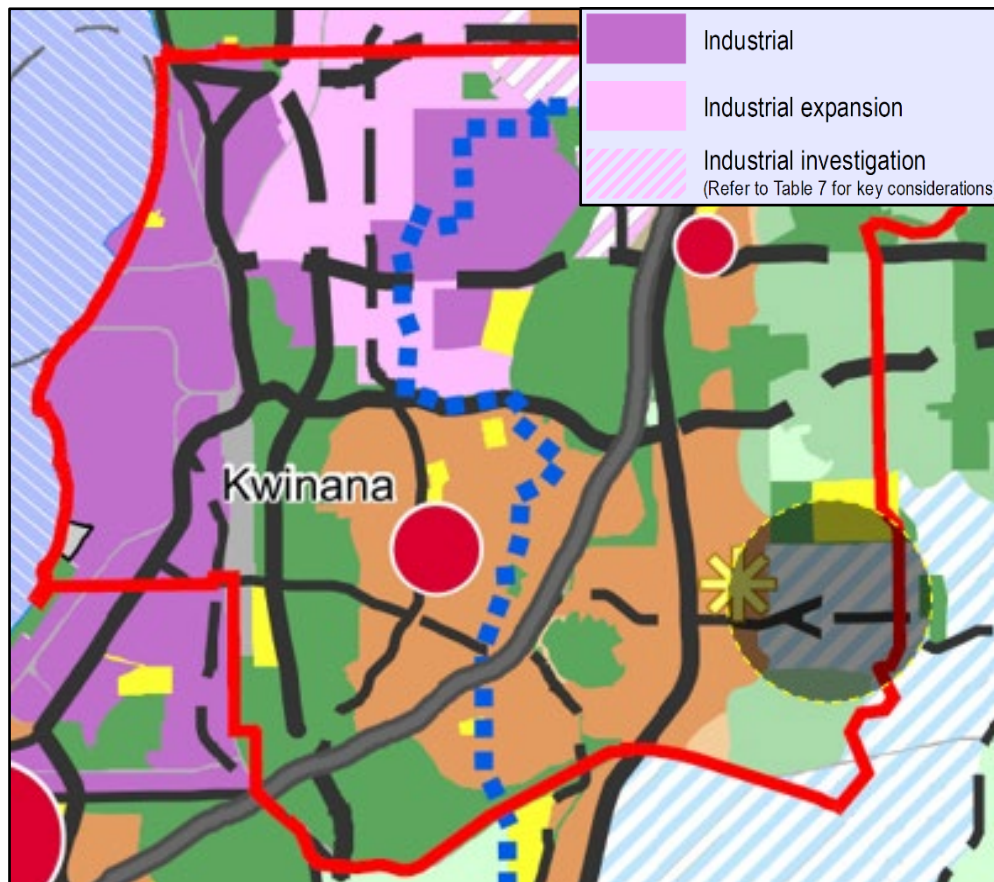


Figure 31: Sub-regional Framework map extract

### 5.2.3 Special Rural zone

The Special Rural zone is located to the east of Kwinana Freeway and is directly abutted by the urban development front. Demand for rural living land is high, particularly given the close proximity to transport infrastructure such as the Kwinana Freeway and railway stations.

The Special Rural area is used predominately for rural living purposes with lot sizes from 1 hectare to 4 hectares which is at odds with the urban development that is occurring along the growth corridor. There is a conflict between the urban and rural interface due to the intensity of urban development that directly abuts large rural living lots. This conflict cannot be controlled within the existing rural living lots and will need to be managed by new urban development.

The Special Rural zone is split into sub-areas each with its own specific controls. As a result there is some variation across the different Special Rural zones, mostly with regard to whether equestrian uses are permitted or not and what mitigation measures are put in place to enable them.

Specifically, Extractive industry and Tailing pond uses are not permitted and most land within the zone is required to preserve Banksia Woodlands. Particular controls exist for building envelopes and some of the principles adopted for development include requiring nutrient retentive effluent disposal systems, preserving remnant vegetation and managing run-off to avoid the impact on wetland and bushland areas.

Provided the land is managed in accordance with those principles the use of the land for rural living purposes is compatible with the Jandakot Water Mound and Peel/Harvey Estuarine System.

#### 5.2.4 Rural Water Resource zone

Rural Water Resource zone applies to those areas over the Jandakot Groundwater Mound which are not otherwise zoned Special Rural, Parks and Recreation reserves or Public Utility. This includes a number of lots along Bodeman Road, Battersby Road and lots along Thomas Road.

Rural Water Resource lots vary in size from 2 hectares in Wandi to over 16 hectares in Anketell. The most common size is about 4 hectares. There are 86 Rural Water Resource lots in total with potential for a further 22 lots through additional subdivision. The main land use of the Water Resource zoned lots is rural living or market gardens, the suitability of which may need to be queried if that land use and its activities cannot be managed to meet the main objective of the zone.

The main objective of the Rural Water Resource zone is to protect and preserve the underground water resource while facilitating rural land use and development. Accordingly, the City will need to ensure land in this area is not subdivided to an extent that it would impact upon the Jandakot Water Mound and thus large lot sizes and retention of existing vegetation to reduce water run-off is paramount in these locations.

#### 5.2.5 Key issues

The Sub-regional Framework has identified the current rural areas to the west of Kwinana Freeway to be set aside for Industrial, Industrial expansion or Industrial investigation, subject to the outcomes of IP47.

The IP47 process will determine the land uses for this locality which will be incorporated into a future Improvement Scheme for the locality. However, Council has adopted Local Planning Policy No. 12 - Mandogalup Future Development which proposes a range of land uses for the Mandogalup area, see Figure 29: LPP 12 - Mandogalup future development land uses, however the primary intent of the policy is to ensure employment related land use is protected within the area and any interface with the existing urban area is managed.

Given the above context, the existing rural areas west of the Kwinana Freeway are likely to be zoned and set aside for industrial land uses. The rural land in Mandogalup is subject to further investigation until the IP47 is resolved. Should the City's LPP 12 position be pursued, there would also be no rural land remaining under that scenario.

A new industrial zone throughout the northern part of the City may be appropriate for some existing rural businesses to operate within, however, it is likely there are agricultural based businesses in those locations that may no longer be suitable within an industrial zone. There are also no identified rural areas within the Sub-regional Framework for agricultural land to be provided within the City.

Separately, there is a need to manage and minimise water quality impacts on both the Jandakot Groundwater Mound and drainage catchment of the Peel/Harvey Estuarine System by controlling land use and intensity of development in these areas.

There are redundant rural zones that applies to land that is no longer suitable for their purpose, particularly as Regional environmental assets. This includes Bollard-Bulrush, a wetland in the suburb of Wellard to the east of the Kwinana Freeway, which is currently zoned as Rural A and should be brought under State responsibility for management as a Parks and Recreation Reserve.

140 Treeby Road, Anketell is a Bush-forever site that also contains a wetland but again still retains a rural zoning which should also be brought under State responsibility for management as a Parks and Recreation Reserve.

With the rezoning of large areas of previously rural land east of the Kwinana Freeway for urban development, the interface between the urban and rural areas is in conflict.

The introduction of urban development into previous rural areas brings with it potential impacts associated with increased traffic, noise, intensity of development and loss of amenity. This interface requires a transition between the intense urban development and rural communities, but there is no ability for it to be treated within the existing rural areas. New development, therefore, must respond to this interface issue.

#### 5.2.6 Response to issues

The City will retain the existing rural living areas, advocate for its retention through any future planning investigation and seek to expand it, where appropriate.

The City will review the appropriateness of rural land uses within the Peel / Harvey Estuarine System and Jandakot Water Mound and if necessary rezone to remove inappropriate zones. This may provide an opportunity to reduce impact on the water sensitive areas whilst providing for an expansion to the rural living area.

The City will require new development that abuts an existing rural area to incorporate a transition to mitigate the negative impacts of urban development alongside rural living areas.

The City will seek to rationalise MRS zones where it is appropriate to Reserve sites for their environmental value.

The City will need to assist rural businesses should they seek to continue to operate within a new industrial zone or seek to relocate.

### 5.3 Character, Identity and Urban Design

The City of Kwinana has a rich history containing many sites significant to Aboriginal and non-Aboriginal culture. Recognising, respecting and celebrating aspects of our past that we value - a site which has played an important part in our history, and buildings or places which are special because of their aesthetic, historic, scientific or social significance - is a way of adding meaning to place, helping people to connect to and value the places they occupy, and to learn from the past.

#### 5.3.1 Aboriginal heritage

The City of Kwinana is located in the traditional lands of the Nyoongar people. Important Nyoongar camping, hunting and gathering sites include Sloan's Reserve, Chalk Hill and The Spectacles. The area surrounding Chalk Hill is part of a trail of fresh water lakes and natural springs running along the greater metropolitan coastal strip. Nyoongar people used Chalk Hill as a way of signaling their location to others, through the lighting of fires. According to Dreamtime stories, the white chalk found on the hill represents the beards of the elders.

Simon Gentle, a Ballardong man, was the first ever employee of the Kwinana Road Board in the early 1950s. Several Aboriginal families camped at Chalk Hill and Thomas Oval in the 1950s and 1960s. While Aboriginal people were employed by the council and on the industrial strip, they were not initially eligible for state housing. Following campaigning by Kwinana Road Board Councillors, the Walley family were amongst the first Aboriginal people in Western Australia to be allocated state housing.

Within the City of Kwinana there are four registered Aboriginal Heritage places, as described in Table 13: Registered Aboriginal sites.

Registered Aboriginal Site ID	Name	Type
3710	Thomas Oval	Camp site
3711	Sloans Reserve	Camp site & Hunting place
3534	Sloans reserve artefacts	Artefacts / Scatter, Arch Deposit
3568	Wally's camp (Kwinana Fwy/Millar Rd)	Camp site

Table 13: Registered Aboriginal sites

In addition to the four registered Aboriginal Heritage sites there are at least 12 further sites listed as having Aboriginal heritage significance. This strong and proud Aboriginal connection to the area should be recognised and celebrated in appropriate ways through the planning framework.

The oversight of the Department of Indigenous Affairs and the application of the Aboriginal Heritage Act 1972 will ensure that significant aspects of Aboriginal history and culture, relating to the period before European settlement, are recorded and preserved.

#### 5.3.2 European character

European presence in Kwinana can be traced back to 1829, to the ill-fated Peel Estate attempts to farm the surrounding districts. Many local families can trace their history back to a second wave of migration that occurred in the 1920s under the Post World War One Soldier Settlement Scheme and Sir James Mitchell's Group Settlement Scheme. Most of the farms were abandoned by the 1930s, however cottages named in honour of the Thorpe, Sloan, Smirk, Key and Mead family still stand today.

Kwinana as we know it today has its origins in the 1950s, when the WA Government entered into an agreement with the Anglo-Persian Oil Company (now known as BP) to construct an oil refinery on the shores of Cockburn Sound. To complement this industrial development, the State Government

appointed WA's first qualified town planner, Margaret Feilman, to prepare the plans for Kwinana to accommodate the refinery's workforce and their families.

The first suburb of the new Town was Medina which was opened up as a state housing area and most of the families that moved in were fresh migrants of Anglo Celtic and European descent. The suburbs of Calista and Parmelia soon followed. Many of the City's suburbs take their names from ships and early settlers. Most homes were built of timber frame and fibro, with approximately 10 percent of the houses built of brick and reserved for occupancy by managerial and professional employees. While some were dispersed throughout the neighbourhood, a concentration of brick houses on a hill, above the average elevation of the rest of the community, still exist in some capacity.<sup>6</sup>

During World War Two, Gentle Road, Medina was the site of a military personnel camp, housing Royal Australian Air Force officers who manned the Radar Detention Huts on Wellard Road. The huts were an integral part of Australian's coastal defence during the war, used to detect the approach of enemy vessels. The huts can still be visited today as part of the Kwinana Loop Walking Trail.

Residential development also started in other suburbs of Orelia, Parmelia and Leda in the following decades and by 1990, the character of these suburbs was established.

The City of Kwinana has a unique place in the history of Western Australia, as the site of some of the earliest land grants in the Swan River Colony, as well as being the largest purpose-built and planned town to serve industry in Western Australia. The historical narrative of Kwinana is captured through its existing Local Heritage Survey and Heritage List.

#### 5.3.2.1 Local Heritage Survey and Heritage List

The City's current Local Heritage Survey (LHS), adopted 2007, will need to be reviewed in accordance with the requirements of the *Planning and Development Act 2005*, *Heritage Act 2018*, SPP3.5 and the *Heritage of Western Australia Act 1990*.

In 2015, the City engaged a heritage consultant to undertake a minor review of the LHS to bring the inventory up to date, however at that time no new nominations were considered for inclusion in the LHS. Table 14: Places listed in 2015 Review of MHI details all of the places reviewed and maintained during the 2015 review of the LHS.

Place Name	Address
Key Cottage	Part Lot 49 Wellard Road
Wheatfield Cottage	Kwinana Golf Course, Wellard Road
Lealholm	Lot 158 Mandurah Road
Sloan Cottage	Sloan Reserve
Postans Cottage	2 Hendy Road
Smirk Cottage	Beacham Crescent
First Municipal Roads Board Office	156 Medina Avenue
Mandogalup Post Office	Lots 77, 78 & 79 Anketell Road
Paradise Cottage	Kwinana Marshalling Yards, north of Tasker Road

<sup>6</sup> MacLachlan I. and Horsley, J., (2015), *New Town in the Bush: Planning Knowledge Transfer and the Design of Kwinana, Western Australia*, Journal of Planning History 2015, Vol. 14(2) 112-134.

Pines Cottage	Kwinana Marshalling Yards, north of Tasker Road
Radar Detection Huts	Wellard Road
Medina Maternity Hospital & Nurses' Quarters	35 Kenton Way
Soldier Settler Homes	Nos 27, 31 & 45 Mandogalup Road
Hazel Glen Farm	Lots 54 & 60 De Haer Road
Post Master's Residence – Wellard	758 Wellard Road
Medina Shopping Centre	Pace Road
Sutton Farmhouse	Lot 66 Bertram Road
Kwinana Signal Box	Kwinana Marshalling Yards, Rockingham Road
Medina Primary School	Medina Avenue
Anglican Church (fmr)	Cnr Medina Avenue & Hoyle Road
Medina Town Hall	Pace Road
The Spectacles Wetland	Spectacles Drive, Thomas, Anketell and McLaughlan Roads
Hall Reserve – Mandogalup	Cnr Mandogalup Road & Anketell Rd
Wells' Park	Kwinana Beach Road & Rockingham Beach Road
Long Swamp	Lot 339 Hope Valley Road
Leda Nature Reserve	Millar Road & Wellard Road
Wandi Nature Reserve	Anketell Road & Lyon Road
Harry McGuigan Park	Reserve 24172, Cnr Medina Avenue & Hoyle Road
Lake Wattleup/Sayer Rd Swamp	Lots 3 and 500 Mandogalup Road
Wellard Swamp/Bollard Bulrush Swamp	Bertram Road
Medina Avenue Trees	Medina Avenue
Chalk Hill Lookout	Chalk Hill Road
S.S. Kwinana (Shipwreck)	Wells' Park, Kwinana Beach Road & Rockingham Beach Road
Barber's Bridge	Lyon Road
Jolly's Bridge	Mandogalup Road
White Bridge	Anketell Road
Mandogalup Townsite	Mandogalup Road
Tramway Reserve	Near McLaughlan Road, Bertram Road & Wellard Road
Armada-Rockingham Road	Near Gentle Road, Sulphur Road, Durrant Avenue & Johnson Road
Nine Mile Dumps	Near Johnson Rd & Thomas Road
B20 Anglican Church (fmr)	Upgraded from D to C

Table 14: Places listed in 2015 Review of MHI

The following place has been listed on the State Register of Heritage Places since the review of the LHS and will need updating accordingly.

Lealholm	Nb: management category already A
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The City is currently undergoing a review of its draft 2015 LHS and will be seeking to adopt a resolved LHS in 2021. A new LHS will inform the City's future Local Planning Scheme Heritage List and any Character Areas that may require identification for further study and protection.

### 5.3.3 Identity & Urban design

The City of Kwinana is a planned satellite community that was developed in the 1950s, some thirty kilometres south of Perth. A planned new town to accommodate an oil refinery's labour force, commenced in 1953 and has been described as the most interesting and progressive experiment ever attempted in Australian town planning. Margaret Feilman was the Perth-based architect and professional town planner selected to plan Kwinana new town and to coordinate the state agencies involved in the project<sup>6</sup>.

The planning for Kwinana embodied the 'British new town style' of designing communities so that local shops and community facilities are within a walkable distance, creating sustainable communities. Feilman also retained major bushland and landscape features and described the town as "hewn out of virgin bush" and that her goal was "to integrate an essentially Australian town in a particularly attractive Australian landscape"<sup>7</sup>.

Kwinana's townsite was planned for the Swan Coastal Plain, some four kilometres inland from the beach, shielded from refinery vistas, emissions and odours by a ridge of relict sand dunes striking north-south. The townsite straddled the valley between a pair of sandy ridges that paralleled the coastline and were vegetated by indigenous sand-tolerant species such as grass trees and banksias in open-canopy tuart woodland. Gilmore Avenue, the main north-south road in the valley bottom between the two ridges, was planned to divide the new town in half. Medina and Calista, the oldest neighbourhoods, were sited on the western ridge while Orelia and Parmelia were developed later on the eastern ridge, further removed from the coast.

Feilman planned Medina as the first suburb of Kwinana and employed the new town planning theory to the design through the use of curved streets following contours, the inclusion of a number of short loops and cul-de-sacs, the avoidance of over-long streets, the carefully considered intermixing of varied housing types, and the subtle use of small open spaces and tree masses to achieve variety in layout. Housing was surrounded by indigenous trees and from almost any vantage point in Medina, the sights and smells of the Australian bush landscape are still ubiquitous.<sup>6</sup>

The town evolved as a series of neighbourhoods or communities that were surrounded by open space and also by the limited-access roads right through the centre, where the concept of the neighbourhood, the community, is as free as possible from through traffic and to have its own local facilities, if possible, within approximately a 1 kilometre maximum walking distance, an average of about five minutes walk.<sup>8</sup>

Feilman's commitment to environmental conservation in the design of Kwinana is still evident almost seventy years after the area was first developed. Kwinana continues to project the planning principles of creating walkable sustainable neighbourhoods within bushland and the natural landscape and this forms an important part of its identity. Of all the environmental initiatives in the design of Kwinana, the

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<sup>7</sup> Kwinana New Town, (1955). *Town and Country Planning*

<sup>8</sup> Feilman, M., (1991). *Kwinana, from the Limestone Up: Kwinana's People Talk about Their History*



preservation and planting of trees, both indigenous and exotic, must be considered among the most successful. This is evident through the significant mature street trees that have been retained throughout the decades and give Kwinana a leafy green identity.

Residential development in Medina and Calista started in the 1950s with other suburbs of Orelia, Parmelia in the following decades as shown in Figure 32: History of residential development in Kwinana. These suburbs have all followed the original urban design, as set out by Feilman, and retain the new town layout and identity.

Between 1985 and 1995 Parmelia was extended to the east and Leda was developed and these suburban areas comprise low density housing on large lots in a curvilinear street layout with low levels of connectivity with cul-de-sacs off local collector streets. They fit same street design and layout of the original suburbs of Kwinana but lack some of the environmental initiatives such as retained natural bushland throughout the suburb and the connectivity through landscaped walkways.

The last 25 five years have seen the extension of the Kwinana Freeway providing new access opportunities to the City of Kwinana, particularly benefiting the growing locality of Bertram. Bertram was planned in the mid-late 1990’s and developed throughout the 2000’s prior to the adoption of Liveable Neighbourhoods and as such the suburbs design is distinctly lacking the retention of any natural vegetation or pedestrian footpaths beyond the main thoroughfares.



Figure 32: History of residential development in Kwinana

The \$2 billion Perth to Mandurah railway was announced, constructed and officially opened in December 2007. This greatly benefited the South West Corridor of Perth, and at a local scale provided stations at Wellard and Thomas Road within the Town. The decision to route the railway through the eastern parts of the City was particularly the result of the hard work and persistence of the Council, who were initially faced with a proposal to route the railway down the newly extended Kwinana Freeway potentially bypassing the Town.

The development of Wellard Village around the Wellard station and Belgravia Central around the Thomas Road station where deliberately planned and developed as Transit-Oriented-Design developments and are now seen as leading examples of transit orientated design within the country, and are testament to the hard work of the Council and developers at the time.

Wellard station precinct provides the most developed example of integration based around walk-on patronage. A higher density mixed-use development, the bulk of residential component was developed well in advance of retail or commercial development however the intention to create a 'main street' shopping centre linking to the station has come to fruition. The density of development is also of interest, with the aim of achieving 11 dwellings per hectare (gross) compared with the metropolitan average of 6 dwellings/hectare.

Wandi, in the City's north, has also been planned and developed around the same time, albeit to a different urban form. Wandi responds not to a transport based design impetus, but instead to the environmental assets that are located throughout the suburbs. There are numerous wetlands located in Wandi that have been preserved as part of the development and are now incorporated into the various public open space areas throughout the area. The retention of established existing vegetation is also a feature in Wandi with Paperbarks being retained in road verges, median strips and local parks.

The City contains a large area of rural living lots that are located to the east of the freeway but outside the current urban development front. Located throughout the majority of Wandi, Anketell and Casuarina these lots are generally 2 – 4 hectares in size, although some lots are up to 10 hectares in size and provide a rural lifestyle choice to the community that is relatively well sought after. These lots are usually permitted to clear a defined building envelope but are otherwise encouraged to maintain the natural bushland on site. These rural living areas account for a significant area of the City, and they contribute to the City's character and identity as a locality that can offer a residential living options within a natural environment setting.

As a result, the City has a distinct group of established suburbs, being Medina, Calista, Leda, Orelia and Parmelia, developing suburbs, Bertram, Wandi and Wellard and rural living areas of Wandi, Anketell and Casuarina.

#### *5.3.3.1 Established suburbs*

The City's established suburbs have a greater emphasis on social, economic and environmental aspects as urban villages that reduce dependency on private vehicles due to an urban structure that is based on walkable, tree lined, mixed-use neighbourhoods with interconnected street patterns to facilitate movement and to disperse traffic. Daily needs are generally in walking distance of most residents and with this design, more people will actively use local streets, enhancing safety. Local employment opportunities are facilitated in the town structure, providing the community with a firmer economic base and enhancing self-containment of neighbourhoods.

These suburbs also retain a spacious, green and leafy character, including views of backdrop vegetation between and over buildings, generous front garden setbacks open to view from the street. There is less emphasis on separate definition of public and private domain with front gardens often not fenced.

However, in the established suburbs of the City, a significant number of dwellings are in poor condition as the homes have now passed their designed life expectancy and require significant renovations or reconstruction. A broad analysis of the residential environment of the suburbs was undertaken for the City's 2009 Local Housing Strategy (more than 10 years ago), which identified the following:

#### Medina

- Construction from 1950s to 1970s;
- Almost 60% of the dwellings constructed from fibro-clad materials, the remainder brick-cladded;
- Houses reaching the end of their serviceable life;

- Almost 90 % of all dwellings in Medina are either in 'very poor' condition requiring serious repair or in 'poor' condition requiring some repair; and
- Asbestos and fibro-cement roofing of about 10% of the dwellings hazardous, and pose threat to health and well-being.

#### Calista

- Construction from 1960s and 1970s;
- Predominantly brick and tile construction; also fibro-cladded dwellings;
- Houses reaching the end of its serviceable life;
- 10% of the dwellings are in very poor or poor condition, however approximately 75% of it do require some form of repairs; and
- Asbestos and fibro-cement roofing of about 10% of the dwellings are hazardous and pose threat to health and well-being.

#### Leda

- Construction started in the late 1980's;
- Housing style reflects contemporary building design and practice;
- Predominant construction material brick or masonry;
- Majority of dwellings have a tiled roof, with 17% being metal; and
- Overall condition good

#### Parmelia

- Significant dwelling development in the 1970s and 1980s;
- Predominant construction materials brick and tile;
- About half of all dwellings are of good or average condition; and
- Small proportion of dwellings constructed with asbestos or fibre-cement sheeting.

#### Orelia

- Public housing since 1952; then rapid growth took place from the mid-1960s into the mid-1970s;
- New dwellings from mid-90s, particularly the Windsor Hills Estate in the north;
- All dwellings constructed of brick, with 86 percent of dwellings having tiled roof;
- Almost 34% of new dwellings in the Windsor Hills Estate constructed with metal roofs; and

Almost 49% of all dwellings are in good condition, 47% in average condition, and 3% in poor condition. Some suburbs such as Medina, Calista and Orelia are over 50 years old and many dwellings in these suburbs have not been maintained and are in poor condition.

In some areas infill development has occurred and commonly, subdivision of these lots has resulted in battle-axe development, where a dwelling is constructed behind the main dwelling with a long narrow strip of land forming a driveway. Whilst this subdivision form does retain the original dwelling, this incidence of additional driveways and crossovers has resulted in the loss of mature street trees. There are also instances where a three lot subdivision has been carried out. In these cases, the retention of street trees and canopy cover can be a significant challenge.

Some pre-existing consistent characteristics have been established through simple yet effective design elements, such as:

- Limited front fencing or open fencing.
- Buildings well set back from the road reserve.
- Porches and verandas on the front of the dwelling.
- Street trees in road verges.
- Elevated well-ventilated housing due to the use of stumps.
- Minimal intervention with the natural topography.
- Pitched yet simple roof typologies.

There is a current Local Planning Policy that intends to ensure that new development will enhance the City's character, generally facilitate a higher quality form of development and ensure a distinctive, diverse and yet harmonious urban environment. The policy sets out subdivision lot layout preferences and design advice for existing dwellings, where retained on a lot.

The policy seeks to reinforce the connection between new development and existing character by maintaining façade treatments such as porches and verandas that are consistent with older original housing forms in the area. Improvements to the quality of front gardens and landscaped areas are also encouraged to create a transition between the home and the street.

Key to implementing the urban design aspects as outlined in the existing policy provisions is the need for future subdivision design to respond to the layout of the original suburb design. It may be necessary to elevate policy controls into the Scheme in order to ensure the character of the area is maintained.

#### 5.3.3.2 *Developing suburbs*

The design of the developing suburbs has resulted in new localities with their own identity. The starkest difference is the tendency of modern developers to adopt a bulk clearing and cut and fill approach to land development.

In some instances the City has seen significant areas of land cleared and earthworks undertaken that has resulted in wholly different landforms requiring substantial retaining, sometimes with materials not found in the local area. This has resulted in a lack of retention of existing vegetation and therefore biodiversity, a loss of landscape and landform and little connection to the site context or area.

Given the City's strong historical character and identity that is evident throughout its established suburbs, there is a need to ensure that developing suburbs also respect the identity of the area and that structure planning processes and subdivision design respond to the local context.

#### 5.3.4 *Key issues*

- Whilst Aboriginal heritage is principally managed by State and Commonwealth legislation there are opportunities to recognise cultural heritage and the role it plays on the City's identity.
- The City's Local Heritage Survey has reached its review period and should be reassessed. Any review should inform a new LHS and heritage list for the scheme.
- The City has a rich history, with Medina and Calista in particular being icons in the planning of Perth's suburbs as a carefully planned community that was not simply a part of suburban sprawl. This original character should be recognised, retained and enhanced.
- It is also apparent that the oldest suburbs of the City have an aging housing stock, a significant portion of which is in relatively poor condition simply because of age.
- Therefore, facilitating redevelopment of well-located housing stock in the older suburbs of the

City, whilst maintaining the unique character of the early planning, maintaining quality streetscapes and trees and encouraging attractive and appropriate built form, will be key in shaping the City's identity over the next decade.

- There is also a need to review and improve upon urban design and site planning to achieve better outcomes for newly developing areas. Current practices are not delivering communities that respond to the site context.

#### 5.3.5 Response to Issues

The City should recognise the strong cultural heritage associated with Kwinana and seek to preserve and celebrate, where appropriate, the influence of culture on the identity of Kwinana.

The City is reviewing its Local Heritage Survey and shall complete that project in time for it to influence the development of a heritage list or other policy or scheme provisions as necessary.

The City shall consider how best to intercede in redevelopment of established suburbs to ensure the best outcome possible and that infill areas are revitalised purposely rather than left to market forces. The City already has policies in place to seek to retain some of the unique and beneficial character of the original suburbs and this policy should be reviewed, expanded and its controls reconsidered for elevation into the Scheme, to ensure greater influence over development outcomes and improve community benefit.

The City should introduce requirements that can be applied in developing areas to ensure structure planning and subdivision design responds to local context and incorporates natural landscape features.

## 5.4 Public Open Space & Community Infrastructure

The provision of community infrastructure and public open space (POS) is essential to achieve socially sustainable communities. They play an important role supporting the health, connectivity and participation of communities by bringing people together, developing social capital, maintaining quality of life and developing the skills, networks and relationships essential to strong communities.

State government policies aim to support connected and healthy communities and recognise the provision of accessible and integrated POS spaces and community facilities as a means of achieving these goals. State government usually provides regional parks, schools, medical facilities, cemeteries and emergency services with the Federal government providing tertiary education facilities, social security and other welfare, children's and family services.

Local governments in Western Australia usually provide local, neighbourhood and district level POS areas and community centres, including parks, sporting grounds, community halls and recreation centres, libraries and child care facilities and assist in the administration of buildings for community health, and youth and aged support services.

Non-government organisations have traditionally provided independent and religious-based schools, and religious and charity welfare services. Private companies are engaged in providing community based facilities, predominantly residential aged care services, recreation facilities and childcare services.

### 5.4.1 Community Infrastructure Plan 2011-2031 (revised November 2018)

The City's Community Infrastructure Plan (CIP) provides a strategic framework for providing community infrastructure and POS provision until 2031, based on an expected population of 75,270. The CIP is focused on community facilities provided by the City, based on community needs as they relate to adopted standards originally prepared in 2009 to determine community infrastructure requirements.

These standards were endorsed by the Western Australian Planning Commission (WAPC) which considered it against its State Planning Policy 3.6 Development Contributions for Infrastructure (SPP 3.6). The standards have been reviewed in 2015 and 2018 to account for changes to community trends in design and the way facilities are provided. Parks and Leisure Australia (WA) have recently released their updated Community Infrastructure Guidelines, dated July 2020, accordingly any future review of the CIP should take into the new standards.

For the purpose of this Strategy, the City's adopted CIP is the standard by which community infrastructure and POS areas have been evaluated. The CIP formulated a community infrastructure hierarchy taking into account the general geographical and spatial characteristics of the City, including the distribution of the current and forecast population. The CIP has been, and will continue to be, an essential tool in maintaining an equitable, accessible and efficient distribution of POS and community facilities across the City and guides what facilities are provided at sub-regional, district, neighbourhood and local levels.

Facilities	Service Level Ratio	Current provision 2018	Required by 2031
<b>Local and Neighbourhood Community Infrastructure</b>			
Local Parks	1:1,200	103	63
Neighbourhood Parks	1:5,000	37	28
Local Community Centres	1:5,000	5	9

Local Sporting Ground	1:5,000	6	13
<b>District Community Infrastructure</b>			
District Sporting Ground & Hard court	1:25,000	5	7
District Community Centre	1:25,000	0	1
District Youth Centre	1:25,000	1	2
District Branch Library	1:35,000	0	1
District Recreation Centre	1:35,000	0	1
<b>Sub-Regional Community Infrastructure</b>			
Sub-Regional Parks	1:75,000	2	2
Sub-Regional Sporting Ground	1:75,000	1	1
Sub-Regional Resource Centre	1:75,000	1	1
Sub-Regional Recreation and Aquatic Centre	1:75,000	1	1
Sub-Regional Arts and Cultural Centre	1:100,000	1	1

Table 15: Summary of CIP analysis

Table 15: Summary of CIP analysis provides a comparative analysis of the facilities available in the City, benchmarked against the CIP developed and adopted ratio of facilities per head of population. From the analysis, it can be established that residents of the City have access to sub-regional facilities, but will require district and local level facilities as the population grows.

One of the considerations of the CIP is to disperse the provision of District level facilities to more local level facilities, particularly sporting grounds. Nonetheless, a number of District level facilities are still required to be provided and are set out to be delivered throughout the City.

Local level sporting grounds will become under supplied for the 2031 population. This is largely a result of the lack of ability to assemble land to provide for District level facilities and as such more local sporting ground facilities will be needed.

The CIP sets out the general location for the required infrastructure as shown in Figure 33: Community Infrastructure Plan map 2018 and the delivery mechanism, usually through local structure planning for local and neighbourhood parks and developer contribution plans for all other facilities, excluding sub-regional facilities.

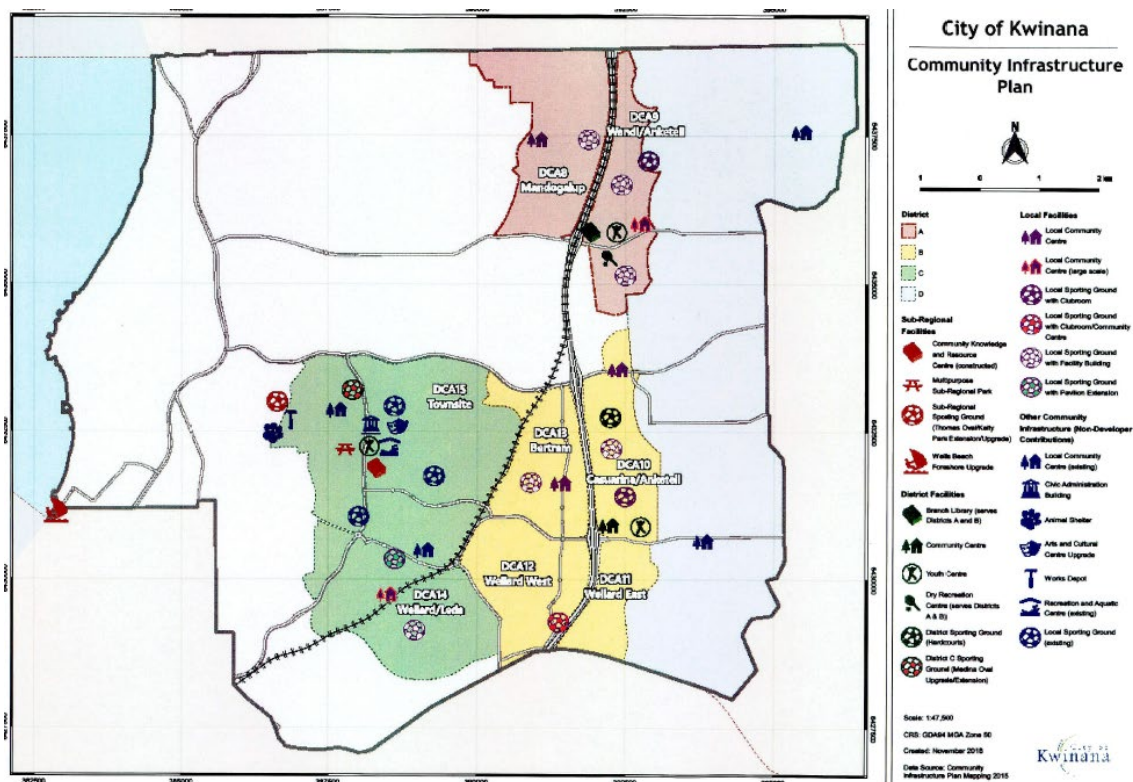


Figure 33: Community Infrastructure Plan map 2018

#### 5.4.2 Public Open Space

Currently Kwinana has over 100 public open space reserves across 17 suburbs. Kwinana has an abundance of vibrant green open space, natural bushland and large pockets of nature interspersed throughout the growing suburbs. As Kwinana has evolved so have the spaces that have been created, leaving a variety of park sizes and amenity.

Over the past 10 years, the City of Kwinana has undertaken extensive evaluation of its public open space. In 2004, the Kwinana Parks Project was undertaken to address the physical activity needs of residents and the consequent implications for public open space. This report was then followed up with the PARKS Final Report 2008. Subsequently, the Parks for People: Public Open Space Upgrade Strategy 2013-2018 aimed to continue the improvements to parks set out by the previous reports by drawing on new research, establishing new classifications for public open space that align with State norms, mapping out current classifications of public open space and facilities, outlining works undertaken since the original 2004 report and setting new objectives for public open space.

In an attempt to coordinate the classification of public open space across the State, the Department of Sport and Recreation recently developed a framework for classification which has been applied to parks in the City of Kwinana. The classification system is intended to provide guidance on the level of service and maintenance requirements of open spaces.

Open Space categories	Size
Pocket Open Space	0 - 0.399 ha
Local Open Space	0.4 - 0.999 ha
Neighbourhood Open Space	1.0 - 4.999 ha
District Open Space	5.0 - 19.999 ha
Regional Open Space	> 20.0 ha

Table 16: Department of Sport and Recreation park classification framework



POS Type	Count	Area (ha)	% Park Area	% Total POS Area*	% LGA Area
Parks	81	146.37	100	4.2	1.22
Pocket Park	15	2.6	1.78	0.07	0.02
Small Neighbourhood Park	35	21.44	14.65	0.61	0.18
Neighbourhood Park	24	56.27	38.45	1.62	0.47
District Open Space	6	44.36	30.3	1.27	0.37
Regional Park	1	21.69	14.82	0.62	0.18
Natural	44	3331.69		95.56	27.78
Residual Green Space	21	8.54		0.24	0.07
Total POS	146	3486.6			29.07
School Grounds	10	26.15			0.22
Total POS + School Grounds	156	3512.75			29.28

Table 17: POS tool data summary - City of Kwinana<sup>9</sup>

A summary of the various parks within the City is provided in Table 17: POS tool data summary - City of Kwinana.

Of note, Local and Neighbourhood parks are generally over provided, however this is largely due to the established suburban areas being developed in accordance with Feilman’s plan for Kwinana as discussed in 5.3.3 Identity & Urban design. Whilst the CIP and the Public Open Space Upgrade Strategy discusses the distribution and walkable catchment for POS areas, they do not map and analyse the catchment areas and their walkability at the City scale.

However, the Public Open Space Upgrade Strategy does map each suburb, reviewed the current classification, standard and provision of facilities of the parks within each suburb. The Public Open Space Upgrade Strategy then sets out the proposed upgrades required to be provided in order to better serve the needs of the community.

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<sup>9</sup> Centre for the Built Environment and Health. 2013: *Public Open Space (POS) Geographic Information System (GIS) layer*, University of Western Australia.  
<http://researchdata.and.s.org.au/public-open-space-pos-geographic-information-system-gis-layer>

### 5.4.3 Trails

#### 5.4.3.1 Kwinana Loop Trail

The Kwinana Loop Trail provides a 21km circuit around the perimeter of the city, offering a unique opportunity to take in some of Kwinana's best views while experiencing the beauty of the City's natural coastal bushland. Information relating to Kwinana's Aboriginal cultural heritage can be found within the trail, guided by the earth red signs along the route.

There are several walk/cycle access trailheads along the trail and car parking is available at Thomas Oval, Sloan's Reserve, Wellard Park, Kwinana Train Station and Sandringham Park.

The Kwinana Loop Trail has been designed for both walkers and mountain bikers. Different stages of the Loop Trail consist of either crushed limestone, concrete, sand or asphalt path surfaces. The sections of wide firm crushed limestone surface suits most age groups and fitness levels.

Recently the City upgraded the Kwinana Loop Trail with approximately \$80,000 being spent on signs acknowledging the Traditional Custodians of the land Kwinana sits on, ancient culture and more recent history.

Twelve signs and two new rest stops featuring Aboriginal artist Cheryl Martin's work have been installed in various locations along the 21 kilometre trail.

#### 5.4.3.2 Tramway Reserve Trail

In the early 1920's, a tramway was constructed extending south from the Jandakot Railway Station to Karnup to facilitate the development of the Group Settlement Scheme in the Peel Estate. Timber harvested in the Peel Estate region was transported out, and materials to support the Group Settlement Scheme transported in. However by late 1925, the need for the tramway had passed and the rails were pulled up.

An enduring legacy of the tramway is a long, narrow reserve running approximately parallel to, and to the west of the Kwinana Freeway, that remains largely intact to this day. This approximately 32km long reserve transects the Cities of Cockburn, Kwinana and Rockingham and was identified by these Local Government Areas (LGA's) to have great potential as the home for a multi-use trail. Such a trail would run past a variety of natural features including the lakes, wetlands and bushland of the Beelir Regional Park which features regionally significant lakes such as Yangebup, Kogalup and Thomsons Lakes.

A trail following the route of the former tramway would also intersect or have the capacity to readily link with a variety of existing trails along its route, significantly expanding the options for and experiences available to users.

The Tramway Trail project was identified as a priority project by the South West Group's Regional – Natural Resource Management (NRM) Strategy (2013) as shown in Figure 34: Tramways Reserve Trail. The potential for such a trail was also identified in each of the Cities of Cockburn, Kwinana and Rockingham's Trails Master Plans.

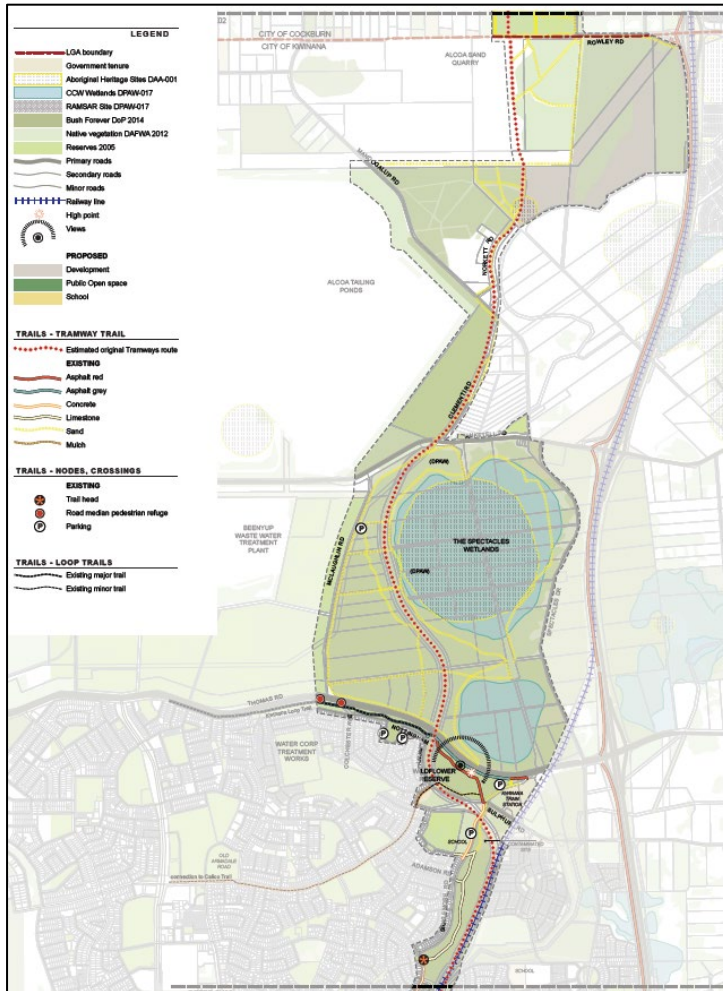


Figure 34: Tramways Reserve Trail

### 5.4.3.3 Off-Road Vehicle area

The Kwinana Off Road Vehicle Area is in Thomas Rd Medina, near the Rockingham Rd intersection. The area is about 20 hectares in size and is mainly sandy terrain with limestone caprock.

As there are no fences around the gazetted area, many riders are not aware of the boundaries of the area. Area Boundary signage was installed but this signage was removed or destroyed.

Management plans for the area have been developed and funding is being sought to develop proper trails and facilities. Kwinana is the only gazetted Off Road Vehicle south of Gnangara and is an important recreational site until a larger facility can be identified and developed<sup>10</sup>.

### 5.4.4 Key Issues

The City currently meets or exceeds benchmark requirements for provision of Community Infrastructure and local and neighbourhood parks for its 2018 population. However, continued growth of the City to a population beyond 75,000 will necessitate provision of new community infrastructure and facilities such as public open space, sporting grounds and community centres, particularly in the developing areas such as Wandi, Anketell, Casuarina and Wellard.

<sup>10</sup> Recreational Trail Riders Association. 2020: <https://rtra.asn.au/>

Further, it should be noted that the CIP standards may need updating given the most recent Parks and Leisure Australia (WA), 2020 Guidelines for Community Infrastructure. This may result in a change to the hierarchy of the facilities. In addition, the DPLH are currently reviewing SPP 3.6 and that may also influence how community infrastructure is provided by the City and could necessitate a review of the CIP and associated developer contribution funding and delivery mechanisms. Should a review be undertaken, it is recommended that the CIP be extended to include a more comprehensive analysis of POS by incorporating accepted standards for POS provision and mapping the catchments of POS areas to determine if their provision alone is sufficient, or due to their distribution there may be justification for further provision.

The City of Kwinana's Trails Master Plan was drafted in 2006, and identified the following characteristics of 'trail supply':

- There are no formally recognised mountain bike (off road cycle touring) trails or horse-riding trails within the Town;
- Trails in the Town of Kwinana are currently not well packaged and promoted;
- Little or no interpretation is provided on the trails – apart from the trails with The Spectacles;
- Sponsorship is a feature of some of the trails, with Alcoa evidently providing funds for interpretive panels within The Spectacles and Edison Mission Energy sponsoring the existing Tramway Nature Trail; and
- Existing trails appear to have no clear focus or 'theme'.

The issues identified in the Masterplan appear to still be evident within the City. Some improvements have been made to the Kwinana Loop Trail quite recently, however further investment and improvements may be appropriate in order to realise the Trails Master Plan.

There are limited off-road vehicle areas within Perth, and the Kwinana area is considered to not be a large enough facility to cater to the community's needs. There is also conflict between users of the off-road area and other trail users due to poor signage and trail demarcation.

#### 5.4.5 [Response to issues](#)

The City should monitor changes to the legislative framework and review and update the Community Infrastructure Plan as necessary.

The City may need to review the Trails Master Plan and seek opportunities to improve the trail network to integrate with the pathway system outlined in the Bike and Walk Plan.

The City should investigate opportunities to improve the off-road vehicle area including the option of finding a more suitable facility that minimises conflict between users.

# Economy



## 5.5 Industry

The primary industry land within the City is known as the Kwinana Industrial Area (KIA), is located on the western edge of the City along the coast line and is part of the wider Western Trade Coast region (WTC). WTC is a heavy industry area covering approximately 1,150ha and includes the Kwinana Industrial Complex (the southern portion of which is in the City of Rockingham) and Henderson in the City of Cockburn.

It is a large well established and complex port-related heavy industry zone with access to a highly skilled labour pool. The WTC is unique in Western Australia and unusual even in international terms. Its core industries, are based on heavy industry, including petroleum and minerals refineries, power stations, chemical plants, cement works and a range of supporting industries. These are all highly integrated and have grown over time which gives it a strong competitive advantages, specifically, the highly sophisticated inter-organisational relationships and networks.

On the fringe of the heavy industrial complex, the Rockingham Industrial Zone and East Naval Base factory area, house well established fabrication facilities, depots for transport, warehousing and a range of construction supply activities.

To the east, the Latitude 32 Industry Zone provides space for transport, storage and laydown areas as well as new industrial land for supporting industries. In the northern part of the WTC is the Australian Marine Complex, which is an area for shipbuilding, marine engineering, manufacturing, fabrication, defence services, technology and general engineering.

Within the City, there is 6.27 hectares of Light Industry zoned land located in Medina surrounded by Parks and Recreation reserves and Kwinana Golf Club. This industrial area contains 42 lots ranging from 728m<sup>2</sup> to 2750m<sup>2</sup> in area and have an assortment of light industrial activities. The location of this light industrial zoned land was a legacy from the Margaret Feilman's plan for the City and remains a local employment and service hub for the community.

### 5.5.1 Industrial land

Industrial land demand and supply was remodelled for the Sub-regional Frameworks, which combined show an estimated demand for 9,650 ha of additional industrial land across Perth and Peel between 2015 and 2050, to cater for an additional population of 1.869 million. This increase is at the rate of 51.6m<sup>2</sup> per head of additional population across the period. However, the growth is spread unevenly, ranging from 42.4m<sup>2</sup> per head of additional population in the metro North West to 116.4 m<sup>2</sup> per head of additional population in the metro North East.

The Sub-regional Frameworks provide an estimate of potentially available industrial land, both zoned and undeveloped and not yet zoned industrial (classified as Industrial Expansion Areas and Industrial Investigation Areas) for the 2015 – 2050 period as shown in Table 18: Perth & Peel Industrial Land.

An Industrial Expansion Area is *land identified* for future industrial development, whereas Industrial Investigation Areas *may be suitable* for industrial development, but require further investigation to determine its suitability and/or refinement of the area and are shown in Figure 35: Industrial expansion and Investigation areas. These sites have taken into account optimal proximity to workforce and service catchments, compatibility with adjacent land uses, accessibility to the freight network, and, where possible, proximity to intermodal transfer facilities.

Sub-regional Framework	Sth Metro/Peel	North West	North East	Central	Overall
Additional Population 2011 - 2050	741,020	417,800	241,430	468,900	1,869,150
Assumed Demand 2015 - 2050	5,070	1,770	2,810	0	9,650
Rate (ha / pa)	164.5	49.2	78.2	0	
Rate (m2 pp)	68.4	42.4	116.4	0.0	51.6
Identified Supply*	3190	3260	4580	0	11,030
Adjustment (gross to net @ 75%)	2393	2445	3435	0	8,273
Net Supply (Shortfall) to meet demand	(2,677)	675	625	0	(1,377)

Table 18: Perth & Peel Industrial Land

Notes in relation to Table 18: Perth & Peel Industrial Land:

- Latitude 32 is excluded as potentially available light and general industrial land supply for the South metro / Peel sub-region.
- The Muchea Industrial Park is included as potential industrial land supply for the North East sub-region. With the imminent completion of the Northlink major road project, this will functionally become part of the Perth metropolitan area and can be assumed into potential industrial land supply.

The results show a comfortable surplus of potential industrial land supply identified in the North East and North West sub-regions, but a considerable under provision of future industrial land in the Metro South / Peel sub-region. Note that the Central area is assumed to have no capacity for additional industrial land supply.

WTC employment has grown from 9,000 in 1997 to 14,350 in 2017, a 59% increase. Over the same period, floor space has increased from 0.67 million m<sup>2</sup> to 1.84 million m<sup>2</sup>, a 176% increase, indicating a sharp reduction in employment intensity. This is due to the major land uses of Manufacturing and Storage / Distribution having the strongest floor space growth.

The exception is Office/business uses, which has grown strongly in employment numbers and proportion. Office/business uses now account for over 50% of all employment in the Western Trade Coast. While some of this employment is directly related to the on-site industrial activity, much of it is not specific to the land uses on the sites on which the offices are located.

Industrial land estates are thus becoming less a place for large scale employment (although they remain important employment locations) and more a location for economic activity to serve the wider economy. The underlying land demand and economic activity to accommodate the increase in floor space remains. However, while employment densities in industrial estates generally, and particularly those that contain a high proportion of freight and logistics and manufacturing, are declining, the economic activity being undertaken there is still essential to support the population and the land is required to accommodate that activity.

For Heavy and Strategic industrial land, some demand can be identified, however many viable future projects may not yet be apparent but their likelihood needs to be allowed for in planning. The recent lithium 'boom' indicates that opportunities can come up very quickly (i.e. within a 5 – 10 year time frame). It was the availability of sites and the particular advantages of WTC that made the Kwinana location viable.

There is a current general shortage of suitable land for heavy industries. The Rockingham Industrial Zone is fully allocated. New entrants are increasingly seeking very large sites, with lithium processors requiring sites of 35 – 70 ha and these are practically non-existent.

A number of new and emerging industries can be pinpointed as likely to have a presence in the Western Trade Coast in the future. Some are already there. Identified shortlisted emerging industries include – but are not limited to – the following sectors:

- future battery metals;
- advanced manufacturing of prefabricated housing;
- circular economy; and
- hydrogen economy.

Each has been identified at the Commonwealth and State level as having high growth potential and each has active government programs for their development. Each has potential for a substantial presence in the Western Trade Coast.

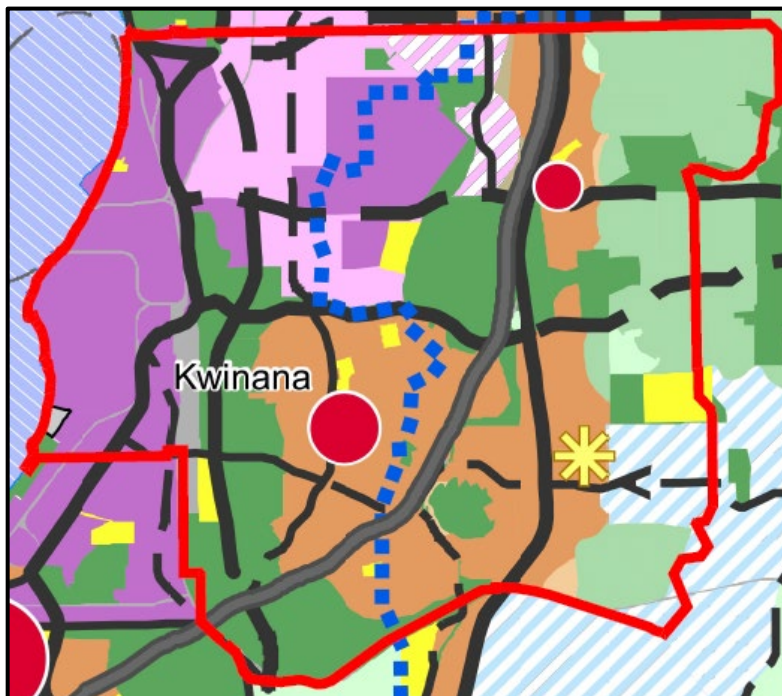


Figure 35: Industrial expansion and Investigation areas

A large proportion of the land identified as future industrial land supply in the Sub-regional Framework is in Latitude 32, as shown in Figure 35: Industrial expansion and Investigation areas. This could be better regarded as future supply for strategic and heavy uses and not for light and general uses. As such it should not be counted as potential land for light and general industrial land supply.

Mandogalup is identified as an Industrial Investigation Area in the Sub-regional Framework and is subject to an Improvement Scheme as described in 3.2.7 Improvement Plan 47 – Mandogalup. It is ideally located for port supply chain land uses (e.g. motor vehicle wholesale preparation and storage) as well as for general light industrial purposes.

The nature of the ultimate development of Mandogalup as an industrial estate and its land uses depends to some extent on the nature of development immediately surrounding it. It is adjacent to a



large residential development to the east and is more likely to have some similarities with Port Kennedy, with a slightly higher proportion of local services.

Postans, which comprises 87 hectares of land west of the proposed Fremantle Rockingham Controlled Access Highway (FRCAH) and approximately 50 hectares of land adjacent to the Motorplex on the Alcoa's A, B & C mud lakes, is recognised as an Industrial Expansion Area in the Sub-regional Framework.

4.2.1.1 Postans Precinct Structure Plan provides details about the Postans Precinct study conducted by the City, Landcorp and DPLH in 2011. The study identified additional general and light industrial land in the Postans area as a transition to Latitude 32. The proposed land use structure for the Postans Precinct as clustered into four land use zones of light industry that comprises:

- Strategic industry to the western and central portion of the Precinct clustered around Abercrombie transport corridor to encourage industry alliance (strategic industry is separated into short and longer term land availability);
- Smaller scale industry to the east with odour producing uses requiring a buffer to fall within the expanded Waste Water Treatment Plant (WWTP) buffer;
- Agri-business industries to the east acting as a separation to the Spectacles; and
- Service Commercial and Service Industrial uses with exposure to Thomas Road.

The intent and general proposals put forward by the Postans study are still sound. However, given the more recent announcements regarding the location of a future Port and new information regarding linkages (both transport and ecological) that traverse this area it may be appropriate to revisit this study. Further, any future potential for Postans to become a general industrial, light industrial and service commercial area should be reviewed in context of the existing Medina light industrial area.

#### 5.5.2 Key issues

There is limited supply of heavy and strategic industrial land in the WTC. This supply is under pressure from uses which do not require the buffer protection, most particularly freight and logistics and office/business uses.

In the WTC, over 50% of all employment is now in office/business activities. While some is directly associated with nearby or adjacent industrial land uses, much is not and could be located elsewhere.

There are several problems with this scale of office use on industrial land:

- in the case of WTC, it takes up scarce land that could be used for purposes that require the heavy industrial / strategic industry zoning;
- the land is not well served by public transport for worker access;
- it has limited facilities for worker amenity; and
- it is within the Kwinana air quality buffer.

There is a limited supply of light and general industrial land in the South Metro and Peel region generally and demand will continue to grow attributed to general population growth plus special uses associated with port activities.

Mandogalup and Postans have been identified as Industrial Investigation and Industrial Expansion areas under the Sub-regional Frameworks. The City's position on Mandogalup is established through its adopted LPP 12 and the Postans area study was completed in 2011 which may assist in guiding development in that location. However, given changes to major infrastructure (port and logistics) within the immediate vicinity and the State's Improvement Plan preparation currently underway, it may be appropriate to review those studies.

### 5.5.3 Response to issues

Heavy industrial land must be protected from encroachment and from inappropriate use. For the Kwinana Industrial Area, this means active discouragement of uses which do not contribute to the network in the KIA and which do not require the operational protection afforded by the buffer uses around the KIA. This particularly includes activities which are large land users but small employers, such as storage, freight and logistics. This may include specific land use controls for the industrial zone and formalisation of a buffer to protect heavy industrial land from encroaching sensitive land uses.

The undeveloped land at Latitude 32 allows a relatively straightforward extension of the integrated Kwinana Industrial Area heavy and strategic industrial complex and it should be preserved for that use.

Seek to expand light and general industrial land into areas that can be located nearby, but not within the Kwinana Industrial Area. Mandogalup and Postans should be regarded for these purposes subject to further planning for those supply chain uses and those activities that find advantage in the Kwinana location, particularly with the announcement of the port, but do not need heavy industrial land capability.

Large employment uses such as office/business uses should be located in areas well served by public transport which provides for a greater worker amenity and benefit by being collocated with other land uses such as a mixed use environment. There may be scope to investigate alternative locations for office based businesses in areas that are better suited to those uses, which could free-up industrial land in key locations for heavy industrial uses.

Given the significance of the Kwinana Industrial Area and the significant changes in the nature of and prospects for industrial activity in the WTC, there is a need for developing a comprehensive masterplan that accounts for these prospects and makes explicit the opportunities for the area and the measures required to ensure these opportunities are maximised. This would provide the information base to inform any statutory planning provisions that might govern future development in the area.

This work would need to be undertaken in concert with the Westport Taskforce to ensure that specific modelling associated with the supply and demand analysis of the port is considered and integrated into the plan formation.

## 5.6 Ports

The State Government has endorsed the Westport Taskforce’s recommended location and footprint design for Western Australia’s next primary container port. The facility will be a new, land-backed style port adjacent to the Kwinana Industrial Area between the existing Alcoa and Kwinana Bulk Terminal jetties as shown in Figure 36: Westport shortlist option B, stage 1. The port will utilise Anketell Road as its major freight route.

This port development will be required once the Fremantle Inner Harbour is no longer commercially and operationally viable to continue as the State’s only container port. Acceptance of the Taskforce’s recommendations by the Government signals the start of the next period of intensive planning, design and assessment – with a particular emphasis on environmental management and industry, community and Aboriginal engagement.

It’s estimated that the time between a decision to proceed and completion of the detailed design, approvals, construction and the commencement of new port operations is a minimum of 10 years.

The port planning and implications for surrounding land use and transportation corridors are significant considerations for the Strategy and the City’s new Scheme.

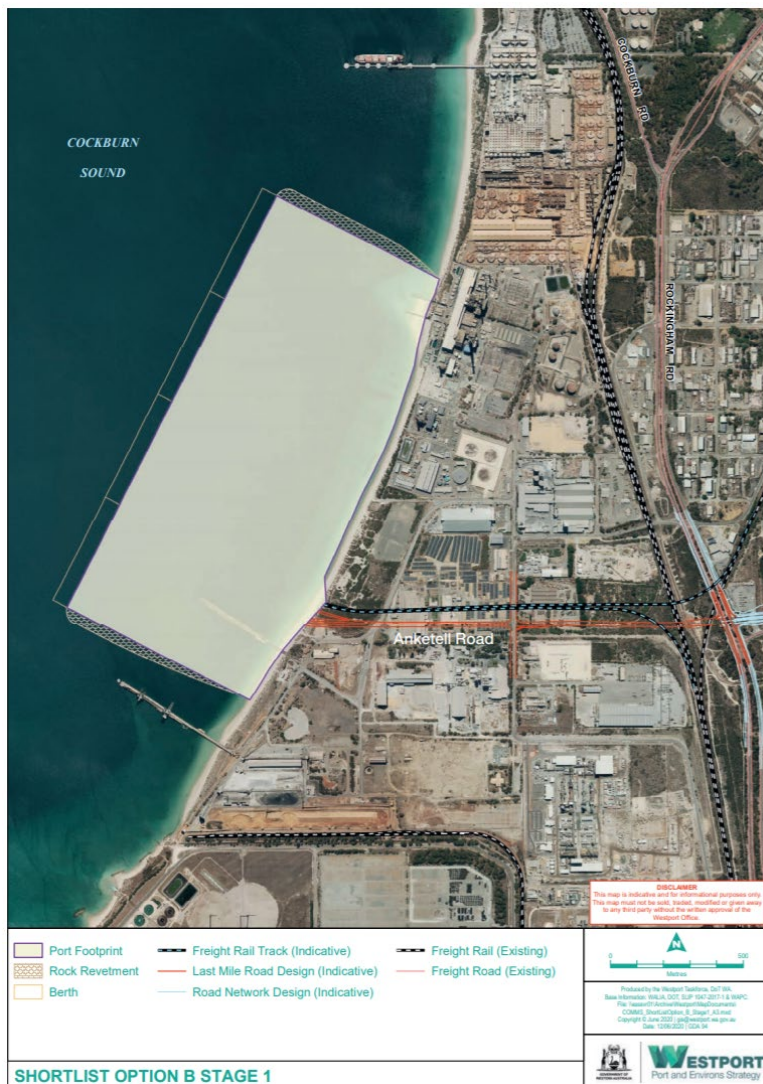


Figure 36: Westport shortlist option B, stage 1

The primary road freight access to the future port will be via Anketell Road extending from the port to Tonkin Highway, with Tonkin Highway providing regional connectivity to strategic industrial areas the airport and regional areas for supply. Anketell Road would be upgraded to a high-standard, four-lane divided freight route with central median.

In order to increase freight efficiency and safety, direct property access would be removed from the freight route by providing service roads, and grade separation would be provided at high-volume cross roads. The new port will be connected by an uninterrupted, modern freight corridor via Anketell Road and Tonkin Highway, effectively shifting the focus of industrial activity in Perth to the south and west and away from the north and east. This will further increase demand for light and-general industrial land in areas accessible to the Port and Tonkin Highway access routes.

As an example, if motor vehicle import unloading transfers from Fremantle to the Port, each major vehicle importer will consider relocating their wholesale new vehicle preparation, storage and warehousing and associated activities (e.g. parts processing, staff and dealer training and development) to be nearer the port. Each distributor would have a land requirement of around 12 – 15 ha for these purposes. These should be on light industrial (not heavy industrial) zoned land. This would create demand for industrial land at a location such as Mandogalup or North East Baldivis.

Rail upgrades under investigation include duplication of a 13km section of track between Kwinana and Cockburn and a marshalling yard at Kwinana. An upgraded rail network could support at least 1.7M TEU on rail and also provide efficiencies for bulk freight traffic servicing export berths and industrial facilities in Kwinana.

#### 5.6.1 Key issues

Time frames for the delivery of a new port at Kwinana are long, however significant economic benefits can be derived from the location of the Port in Kwinana and the City should position itself to best capitalise on these opportunities.

Analysis of these impacts and the exact nature of land required to support port related uses is unknown at this stage and could be impacted by short term land use objectives.

The road infrastructure upgrades required to service the Port will impact existing and future residents.

The Port may impact negatively upon existing industrial businesses within the Kwinana Industrial Area.

#### 5.6.2 Response to issues

The City will take into account the location of the port in Kwinana in this Strategy and ensure industrial land expansion for general, light and heavy industry uses symbiotic to the port are protected.

The City should acknowledge the significant upgrades required to the road and rail infrastructure to ensure adequate buffers are in place for any new development that may occur along those routes and seek to ensure land use separation for sensitive land uses.

However, the City should work with the Westport taskforce in order to ensure the community is given a voice and their concerns are adequately addressed through the Westport taskforce consultation processes.

## 5.7 Activity Centres

The City had a Local Commercial and Activity Centres Strategy (LCACS) prepared in 2014 to assist in the identification and confirmation of sufficient sites for activity centres in appropriate locations throughout the City, so that the shopping and other commercial and community needs of the population could be conveniently satisfied to the maximum practicable extent.

The LCACS's main purpose was to allocate retail floor space for commercial purposes across the hierarchy of activity centres within the City and it did this through a framework for the size, distribution and location of activity centres as well as commercial and office uses across the City.

Figure 38: Activity Centres Strategy map shows the activity centres identified in LCACS. These are likely to develop in a manner consistent with the Strategy as local retail and services centres, supporting population-driven employment.

While, in theory, there are no retail floor space caps applicable to any centre in the City, the maintenance of a retail hierarchy does require some effective mechanism to limit centre sizes. Maintenance of a retail hierarchy is important to ensure that appropriate levels of retail and other community services convenient to the population are provided to optimise the return on public investment in infrastructure, particularly transport and community infrastructure. It is also desirable to achieve a mix of non-retail land uses within activity centres which improves sustainability in terms of multi-purpose trips, contributing to place activation and congregation.

It is important to recognise that the retail landscape is evolving with work patterns changing and with the increasing application of information technology, Artificial Intelligence (AI) systems and automation, and will continue to change.

Figure 37: Australia's new and lost jobs shows the jobs gained and lost in Australia in the five years to 2017. It shows that jobs were lost in the primary and secondary sectors (manufacturing and mining), but gained in tertiary and quaternary sectors, including:

- Health and social assistance;
- Construction;
- Hospitality;
- Education;
- Professional and Technical Services;
- Transport/Postal;
- Retail;
- Public admin and safety; and
- Personal and other services.

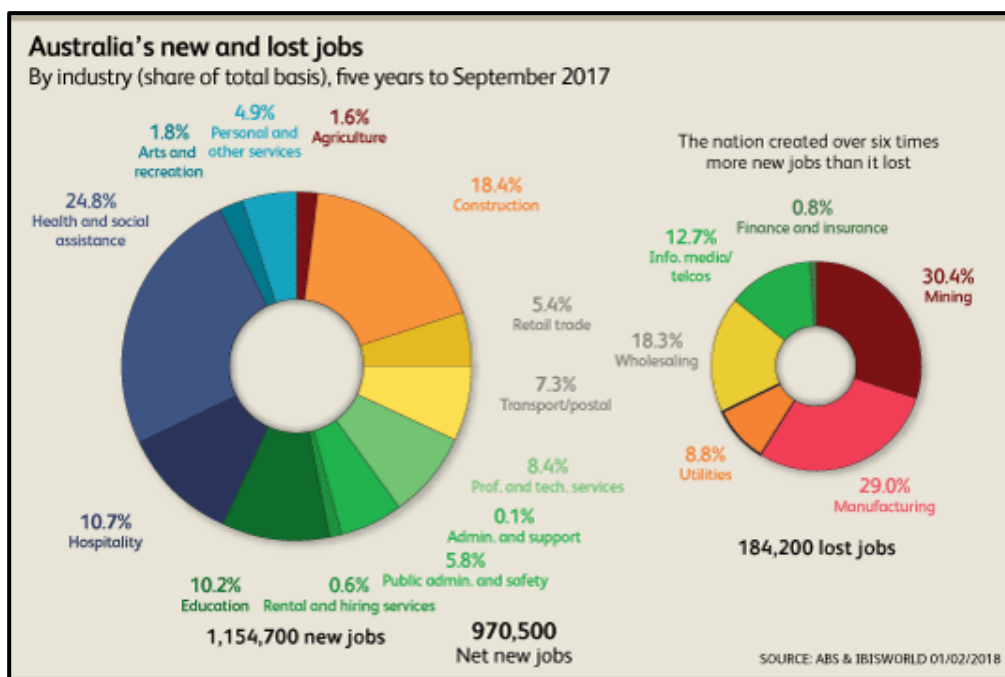


Figure 37: Australia's new and lost jobs

With respect to 'Retailing', the rate of change has increased over recent years, and major centres must move quickly if they are to remain competitive. Some changes have occurred on the demand side, with the changing nature of shopping centre consumers and their behaviours. An ageing population is one aspect of this, generally spending less per head on retail goods which together with increasing pressures from other household expenditures (the costs of health, education, housing and transport are all increasing more rapidly than retail inflation) affect all shopping centres, particularly large centres. The increased use of technology, and particularly online shopping, has added further to these pressures.

This shift will continue and the response will be to increase the importance of town centres as places where people congregate and interact on a personal basis or for other non-retail offerings. This is an important aspect of the economic future for the Kwinana City Centre and all other centres as social, personal and entertainment nodes rather than just as retail spending areas.

### 5.7.1 Kwinana City Centre

The Kwinana City Centre is designated as a Secondary Centre under the Activity Centre hierarchy and will remain the largest, and by far the most important, activity centre in the City. It contains significant retail, other commercial, educational, civic, cultural and recreational activities. Its ongoing prosperity and appropriate development should be the main focus of the City.

A significant retail floor space expansion of the Kwinana Marketplace (previously named the Kwinana Hub) was completed late in 2012. The expansion increased retail tenancies in the centre from 20,600m<sup>2</sup> to approximately 27,000m<sup>2</sup>. Most of this was the addition of a Big W discount department store to the southern end of the main shopping centre. The new development also included some main street tenancies fronting Chisham Avenue as a community focussed development opportunity.

The City Centre has been guided by the Kwinana Town Centre Master Plan & Design Guidelines and Town Planning Scheme No. 3. It is recommended that the Master Plan continue to serve as the primary guiding plan for the city centre but the Master Plan and Town Planning Scheme No. 3 be reviewed and updated into contemporary planning mechanisms, most likely a Precinct Centre Plan and appropriate zoning within a single planning scheme for the entirety of Kwinana rather than its own separate scheme.

The modelling indicates that the largest and most important centre in the City has the potential to expand over an extended period up to 45,000m<sup>2</sup> of Shop/Retail floor space. This future potential is very useful as it represents an economic incentive for continued incremental improvement and expansion of Kwinana, without significant adverse impact on the continued viability of other centres.

However, given Kwinana's history, its existing pattern of centres and the geographic centrality of the city centre to urban Kwinana, the City centre is somewhat vulnerable to competition from the larger centres of Rockingham and Cockburn Central. Those centres are not that distant in terms of travel time and are considerably more accessible in sub-regional terms. There is considerable leakage of retail trade from Kwinana suburbs to those larger centres, particularly Rockingham. This is not surprising given the relative size of Rockingham and its higher level in the regional hierarchy of centres (it is a Strategic Metropolitan Centre), but it serves to highlight the comparative vulnerability of the Kwinana city centre.

As the key activity centre within a rapidly growing catchment area, the form of the Kwinana City Centre will evolve over time and current planning and land use can allow for that. Ensuring available land in the City Centre is managed in a way that allows full advantage to be taken of evolving opportunities is the key. Health services is one such opportunity where the best form of delivery will only become apparent in the medium term as the population grows to its longer term size and the service delivery models themselves mature.

#### 5.7.2 Wandi District Centre

The future Wandi district centre proposed at the corner of the Kwinana Freeway and Anketell Road has the potential for a first stage of 6,000m<sup>2</sup> Shop/ Retail floor space by 2021, increasing to 16,000m<sup>2</sup> by 2026 and 20,000m<sup>2</sup> in the longer term. This growth potential is somewhat constrained by the need to maintain the strength of the Kwinana city centre over time, as well as the significant future growth potential identified for Cockburn Central further north. The narrowness of the urban corridor in the vicinity of the district centre is also something of a constraint on the Wandi centre's Shop/Retail floor space potential, but this is being offset to some extent by the relatively high suburban residential densities being planned in the locality.

There are considerable site constraints associated with Western Power easements and access to Anketell Road that may limit the developer's ability to neatly stage development of the Wandi district centre in the manner recommended by LCACS. The staged manner for development of Wandi district centre is largely determined through the need to manage the centre's potential impact on Kwinana city centre so it does not draw from the Kwinana city centre. For this reason, some development scenarios for the centre will require a Retail Sustainability Assessment (RSA) if the staging of development does not accord with LCACS recommendations.

There is also considerable potential for Other Retail and bulky goods floor space in the Wandi district centre. It is estimated that the centre could support 5,000m<sup>2</sup> of this floor space by 2021, increasing to 10,000m<sup>2</sup> by 2026. This potential relies more on freeway exposure and accessibility and much less on local population. The centre's location is such that, if a high profile region-serving tenant wished to establish in the centre, then the amount of supportable Other Retail floor space could, if required, be increased well beyond these calculated levels.

#### 5.7.3 Neighbourhood Centres

The total amount of floor space in neighbourhood centres has the potential to increase from a fairly low 6,977m<sup>2</sup> in 2011 to a substantial 26,068m<sup>2</sup> in 2031.

The various existing and future neighbourhood centres are a mixed bag in terms of potential performance, with the existing, well-established, Medina and Leda centres showing no expansion potential, but newer and planned neighbourhood centres of Wellard, Bertram, Anketell South and Casuarina South (shown as centre 6 and 7 respectively in Figure 38: Activity Centres Strategy map), show good future growth potential.

One planned neighbourhood centre that the modelling indicates could be slow to develop and have limited potential is Anketell South (shown as centre 6 in Figure 38: Activity Centres Strategy map). This centre is in a narrow part of the urban corridor and will face significant competition from the Wandri district centre and Bertram neighbourhood centres. It should be noted, however, that the modelling is based on the demographics of the various localities and does not reflect other factors that may influence a particular centre's performance, such as (for example) large quantities of passing traffic.

Medina is an attractive main street neighbourhood centre, which is well liked and used by a highly familiar and loyal local community. A development plan for the area immediately north of the centre has been implemented which has a residential component which should contribute to the centre's turnover.

Leda is well used centre close to Leda primary school. It has undergone recent rezoning and redevelopment and seems to be trading reasonably well. As this centre has undergone rezoning since LCACs was drafted it has a smaller retail footprint that has been given over to residential zoning. There is a need to review the LCACs document to ensure it is kept up to date with current centre sizes.

Bertram is a newly developed neighbourhood centre currently anchored by an IGA of about 1300m<sup>2</sup>. It has been designed primarily as a main street centre, but urban design main street aspirations that would have looked state-of-the-art on a plan have not been realised on the ground. In planning/ design terms, what were intended to be "active" frontages were not developed as such.

Wellard is a neighbourhood centre adjacent to the Wellard railway station. It is becoming a relatively large centre as the local population expands and has been planning around main-street and Transit-Oriented Design principles. This centre is still evolving with the growing community but its layout, design and location adjacent a train station give it good potential to become a thriving main street.

#### 5.7.4 Local Centres

The modelling indicates that all of the existing Local centres in the City have virtually no expansion potential and that, for some, continued survival will be an issue.

In the planned newer urban areas, however, this is not the case with planned and potential new local centres in Casuarina and Wellard showing potentially viable, if not startling, trade performance in the longer term. Modelling also demonstrates that there would be potential for a 500m<sup>2</sup> local centre in close proximity to the Kwinana railway station from 2016.

#### 5.7.5 Other Centres / Mixed Business / Industrial Areas

Land fronting Meares Avenue, to the east of the Kwinana Marketplace, has been developed for this purpose, although the master plan for the city centre previously envisaged this as a residential area.

There is potential for new mixed business/service commercial areas at the corner of Thomas and Johnson Roads and another in Mandogalup.



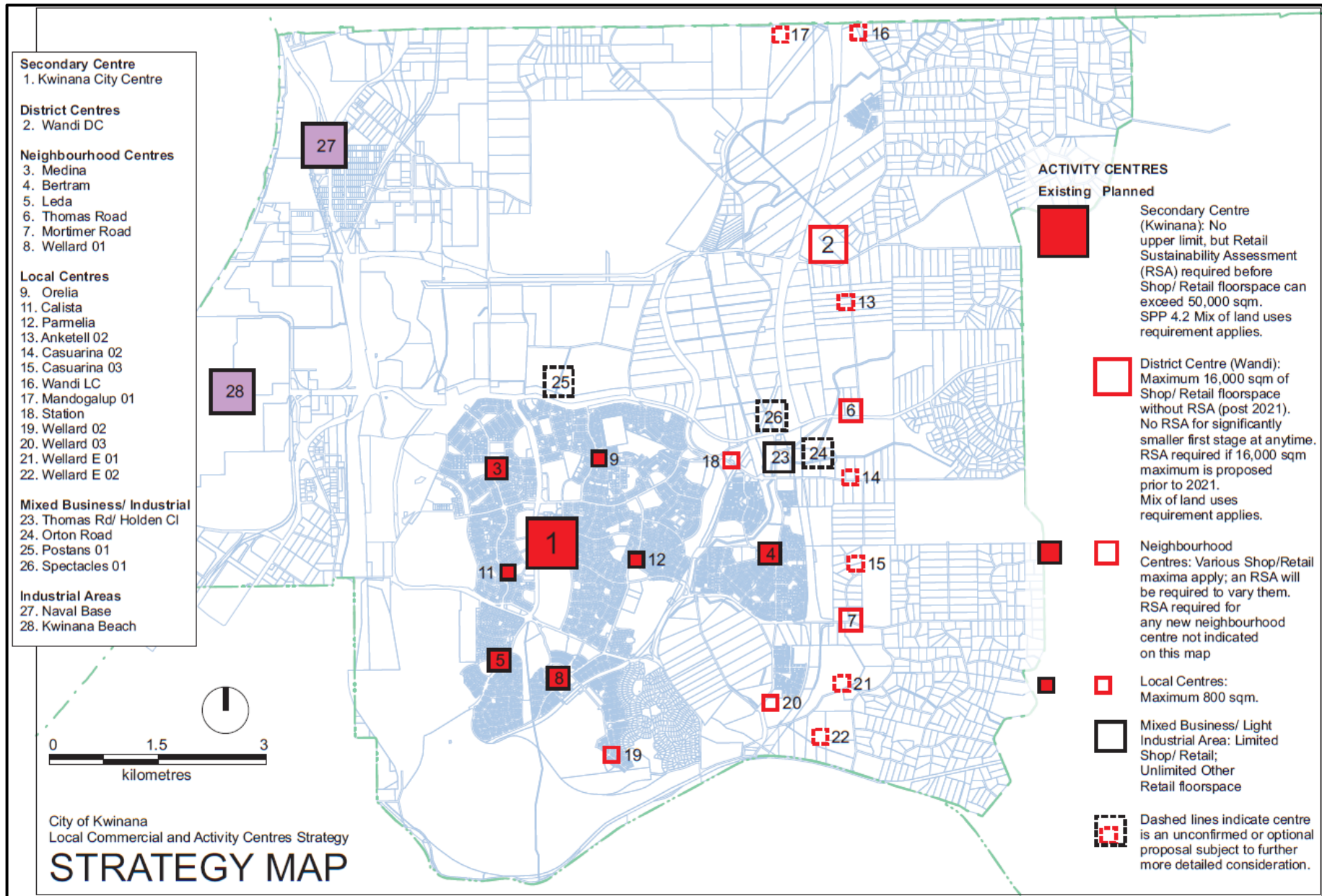


Figure 38: Activity Centres Strategy map

### 5.7.6 Key issues

The City centre is vulnerable to competition from the larger centres of Rockingham and Cockburn Central, evidenced by the vacancies in the centre today. There is considerable leakage of retail trade from Kwinana to those larger centres and the City of Cockburn is advocating for Cockburn Central to be elevated in the centres hierarchy to a Strategic Metropolitan Centre, the same as Rockingham, which may further impact the Kwinana City Centre.

An expansion of the Kwinana City Centre to form a role beyond that of a district-level activity centre is very unlikely. The form of the City Centre is modular and singular in its dimension and lacks a clear Main Street focus. The rapidly changing nature of retail activities has the potential to influence the nature and functions of all activity centres within the City, particularly the City centre. Existing planning mechanisms that govern the City Centre require review, modernisation and consolidation to the latest planning frameworks.

The projected size of the Wandí district centre and the potential for its premature development (in immediate catchment terms) may adversely impact upon other centres, including the Kwinana City Centre.

Existing neighbourhood centres need to be further supported to ensure viability (via their residential catchments) and the urban design of existing centres presents issues that could be addressed through future master planning.

Some local centres are no longer viable and their ongoing operation is also drawing activity away from other more appropriately located and sustainable centres.

There is a need to achieve a high standard of development in new centres and revitalise the design of existing centres.

### 5.7.7 Response to issues

The primary strategic response for the City Centre must be to ensure that planning scheme provisions place the viability of the Kwinana city centre above all other centre planning considerations.

Ensure land in the City Centre is managed to allow full advantage to be taken of evolving opportunities that seek to support and build on the City Centres function. This will require active management of City owned land and management of the Centre as a whole and may require advocacy from the City or public private partnerships to support the centre. Seek to elevate the Kwinana City Centre, and all other centres, as social and personal nodes rather than just as retail spending areas.

To protect the viability and activity of the Kwinana City Centre in fulfilling its role for its community, the following should be applied:

- No upper retail floor space limit but a Retail Sustainability Assessment (RSA) will exist for any major development/s that would result in Shop/Retail floor space equaling or exceeding 50,000m<sup>2</sup>;
- The Kwinana City Centre Master Plan and Design Guidelines will remain as the guide for future City Centre development, but the reviewed SPP 4.2 Activity Centre Plan requirements apply and should require a Precinct Structure Plan to be prepared.

Given the increasing size and function of nearby centres and the ongoing development of new urban areas within the City, a review of the current LCACS is recommended.

For Wandii district centre, a reasonable balance between facilitating development in response to market demand, yet avoiding premature development that might prevent more timely and appropriate development of other centres, is proposed. The thresholds planned to achieve this are regarded as reasonable and unlikely to thwart or limit the timing of the centre's development where this is aimed at catering for the needs of its own primary catchment area.

The specific strategy for the Wandii district centre can be summarised as follows:

- The maximum amount of Shop/Retail floor space permitted without an RSA is 20,000m<sup>2</sup>.
- An RSA would be required before development is allowed to exceed 20,000m<sup>2</sup> of Shop/Retail floor space.

Neighbourhood centres should be retained and encouraged to fulfil a significant local convenience role, without becoming any larger than necessary to perform that function. There is a presumption against expansion of any neighbourhood centre beyond that defined below. A convincing RSA will be required if the following thresholds are to be exceeded (Shop/ Retail floor space only).

- Medina: 4,500m<sup>2</sup>
- Bertram: 6,000m<sup>2</sup>
- Thomas Rd: 4,000m<sup>2</sup>
- Mortimer Rd: 6,000m<sup>2</sup>
- Wellard: 6,000m<sup>2</sup>

Mix of land use requirements need not apply to neighbourhood centres, but local offices, community services, and adjacent or integrated higher density residential development should be encouraged wherever practicable. There are opportunities to undertake master planning or precinct planning exercises for neighbourhood centres in association with business owners and the community.

Local centres identified on Figure 38: Activity Centres Strategy map may be permitted to be developed or expanded to a maximum of 800m<sup>2</sup> of Shop/ Retail floor space provided no individual tenancy, other than a supermarket, exceeds 200m<sup>2</sup>. Single large-format Shop/ Retail activities (e.g. a chain liquor store) are not considered appropriate in local centres and should not be permitted in them. Mix of land use requirements do not apply to local centres.

Responses to specific local centres is provided below:

- Orelia needs a pro-active approach to facilitating its redevelopment.
- Calista local centre should seek to facilitate redevelopment of the site with medium density residential development and a corner store/ deli of between 250-300m<sup>2</sup> of Shop/ Retail floor space.
- Summerton commercial complex (corner of Calista Avenue and Summerton Road Medina) should be rezoned to remove any development potential for additional retail floor space at this location.
- The City will need to review the number and function of local centres in new urban areas.

The City should identify the Thomas Road precinct area as a suitable site for the future development of showrooms suitable for Other Retail and low intensity service industrial and mixed business uses as discussed in 5.10 Services and businesses. The identified area is illustrated on Figure 38: Activity Centres Strategy map as mixed business locations 23, 24 and 26. The RNA modelling indicates that all these areas have excellent long term potential for Other Retail and bulky goods uses. The specific strategy for the mixed business areas in that location may be summarised as:

- Limited Shop/ Retail floor space (Clause 5.6.2 of SPP 4.2 applies).
- Unlimited Other Retail, bulky goods and general service commercial/ industrial floor space within the designated areas.

## 5.8 Employment

### 5.8.1 Employment status

The size of the City's total labour force in 2016 was 18,829, which includes all persons aged above 15. Overall, 89.2% of the labour force was employed and 10.8% unemployed, compared with 91.7% and 8.3% respectively for South West Group. When added to the people looking for work, either full or part-time, there are 21.6% of the labour force not in work. By comparison the South West Group equals 16.5%.

Employment status	2016		
	Number	Kwinana %	South West Group %
Employed	16,802	89.2	91.7
Employed full-time	10,801	57.4	56.4
Employed part-time	5,624	29.9	33.9
Hours worked not stated	377	2.0	1.4
Unemployed (Unemployment rate)	2,027	10.8	8.3
Looking for full-time work	1,353	7.2	5.0
Looking for part-time work	674	3.6	3.2
Total labour force	18,829	100.0	100.0

Table 19: 2016 Employment Status

This shows that within the City of Kwinana there is a lower proportion of the labour force in employment and a higher proportion either unemployed or looking for work (and is one of the highest in the Perth Metropolitan Area).

### 5.8.2 Employment by Industry

At the 2016 Census there were 12,650 people working in Kwinana. Although employed in Kwinana, they may or may not be local residents. As shown in Table 20: Employment by Industry, employment is heavily concentrated in two employment sectors – manufacturing (23.9%) and construction (11.6%).

Industry	Number	Kwinana %	Greater Perth %
Agriculture, Forestry and Fishing	314	2.5	0.7
Mining	506	4.0	3.1
Manufacturing	3,021	23.9	5.9
Electricity, Gas, Water and Waste Services	388	3.1	1.2
Construction	1,463	11.6	8.3
Wholesale Trade	381	3.0	2.8
Retail Trade	800	6.3	10.4
Accommodation and Food Services	424	3.4	6.8
Transport, Postal and Warehousing	660	5.2	4.6
Information Media and Telecommunications	42	0.3	1.1
Financial and Insurance Services	318	2.5	2.9
Rental, Hiring and Real Estate Services	214	1.7	1.9
Professional, Scientific and Technical Services	339	2.7	7.5

Administrative and Support Services	317	2.5	3.0
Public Administration and Safety	788	6.2	6.8
Education and Training	826	6.5	9.4
Health Care and Social Assistance	744	5.9	13.0
Arts and Recreation Services	138	1.1	1.8
Other Services	325	2.6	4.1
Industry not classified	642	5.1	4.7
<b>Total</b>	<b>12,650</b>	<b>100.0</b>	<b>100.0</b>

Table 20: Employment by Industry

In contrast, across the Greater Perth area, the focus is Health Care and Social Assistance (13%), Retail Trade (10.4%), Education and Training (9.4%) and Construction (8.3%) with Manufacturing accounting for just 5.9% of total employment.

In part this reflects the history of Kwinana, especially its role as the primary heavy industry area in the State but has not yet been able to evolve and diversify its employment base despite population growth.

### 5.8.3 Industry of employment

The industry sectors in which the residents of Kwinana work are shown in Table 21: Industry of Employment. These are the jobs that Kwinana residents occupy and indicates the skill base of the City's residents. It is important to note that these jobs may be located anywhere and are not exclusive to being located within Kwinana.

<b>Industry of employment</b>	<b>Number</b>	<b>Kwinana %</b>	<b>Western Australia %</b>
Agriculture, Forestry and Fishing	167	1.0	2.5
Mining	718	4.3	6.3
Manufacturing	1,358	8.1	5.6
Electricity, Gas, Water and Waste Services	227	1.4	1.2
Construction	1,780	10.6	9.8
Wholesale Trade	530	3.2	2.5
Retail Trade	1,808	10.8	9.5
Accommodation and Food Services	1,045	6.2	6.4
Transport, Postal and Warehousing	993	5.9	4.6
Information Media and Telecommunications	152	0.9	1.0
Financial and Insurance Services	329	2.0	2.4
Rental, Hiring and Real Estate Services	233	1.4	1.7
Professional, Scientific and Technical Services	850	5.1	6.4
Administrative and Support Services	635	3.8	3.3
Public Administration and Safety	1,123	6.7	6.2
Education and Training	1,103	6.6	8.7
Health Care and Social Assistance	1,973	11.7	11.7
Arts and Recreation Services	211	1.3	1.6
Other Services	672	4.0	4.0
Industry not classified	895	5.3	4.6
<b>Total</b>	<b>16,802</b>	<b>100.0</b>	<b>100.0</b>

Table 21: Industry of Employment

An analysis of the jobs held by the resident workforce in the City in 2016, shows the three most popular industry sectors were:

- health care and social assistance (1,973 people or 11.7%);
- retail trade (1,808 people or 10.8%); and
- construction (1,780 people or 10.6%).

This is more consistent with the breakdown of jobs by industry for greater Perth as shown in Table 20: Employment by Industry. This would tend to indicate that residents of Kwinana are employed in largely the same types of jobs as the remainder of the Perth metropolitan area, but that the jobs located in Kwinana are more specialised towards the manufacturing and construction sectors. It also suggests that the workforce is available for those industry types to establish locally.

#### 5.8.4 Employment self-sufficiency and self-containment

Employment self-sufficiency and self-containment measure the relative participation and potential participation of a local workforce in locally available jobs, and thus indicate the extent to which the quantity and nature of employment provision in an area might be problematic.

Employment self-sufficiency and self-containment are important metrics to achieve overall planning objectives to try and reduce the need for lengthy commuting between homes and workplaces. Time and energy wasted in this manner has both economic and environmental negative effects, and is the main cause of traffic congestion.

In 2016, in the City, there were 12,650 jobs available and 18,829 people in the labour force, which equates to an employment self-sufficiency of 67.2%. The City also had 3,092 residents who work locally in one of the 12,650 local jobs, which represents an employment self-containment of 24.5%. It indicates that local residents are not dominant in filling local jobs and travel outside of the city for work.

A further breakdown of where Kwinana workers are traveling for work is indicated in Table 22: Major employment location of resident workers by LGA.

Major employment location of resident workers by LGA	Number	%
Kwinana (C)	3,092	18.5
Cockburn (C)	2,412	14.4
Rockingham (C)	1,749	10.5
Perth (C)	1,677	10.0
Melville (C)	1,006	6.0
Canning (C)	942	5.6
No Fixed Address (WA)	833	5.0
Fremantle (C)	793	4.7
Belmont (C)	452	2.7
Stirling (C)	400	2.4
Victoria Park (T)	326	1.9
Gosnells (C)	293	1.8
Armadale (C)	260	1.6
Ashburton (S)	250	1.5
Mandurah (C)	235	1.4
Swan (C)	197	1.2
Subiaco (C)	179	1.1
Vincent (C)	179	1.1
South Perth (C)	127	0.8

East Pilbara (S)	126	0.8
Nedlands (C)	122	0.7
Serpentine-Jarrahdale (S)	118	0.7
Kalamunda (S)	95	0.6
Cambridge (T)	81	0.5
Bayswater (C)	70	0.4
Wanneroo (C)	65	0.4
Total	16,079	96.3

Table 22: Major employment location of resident workers by LGA

The data shows that although self-containment within the City of Kwinana is quite low, a further 25% of resident workers are employed in the adjacent Cities of Rockingham and Cockburn, equating to a total of 43.4% of Kwinana residents working in one of the three immediate local government areas.

The major reasons for the City's resident workers seeking employment outside the City could be the nature of employment opportunities versus the residents skills and qualifications; transport options available and commuting times; relationship between wages and salaries (people will travel further for higher paid jobs); and house prices in the local area.

#### 5.8.5 Employment growth

There has been some recent growth in employment in Kwinana, however employment is not keeping up with the rapid population growth. As shown in Figure 39: Employment growth, from 2006 to 2018 employment in Kwinana has remained steady at around 15,500 jobs, while the population has increased from 24,000 to over 42,000 over the same time period.

The Kwinana share of employment has fallen compared to both Perth and the South West Group, as shown in Figure 39: Employment growth. These trends in employment growth show the challenge associated with having job growth in Kwinana keep pace with projected population growth.

Employment growth has not matched the employment growth in the broader South West Group area nor the employment growth in Greater Perth. If the City seeks to maintain its employment opportunities for its residents it will need to find another 12,000 jobs.

A proportion of these jobs (around one-third) will be population-driven, namely jobs that arise to directly service the local population. They include local retail and business services, health services, education services (particularly primary and secondary education) and personal services. However, this leaves 6,900 jobs that must be found.



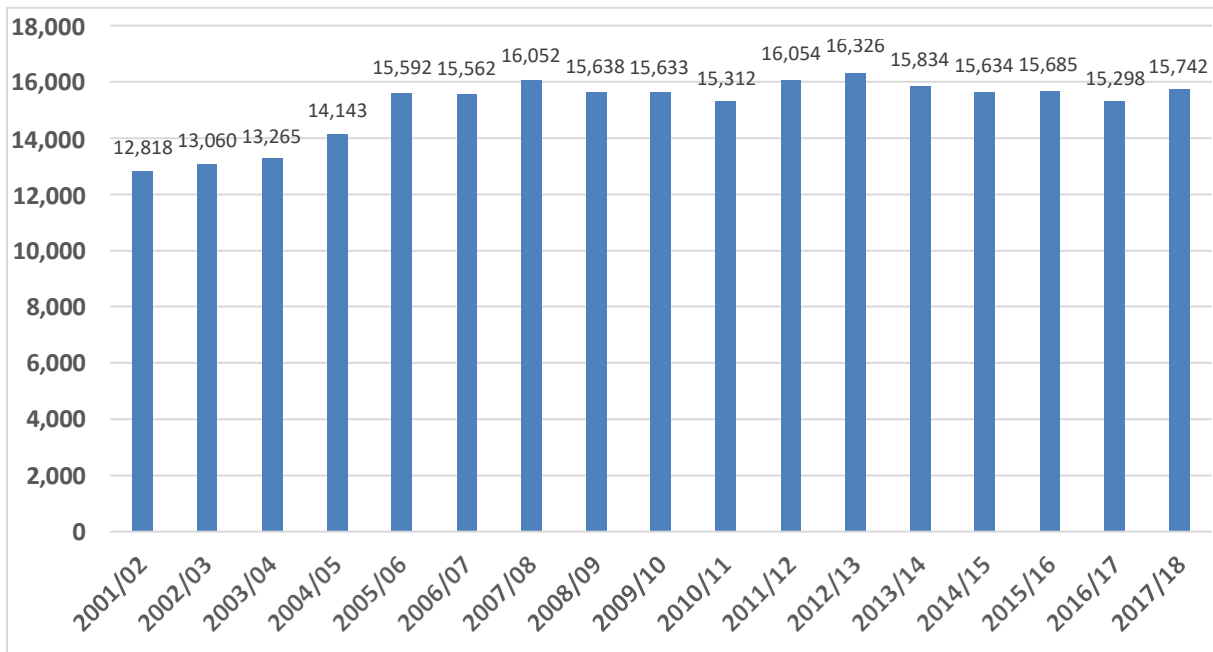


Figure 39: Employment growth<sup>11</sup>

#### 5.8.6 Key issues

Of particular interest for Kwinana is manufacturing, which as noted above, dominates employment in the City. Interestingly of the 16,727 employed Kwinana residents, only 1,358, or 8%, work in manufacturing. Accordingly, manufacturing employment in Kwinana is very important to many employees from outside the City, whereas employment elsewhere in key employment sectors like retail and services is very important to the employment prospects of Kwinana residents.

Other major challenges that come from the population growth and statistical data provided in 5.1 Population and Housing includes:

- the 70 – 84 age group will increase by around 3,100 and by around 835 persons in the 85 and above age group; and
- the location of the forecast population growth is mostly east of the Kwinana Freeway, and whilst these locations are close to public transport they are generally away from employment centres.

These points have several direct implications:

- an increased resident workforce will demand increased employment opportunities;
- employment opportunities will need to match resident skills;
- there should be an approximate doubling of population related employment (health services, local retail and business services, education services (particularly primary and secondary education) and personal services;
- increased demand for activity centre floor space (retail, commercial, services) primarily affecting existing activity centres in the City; and
- an increase in the aged population, with the consequent need for 2 - 3 additional aged care facilities plus in-home services.

<sup>11</sup> Source: i.d. forecast (2017)

### 5.8.7 Response to issues

The City should protect the industrial base that offers significant employment opportunities for the whole of Perth metropolitan region.

The City should seek to expand and diversify the employment opportunities offered in the City to better suit the skills and qualifications of the local workforce, through appropriate land use zoning and in the City's activity centres.

The City should facilitate employment growth in aged care facilities plus in-home services. These are large employers and the employment opportunities offered by these industries matches the skills base of Kwinana residents.

## 5.9 Tourism and Visitors

In 2017/18, the total tourism and hospitality sales in the City of Kwinana was \$29.4m, the total value added was \$16.4 million, as shown in Table 23: Value of tourism and hospitality. There are 339 people who make up the tourism and hospitality workforce in the City, of this 28.9% worked full-time and 69.9% worked part-time or were away from work.

City of Kwinana	2017/18			
Measure	City of Kwinana	% of total industry	Western Australia %	CoK as a % of WA
<b>Employment (total)</b>				
Direct	174	1.1	5.8	0.2
Indirect	39	0.2	2.2	0.1
Total	213	1.4	8.0	0.2
<b>Employment (FTE)</b>				
Direct	128	0.9	4.9	0.2
Indirect	31	0.2	2.4	0.1
Total	158	1.1	7.3	0.2
<b>Output/Sales (\$m)</b>				
Direct	23.2	0.3	2.5	0.2
Indirect	6.2	0.1	1.5	0.1
Total	29.4	0.3	4.0	0.2
<b>Value added (\$m)</b>				
Direct	14.5	0.5	3.0	0.2
Indirect	2.5	0.1	1.5	0.1
Total	17.0	0.6	4.4	0.2

Table 23: Value of tourism and hospitality

There were an average of 5,743 international visitors to the City of Kwinana, with 69.6% of those international visitors, visiting friends and relatives. This demonstrates that Kwinana is not a traditional tourism destination and that the main reason for visitors to Kwinana is for social connection with residents.

The City lacks a profile locally, nationally and internationally and this is reflected in the tourism numbers and the reasons for visitation.

### 5.9.1 Key issues

Whilst Kwinana is not seen as a tourism destination, there are a number of tourism opportunities for eco-tourism and industrial tourism that have the potential to be pursued.

### 5.9.2 Response to issues

Opportunities for tourism destinations, experiences and promotion needs to be considered in the context of the City's sustainability focus. Should the City pursue these options, further research and investigation should be undertaken, potentially through an economic development strategy or as part of any feasibility study for an individual project.

## 5.10 Services and businesses

Within the City there are a total of 1945 businesses in Kwinana and while there are many large businesses located in the KIA, approximately 1851 of these, or 95%, are small businesses. Of these, 1264 of the businesses are sole traders, with the majority working in Transport, Construction, and Professional roles.

While Kwinana is home to some 13,270 jobs, there has been an ongoing decline in traditional manufacturing jobs, with high growth in small business numbers. In fact, despite Kwinana appearing to be the home of large industries, there are only 68 businesses based in Kwinana that employ over 20 people. The major areas of small business as set out in Table 24: Number of businesses by Industry type are:

Industry	Number of businesses
Transport, Postal and Warehousing	355
Construction	272
Professional, Scientific and Technical Services	116
Rental, Hiring and Real Estate Services	260
Financial and Insurance Services	122
Administrative and Support Services	109

Table 24: Number of businesses by Industry type

Over the last year, businesses grew at over 2 per week to 134 new businesses commencing in Kwinana, appearing to be exclusively small businesses.

Given the high unemployment faced in Kwinana, as established in Table 19: 2016 Employment Status, the City is cognisant of the need to support small business start-ups to help address this problem.

An expanding population will mean increased demand for local population-driven services (e.g. retail, education, health) and traditionally these services provide employment opportunities for around one-third of the population. However, the delivery of these services is changing rapidly and this has some implications for land use in Kwinana. In brief:

- There is a significant trend towards home based health services and more day surgery with reduced emphasis on big hospitals, except for acute needs and specialist services.
- Rockingham Hospital is a reasonable distance from Fiona Stanley Hospital (FSH) and has some capacity to expand. Rockingham will deliver increased day surgery and specialist services and this will be the acute and specialist hospital for Kwinana. It is likely to be the primary regional health campus for the long term, with any additional future hospital likely to be located much further south in the Peel area. The time frame for this is long.

These changes mean the physical focus of health services in Kwinana will be in the form of community-focused health services that might include expanded GP services, tele-connected to specialists with visiting services to patients' homes, ideally located within the City Centre.

There are high prospects that expanded uses arising from the on-line economy (for example dark supermarkets and dark kitchens catering solely for on-line purchases) will increase demand for well-located light and general industrial land. There are also high prospects that the large labour pool in the metropolitan south west and Peel and long commuting times to the inner Perth employment areas, exacerbated by expansion limits on the regional road and rail network, will provide conditions for the establishment of a substantial business park / service commercial park in the region.

The City's location provides the opportunity for very large format retail outlets, which have a metropolitan wide catchment, to locate on the Kwinana Freeway and the main east-west freight routes to the Port, within the growing southern sub-regional areas.

Properties around the off ramps to the Freeway, particularly at Thomas Road with its proximity to the Kwinana rail station and freight and logistics and servicing opportunities, are well placed to take advantage of this exposure. A significant potential benefit is that large format retail employment opportunities offer the best option to make inroads into unemployment in younger age groups which is well above average in Kwinana and train based access from the City and Road links to the Kwinana Industrial area offer good connectivity for office based uses that could locate to this area.

#### 5.10.1 Key issues

Small business is a major contributor to the economy and employment of Kwinana and its residents. Barriers to enabling small business to establish and operate within the City through over regulation should be addressed and as small businesses expand they should be supported through the identification of suitable land to cater to their needs.

Health services in Kwinana will likely become community-focused that might include expanded GP services, tele-connected to specialists with visiting services to patients' homes. These services will need a central hub from which to operate and a good location for this would be the City Centre.

The City does not have any office or service commercial based locations that can support expanding local or newly establishing businesses.

There are no suitable locations for large format retail businesses that could help address local unemployment.

The City's strategic freeway locations can capitalise on the access and location advantages that it currently has and provide opportunities for strategic employment and economic growth (i.e. jobs to support the anticipated population increase).

#### 5.10.2 Response to issues

The City should seek to support small business through appropriate zoning permissibility's and a whole of Council approach to facilitating business within Kwinana.

Land use permissibility should support the location of medical based uses within the City centre. This would support the community as a whole in terms of providing necessary services and also support the viability of the City centre. It may require land use permissibility in other lower order centres to discourage location of medical based businesses to those lower order centres.

The City should expand locations for large format retail businesses to locate in and around the freeway interchanges. This is already occurring at Thomas Road and should be further encouraged and strengthened.

In addition, there is an opportunity for an office precinct to develop at the Thomas Road / Freeway intersection and around the Kwinana Train Station as outlined in Figure 40: Potential office business precinct. Sites here have easy access to the WTC and could service activities there.

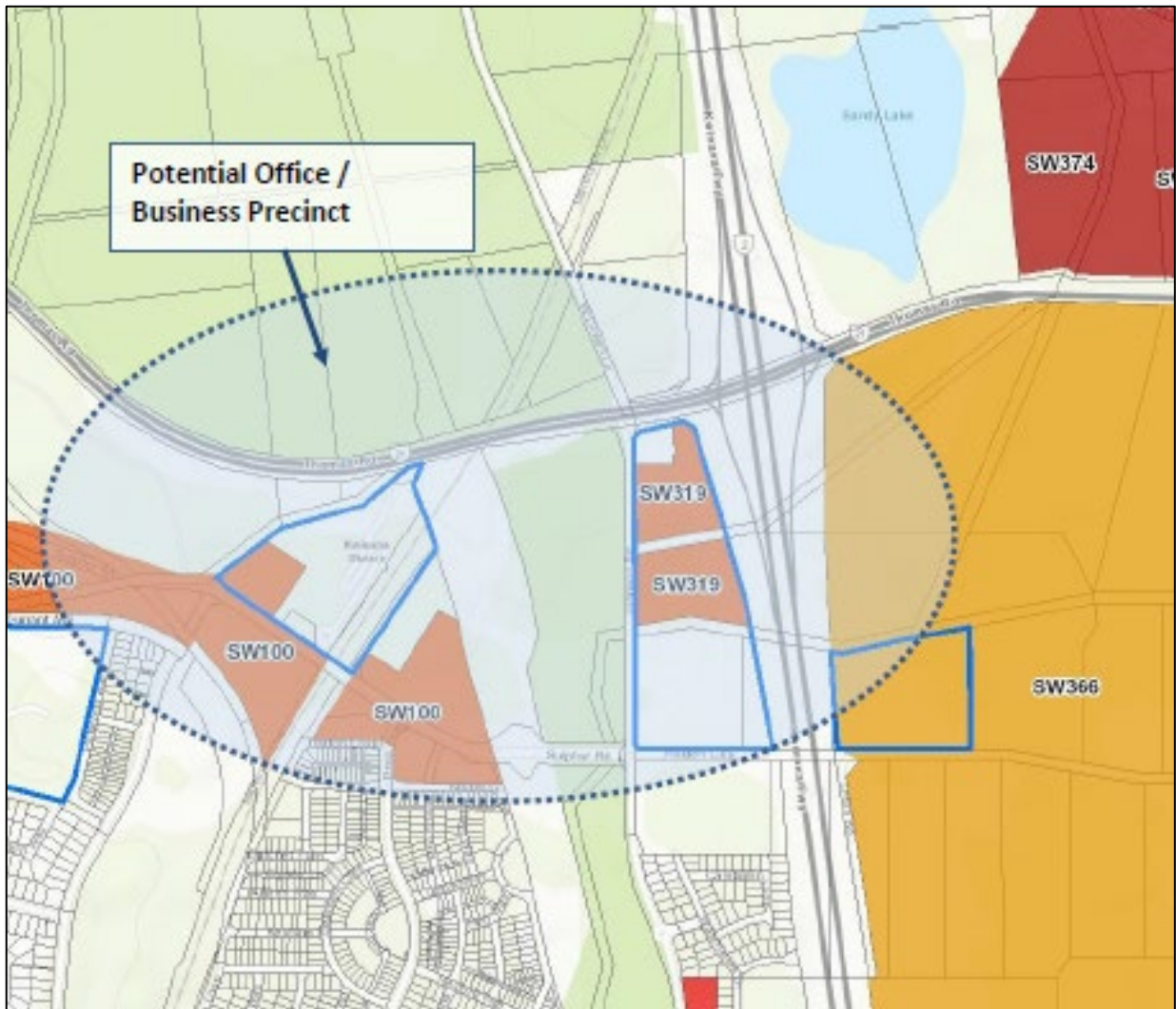


Figure 40: Potential office business precinct

## 5.11 Agriculture

The City undertook an 'Employment and Economic Analysis' in 2020 to estimate employment growth and value added to the economy based on major land use categories.

In 2019, 414 jobs were specifically related to Agriculture, Forestry and Fishing, comprising 2.4% of all jobs within Kwinana. This sector has remained relatively stable, with 41 new jobs being added since 2013, as shown in Table 25: Employment by Industry – Agriculture, Forestry & Fishing.

Industry	2018/19			2013/14			Change 2013/14 - 2018/19
	People employed	%	Western Australia	People employed	%	Western Australia	
Agriculture	336	1.9	2.1	329	2.1	1.9	+6
Aquaculture	11	0.1	0.1	10	0.1	0.1	+1
Forestry and Logging	6	0.0	0.1	4	0.0	0.1	+2
Fishing, Hunting and Trapping	19	0.1	0.1	3	0.0	0.1	+15
Agriculture, Forestry and Fishing Support Services	43	0.2	0.2	25	0.2	0.2	+18
<b>Total</b>	<b>414</b>	<b>2.4</b>	<b>2.6</b>	<b>372</b>	<b>2.3</b>	<b>2.4</b>	<b>+42</b>

Table 25: Employment by Industry – Agriculture, Forestry & Fishing

Industry	2018/19			2013/14			Change 2013/14 - 2018/19
	\$m	%	Western Australia	\$m	%	Western Australia	
Agriculture	59.1	1.8	2.3	61.9	1.7	2.3	-2.8
Aquaculture	0.8	0.0	0.0	1.1	0.0	0.1	-0.3
Forestry and Logging	1.3	0.0	0.2	0.7	0.0	0.1	+0.6
Fishing, Hunting and Trapping	3.4	0.1	0.1	0.3	0.0	0.1	+3.1
Agriculture, Forestry and Fishing Support Services	7.3	0.2	0.2	3.3	0.1	0.2	+4.0
<b>Total</b>	<b>71.8</b>	<b>2.2</b>	<b>2.8</b>	<b>67.3</b>	<b>1.8</b>	<b>2.9</b>	<b>+4.5</b>

Table 26: Value added by Industry - Agriculture, Forestry & Fishing

In 2019, Agriculture, Forestry and Fishing added \$71.8m to the Kwinana economy, comprising 1.8% of economic activity within the City. The agriculture sector itself has reduced over time, but interestingly the support services have increased as shown in Table 26: Value added by Industry - Agriculture, Forestry & Fishing.

It should be noted that the Costa Mushroom farm located in Casuarina employs approximately 200 people and thus is about half of the City's agricultural activity, in terms of people employed. The Mushroom Farm is located on urban zoned land and there is an associated approved structure plan zoning the site as Service Commercial which permits the Mushroom Farm on that site. There are also structure plans being assessed for urban development abutting the Mushroom Farm and the land use interface between the Mushroom Farm and urban development is being resolved through structure planning processes.

Also of note is the stock holding yard located across the southern border in the City of Rockingham. Whilst this operation is not within the City its buffer area also impacts on urban zoned land within Kwinana.

The large majority of other agricultural activity is taking place on rural zoned land that, as discussed in 5.2 Rural living, has been indicated in the Sub-regional framework as industrial expansion and investigation areas.

There are also agricultural businesses operating in the rural and special rural zones of the City, however, as discussed in 3.3.3 SPP 2.1 The Peel-Harvey Coastal Plain Catchment and 3.3.4 SPP 2.3 Jandakot Groundwater Protection, these locations are affected by the Peel-Harvey Estuarine System and/or the Jandakot Water Mound and as such may not be appropriate in these locations.

#### 5.11.1 Key issues

Changes from the Sub-regional framework will result in the relocation of agricultural businesses and there is little suitable land within Kwinana for those businesses to locate on.

Urban development pressures are impacting on the operations of intensive agricultural businesses, specifically where there are buffers required for their operation.

Local agricultural uses located on water sensitive areas may be having an environmental impact.

#### 5.11.2 Response to issues

The City should investigate appropriate land uses that can locate within the City. This may result in intensive agricultural uses not being supported where they impact upon water sensitive areas.

Whilst agricultural uses continue operation in locations experiencing urban development pressures, urban development will need to acknowledge their presence and structure planning and subdivision design will need to respond accordingly.



## 5.12 Basic Raw Materials

Non-metallic mineral mining and quarrying operations account for \$15.4 million or 0.5% of the City's economic activity. These locations have been mapped and are discussed under section 3.3.5 SPP 2.4 Basic Raw Materials.

This generally occurs within the Spearwood landform unit, which has high value limestone resource extraction potential, and in some Bassendean sand landform units, which supply sand to the metropolitan development industry. These supplies are limited and thus continued extraction is not sustainable.

As the City is becoming increasingly urbanised and developed, the ability to access these materials is becoming increasingly constrained. There is also an increasing need to protect the valued landscape and ecological values that comes with the remaining undeveloped areas and these factors should play a greater role in the decision making process for allowing access to basic raw materials.

The need to reduce the requirement for basic raw materials in all stages of the development process will become increasingly important for future development alongside sourcing alternative construction materials in order to reduce the reliance on basic raw materials and improve sustainability outcomes.

### 5.12.1 Key issues

Basic raw material extraction often occurs on environmentally valued landscapes which are becoming increasingly rare in themselves. As the City continues to grow, material extraction will have a negative impact on the community when located near settlement areas.

Basic raw materials are also a finite resource.

### 5.12.2 Response to issues

The overall driving need for basic raw materials should be reduced through all stages of the development process and the City should support developments that seek to minimise their basic raw material use through good design or the use of alternative materials, particularly where they meet other sustainable objectives.

## Environment



## 5.13 Landform & Landscape

### 5.13.1 Landform

The City is divided into 3 major landform units. These are the Quindalup Dune System to the west, Spearwood Dunes System in the centre, and the Bassendean Sands System to the east. At the interface of these are a chain of wetlands associated with peaty lacustrine material, broadly described as "Herdsman Deposits". Figure 41: Landform Units depicts these landform units.

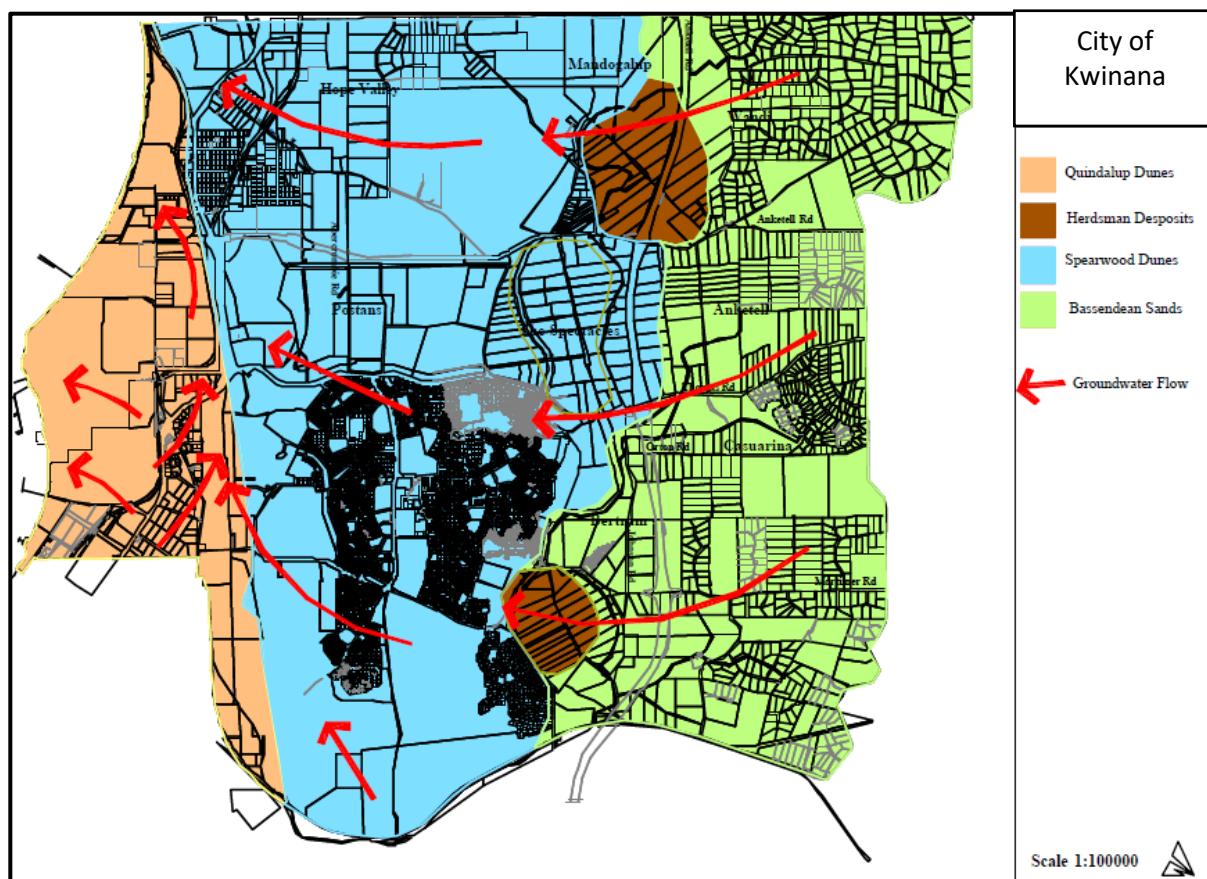


Figure 41: Landform Units

The characteristics of the soil types associated with the Spearwood and Bassendean Systems have implications on the use scenarios for the area. Firstly, soil fertility dictates the suitability of the land to sustain agriculture without major nutrient input and without soil degradation. Secondly, the extent to which the soil types are able to absorb, fix, negate or convert contaminants produced by the range of land use activities, will dictate the capability of the land to support a range of land uses.

As a general rule, the more highly leached sands have a lower fertility rating and a lower capacity to assimilate contaminants than less leached soil types. The ability of the soil types to act as a microbiological filter clearly has consequences in terms of public health given that groundwater is often used for domestic consumption.

#### 5.13.1.1 Spearwood Dunes

The western extent of the Spearwood Dune System consists typically of coastal limestone (Tamala Limestone) thinly overlain by yellow brown siliceous sand, of the Cottesloe Soil group. Wind erosion over the geological time frame, has exposed north to south aligned limestone ridges, which represent a high value regional resource.

The eastern section of the Spearwood Dune System comprises deep yellow soils of the

Karrakatta Soil type, which overlays Tamala Limestone and is often exposed in the western sectors. The Spearwood Dune System, although generally infertile, is more fertile than the Bassendean Sands System to the east.

#### 5.13.1.2 Bassendean Sands

The Bassendean Sands consists of low dunes of deep grey siliceous sands and bleached sands, overlaying a shallow iron-organic hardpan. Certain sections of the Bassendean Sands are thinly overlain by Guildford soils (yellow duplex sands). Because of their highly leached nature, the soils have little ability to sustain agricultural activities without major application of fertilizers. Equally they have little or no phosphorous retention ability and limited ability to fix nitrogen contaminants. They do however possess a greater denitrification ability, than Spearwood Soil associations. Much of the landform unit is low lying with minimal separation from groundwater and as such transportation of contaminants to the water table is rapid. For this reason, significant areas of Wandj, Anketell and, to a lesser extent, Casuarina are designated as Public Drinking Water Source Areas (PDWSAs), also referred to as the Jandakot Water Mound and proclaimed under the Metropolitan Water Supply, Sewage and Drainage Act, 1909. These areas restrict land-use activity to protect regional drinking water quality. Consequently, in the City of Kwinana, this has dictated restrictions on urban expansion into these areas.

#### 5.13.1.3 Herdsman Deposits

The Herdsman Deposit soil system lays in a generally north-south alignment at the interface of the Spearwood and Bassendean landform units and consist of a series of major wetland systems. Their associated lacustrine deposits are interspersed with pockets of Spearwood and Bassendean Soil associations and elements of landforms of each adjacent system.

The Herdsman Deposits comprise predominantly of wetland associated soils of lacustrine origin, with a predominance of peat and loam deposits of a high organic content associated with highly fertile soils.

#### 5.13.1.4 Quindalup Dunes

The Quindalup Dunes form a series of elongated dunes that run parallel to the current coastline forming a narrow strip between the coast and the Spearwood System. These soils are, geologically, the youngest soil type on the Swan Coastal Plain. There are two major alliances; the mobile fore-dune alliance and the stable dune alliance. These free draining soils consist of calcareous sands, unconsolidated quartz grains and shell fragments which are high in lime and are therefore alkaline. The Quindalup Dunes support low, closed forests of Melaleuca and closed heath and scrub vegetation closer to the coast. Lakes and swamps occur in the stable dune alliance where there are deposits of peaty Herdsman Soils.

### 5.13.2 Landscape

The landscape features of an area define a particular character and sense of place for its community. The landscape comprises elements of topography, vegetation, water and the built environment. While the 'landscape' of an area can be described as the totality of these features, specific features, which represent the extremes of these components, contribute most to the area's identity and character.

The City's landscape character is defined by the low, closed scrublands of the coast, bordered by a ridge of dunes of open Tuart (*Eucalyptus gomphocephala*) forest and closed limestone heaths, transitioning to open forests of Tuart, Jarrah (*Eucalyptus marginata*) and Marri (*Corymbia calophylla*) interspersed by a chain of north-south oriented wetlands and bordered on the east by low woodlands of Banksia, Marri, Jarrah and Sheoak (*Allocasuarina*).

The elements of a landscape are broken into three levels. These are:

- Individual precinct landscape elements;
- District landscape elements, the influence of which extends beyond individual precincts; and
- Regional landscape elements that add to the landscape identity of the region, such as The Spectacles Wetlands.

Figure 42: Landscape analysis depicts a landscape analysis of the City of Kwinana.

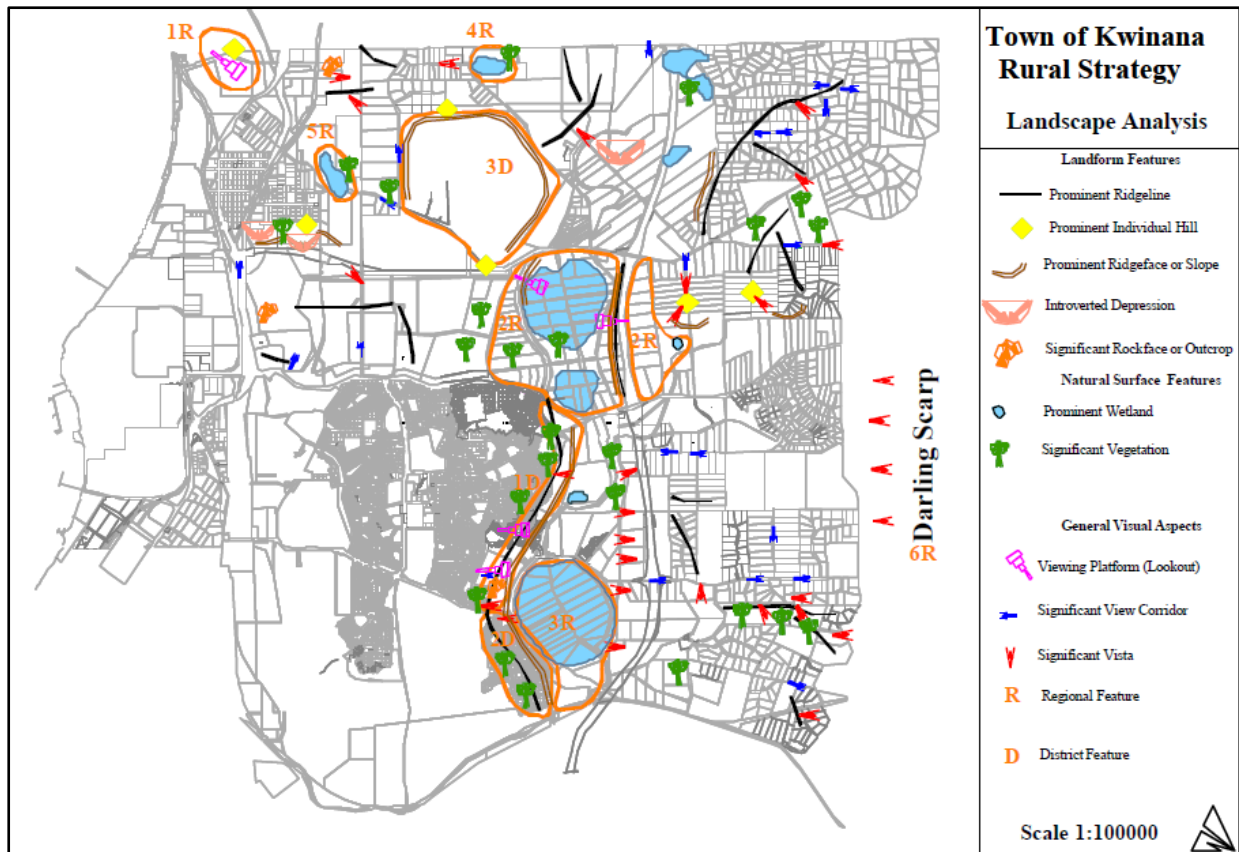


Figure 42: Landscape analysis

The City's Local Planning Scheme No. 2, through Section 6.16.2 Areas of landscape protection (ALP) provides for measures to be adopted for conserving the natural ecological values and amenity values existing in those areas, while allowing development. In Zoning Table No. 1 of the Scheme, the following objectives are set out:

In any ALP no person shall, without City planning approval in writing:

- Carry out clearing of trees or other vegetation;
- Carry out any filling, dredging or changes to the contour of the land;
- Erect any advertising sign;
- Erect or construct any building or outbuilding;
- Degrade any natural wetland system or vegetative complex; and
- Detract from the amenity of the locality.

In considering an application for planning approval for any land within an ALP, the City shall have regard to the following:

- The overall impact of the proposed development on the landscape amenity;

- The need for conditions and management plans to avoid, mitigate and/or manage any impacts of a proposal; and
- The extent to which any subdivision proposal should provide for the protection of natural ecological features and/or areas of landscape amenity and their subsequent ceding to the crown.

### 5.13.3 Key Issues

Each landform unit and associated soil complex imposes limitations on its land-use. An example of this is the free draining, low denitrification qualities of the Bassendean Sand System restricting urban expansion over the Jandakot Groundwater Mound. A summary of land-use limitations for the City's soil types include:

- Nutrient contamination of wetlands by contaminated surface or groundwater, particularly from agricultural, industrial and urban development;
- Public health problems may arise from contaminated groundwater which is subsequently abstracted via bore for domestic consumption;
- Land use activities within the Spearwood Dunes landform unit, which generate residual nitrogen contamination, create the risk of environmental and public health problems related to groundwater and surface water contamination;
- Land use within the Herdsman Deposit landform unit, as a result of high fertility and nutrient retention, require lower levels of fertiliser application. Excessive applications of nutrients result in rapid contamination of groundwater and eutrophication of wetlands; and
- Conventional effluent disposal systems are inadequate in the Bassendean Sands landform unit, and Nutrient Retentive Effluent Disposal Systems (NREDS) are compulsory to reduce the nutrient levels permeating into groundwater.

Whilst protection measures are in the scheme that require landscape to be taken into consideration as part of development approval, the resulting development outcomes do not appear to be preserving landscape features as intended by the landscape analysis.

This is particularly the case with newly developed areas where landscape analysis does not feature heavily in the site context of structure plans.

### 5.13.4 Response to issues

The City will seek to protect landscape features and sustain the City's landscape character through the identification of these features. A revised Landscape Analysis may be required to achieve this.

The City will also ensure appropriate land use and sustainable development outcomes by building on the objectives set out in LPS No. 2 to conserve and protect its remaining natural areas.

## 5.14 Water Resources

### 5.14.1 Hydrology

Land-use and development within the City is influenced to a large degree by water related issues. The existence of the Jandakot Water Mound, wetlands and seasonal expression of ground-water in areas with high ground-water tables limits the potential for development. Increasingly, sea level rise and coastal recession will require consideration of planning conditions that limit time or particular events.

The vast majority of the eastern and central portion of the City are within the Peel Estuary Serpentine River Catchment, this also includes the Jandakot Water Mound. To the west, Kwinana Beach and Naval Base constitute the Coastal Catchment, with the northern and central areas of Postans, Hope Valley and Mandogalup falling within the Bartram Road Catchment. Figure 43: Catchments in the City of Kwinana shows the City's water catchment areas.

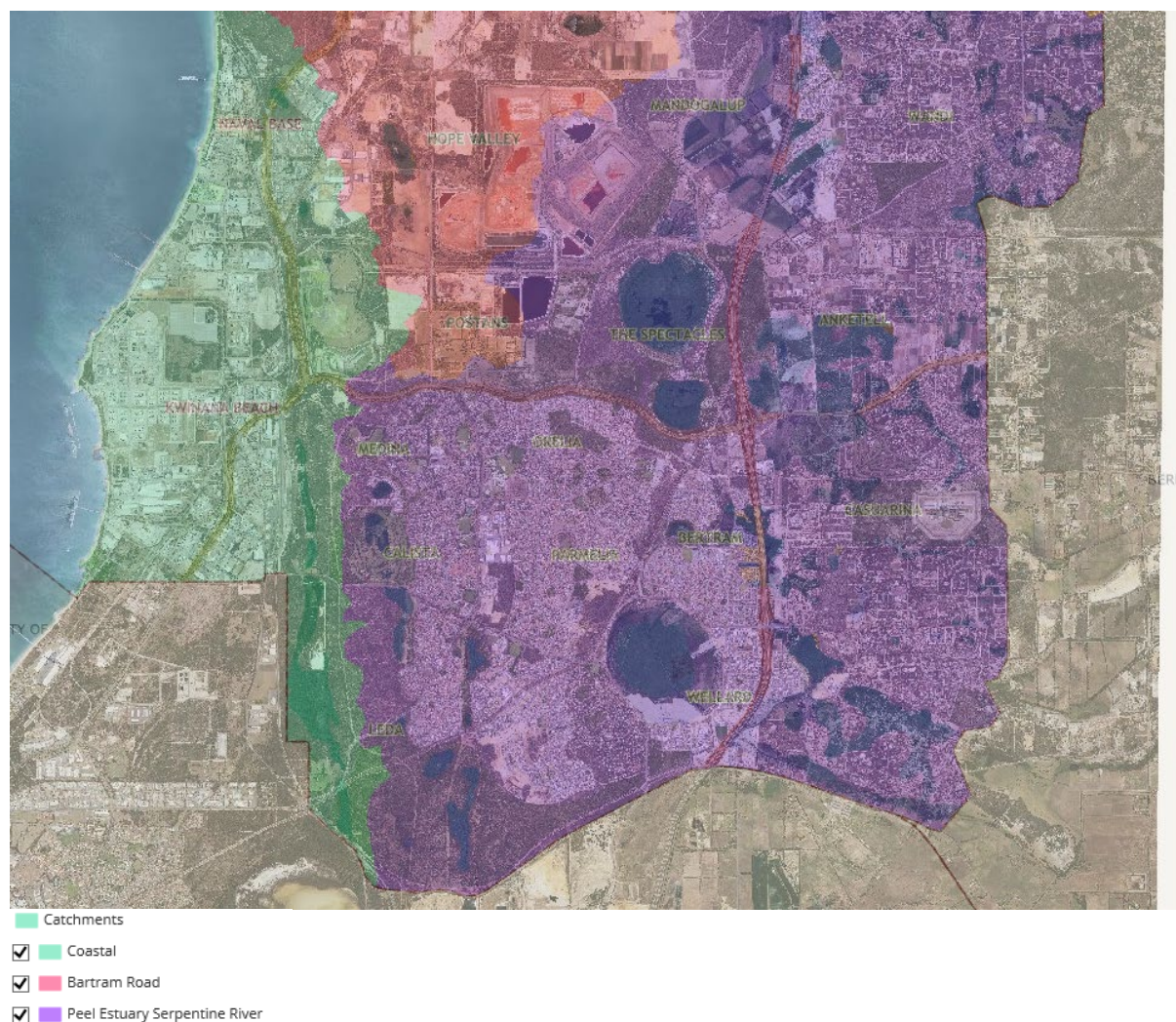


Figure 43: Catchments in the City of Kwinana

#### 5.14.1.1 Ground Water

The Jandakot Ground Water Protection Area is critical to the public water supply, in that the gazetted area protects the superficial layer of the Jandakot Groundwater Mound. One third of Perth's scheme water is currently drawn from groundwater supplies. Protection of this value resource restricts land-uses to ensure the quality of ground-water is not compromised. As such, land uses which place disproportionate levels of demand on groundwater quantities for private

usage and those which lead to degradation of groundwater quality are discouraged. Areas of Wandii, Anketell and Casuarina are within the Jandakot Ground Water Mound, consequently restricting development within these areas.

Groundwater flow throughout the City is generally in an east to west direction with minor variation in flow direction influenced by the presence of wetlands. As groundwater approaches the western extremity of the Spearwood Dune System, the direction of flow is impacted by topography resulting in flow path alignment to a north to north-west directional change. Flow paths continue to travel north along the interface of the Spearwood and Quindalup Dunes before outflow into the ocean. The hydrological variation across the City is a result of its topography. Dune systems interspersed with low lying areas constitute a wide variation of hydrological influences, including:

- Groundwater levels range from zero metres AHD at Cockburn Sound to 26 metres AHD in the Wandii;
- The majority of the Spearwood Dune landforms has elevations above the water table of 20 - 30 vertical metres;
- The Bassendean Sand unit's vertical separation from groundwater is predominantly between 0-10 metres with substantial areas being less than 3 metres; and
- Seasonal surface water expression in areas associated with Herdsman Deposit soil systems.

As a result, the Spearwood Dune System has a high separation from groundwater resulting in a low threat of inundation. Areas within the Bassendean Sand System are often subject to high water tables and seasonal inundation in parts. Hydrology has implications for development and consequently dictates surface and ground water drainage. The City seeks to ensure that Water Sensitive Urban Design (WSUD) principles are applied to all new developments, whilst investigating opportunities for retrofitting existing drainage infrastructure.

#### *5.14.1.2 Drainage*

The City's drainage system consists of two distinct drainage areas, separated generally at the interface of the Spearwood Dunes and the Bassendean Sands. This interface broadly defines the drainage areas for the Peel Estuary Serpentine River Catchment.

##### Spearwood Dune System

Drainage within the Spearwood Dunes is generally 10 - 30 metres above groundwater levels in the unconfined aquifer. The porous nature of these soil associations, low clay content and the depth to the unconfined aquifer facilitates rapid surface water infiltration into the groundwater table.

As such, with the exception of perched wetlands, inundation of land for any great length of time is infrequent. Consequently, there are no public drainage systems servicing the area, except for minor systems within the Hope Valley area. Storm water is generally infiltrated immediately or transported via drainage systems, in high severity storms, to wetlands or natural depressions within the landscape. There is however the potential for localised environmental flooding resulting from high volume rainfall events within these areas. Whilst the potential for flooding is minimal, managing the water quality associated with this drainage is critical. In these areas the focus is on the application of WSUD to treat surface water flows and minimise nutrient transfer into wetlands and groundwater. Focus on policy to achieve this outcome, especially within the urban and industrial development context, is critical in protecting the groundwater quality and the City's wetlands.

##### Bassendean Sands System

This Division comprises the Bassendean Sands to the west of the City and includes the eastward sloping land adjacent to the Spearwood System. The Bassendean Sands unit is generally subject to high water table and in many areas is prone to inundation by seasonal fluctuations of the water



table. Groundwater levels in the catchment, range from 0-30 metres below natural ground level. The majority of land is 10 meters or less above the water table and there are significant areas being 3 metres or less above groundwater. In these areas there is potential for flooding and drainage must be managed, particularly within the development of urban areas, to mitigate this.

#### 5.14.1.3 Flood Risk

The Bassendean Sands and Herdsman Deposit landform units are, in some areas, prone to high groundwater levels and have the potential for sheet flooding during periods of prolonged high rainfall. However, it should be noted that a high severity rainfall event would not produce fast flowing currents because of the flatness of land in the area. Such conditions would produce wide sheeting of water which would then be transported slowly away from the area by constructed district drainage channels. Weak flood currents could occur along the north to south drainage corridor through the Herdsman Deposit landform unit.

The Spearwood Dune system is relatively low risk potential for long term flooding, except in the vicinity of Long Swamp, where the ground level is at or close to natural ground level. Development around Long Swamp has mitigated this risk by the installation of drainage infrastructure that includes management of the 1 in 100-year rainfall event.

#### 5.14.1.4 Salinity

Groundwater salinity for the City ranges from 250mg/l Total Dissolved Solid (TDS) in the Wandi area to 759mg/l TDS in the south west near the Mundijong railway line. The long-term objective for drinking water is 500mg/l and the desirable current criterion is 1500mg/l. Stock use is generally possible for <3000mg/l TDS. Industrial uses (except for boiler feed) are possible up to 1000mg/l.

Groundwater salinity levels for the area are clearly within the recommended parameters, however land use and management scenarios should be aimed at maintaining current salinity levels.

#### 5.14.2 Wetlands

Wetlands comprise topographic low-lying areas or depressions that have expression of either surface water or groundwater, seasonally or continually. These wetlands and the flora and fauna that they support, are adapted for periods of seasonal inundation and drying. Maintenance of this hydrological balance is critical to their survival. Other wetlands, such as the Spectacles, are ephemeral and maintain surface expression of water year-round.

Guidelines for the Determination of Wetland Buffer Requirements (Draft WAPC, 2005) sets out standards for the protection of Conservation Category and Resource Enhancement Wetlands. LPS2 identifies wetlands within the City as 'Areas of Landscape Protection' to ensure consistency with State requirements for the protection of wetlands impacted by development.

Wetlands, such as the Spectacles and Bollard Bullrush Swamp have been important for food, recreation, and cultural reasons for tens of thousands of years, and continue to serve these purposes today. These and other wetlands provide a home for endangered birdlife and orchids, filter our water, and provide a water storage function which protects against floods (Department of Environment and Conservation, 2012).

##### 5.14.2.1 Conservation Category Wetlands

Conservation Category Wetland (CCW) afford the highest protection and require a 50m vegetated buffer is provided in addition to the wetland boundary to ensure the ecological integrity of the wetland's flora, fauna and hydrology. In addition, the City requires developers to produce and implement a wetland management plan for any CCW retained as part of subdivision. The City also

requires developers to produce a Local Water Management Strategy for all subdivisions that addresses the pre and post hydrological balance for wetlands.

#### 5.14.2.2 Resource Enhancement Wetlands

Resource Enhancement Wetlands (REW) are the second category of wetland with conservation significance and represent wetlands with lower environmental value. State Guidelines require that a 30 metre vegetated buffer is provided for REWs to ensure the protection of the ecological function of the wetland. Accordingly, the City requires the provision of a 30 metre buffer and the preparation and implementation of a wetland management plan for all subdivisions containing REWs. Figure 44: Wetlands in the City of Kwinana shows wetlands of conservation significance within the City.

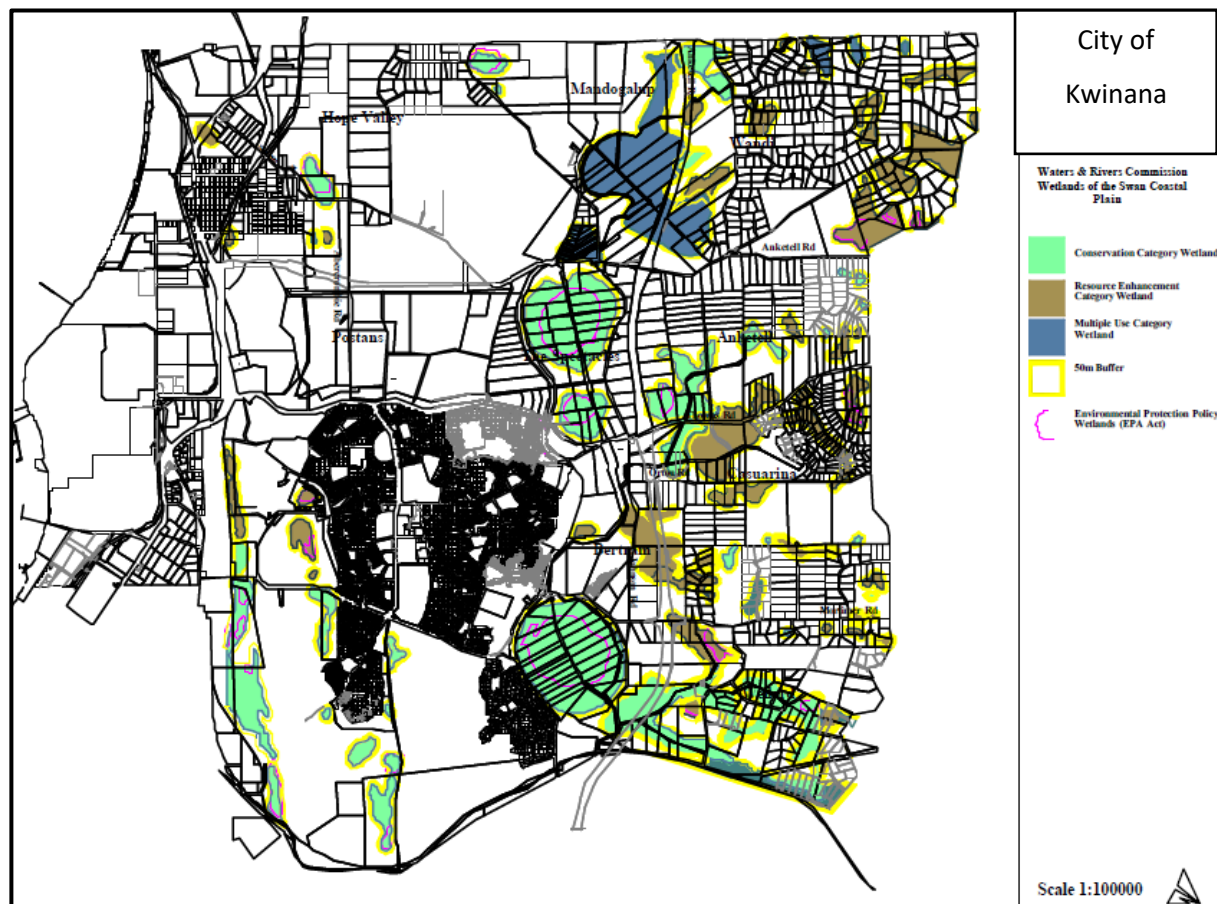


Figure 44: Wetlands in the City of Kwinana

#### 5.14.3 Key issues

Different soil, topography and land-use activities within the City have implications for drainage management. Drainage for the major catchment, the Peel Estuary Serpentine River Catchment, flows via the Peel Main Drain and outfalls to the Serpentine River 15km to the south of the City. The Peel Estuary region is an important environmental asset that supports biodiverse ecosystems. Drainage from the City has the potential to impact these ecosystems, especially with regard to water quality.

The Peel Main Drain intersects a number of regional and locally significant wetlands. Contamination of water within this drainage system has potential to directly impact these wetlands, including the Spectacles and Bollard Bulrush Conservation Category Wetland. High nutrient loads within surface water runoff into drainage systems is often associated with algal bloom in water bodies and elevated midge and mosquito breeding. The combination of high-water tables and porous free draining soil

within this catchment requires a localised drainage outcome that includes high percentile recharge of surface water to maintain wetland hydrology, whilst ensuring that flooding is mitigated.

Drainage within the Hope Valley and Coastal Catchments is subject to similar water management issues. Contamination from industry is largely considered by the State Department of Water and Environmental Regulation. However, within these areas WSUD principles will be incorporated into planning and engineering policy to manage the potential for flooding and ensure water quality is not impacted.

#### 5.14.4 Response to issues

The City should require Water Sensitive Urban Design (WSUD) to be standard for all new development with consideration given to practices that limit and manage nutrient runoff and contamination within older areas of the Peel Estuary Serpentine River Catchment.

Treatment of surface water via vegetated swales, detention basins and living streams will assist in reducing nutrient and hydrocarbon contamination of wetlands.

Within Hope Valley and Coastal Catchment areas WSUD principles will be incorporated into planning and engineering policy to manage the potential for flooding and ensure water quality is not impacted.

## 5.15 Natural Areas and Biodiversity

The rich diversity of Western Australia is in decline due to human activities. Local government can slow, stop and reverse this trend by showing leadership within the community and also encouraging local land and property owners to conserve native flora and habitats of fauna.

Naturally vegetated areas in Western Australia's cities and towns support significant biodiversity and provide important amenity to urban residents (Environmental Protection Authority 2013).

A land use framework/mechanism should recognise the importance of biodiversity while considering changes to land use, including consideration of any future potential value, such as a source of genetic material.

### 5.15.1 Biodiversity Conservation

The City commissioned a Biodiversity Strategy (Ironbark Environmental, 2013) to provide a snapshot of the biodiversity within its area. An analysis of relevant information from a range of sources at the local, regional, and national scale was undertaken. The following section provides a summary of biodiversity values that exists in the City of Kwinana and includes: flora, fauna, Threatened and Declared Rare species, wetlands and ecological communities.

#### 5.15.1.1 Vegetation Complexes

In regards to pre-European vegetation, the Perth Metropolitan Region has been interpreted based on a system of vegetation classification developed by Heddle, Longeran and Havel (1980) and Matiske and Havel (1998). These vegetation complexes contain Floristic Community Types (FCT) and have been mapped across the region in a way that allows them to be used for quantitative analysis based on area (WALGA & PBP 2004).

These vegetation complexes are formed by the underlying key determining factors of landform, soil, hydrology and climate. Each unique vegetation complex supports a biodiverse assemblage of flora, fauna and ecosystems that gives the City a unique environmental character and sense of place.

Within the Perth metropolitan area, a vegetation complex that has less than 10% of its original area remaining is considered a Threatened Ecological Community. Regionally, 30% of any vegetation complex is the threshold to which retention is benchmarked. Unlike many Local Governments on the Swan Coastal Plain, the City has remnant representatives of all but one vegetation complex that historically occurred within its boundaries.

These include the Quindalup, Cottesloe (Central and South), Karrakatta (Central and South), Herdsman, Serpentine River, Bassendean (Central and South), and Guilford Complexes. Five of these vegetation complexes: Bassendean (Central and South), Herdsman, Guilford, Karrakatta (Central and South) and Cottesloe (Central and South) are considered regionally significant, as less than 10% of the original pre-European extent is found within formally protected reserves.

Figure 45: Original extent of vegetation complexes in the City of Kwinana (adapted from the Local Biodiversity Strategy Draft for Public Discussion 2007) shows the original extent (pre-European) of vegetation complexes that existed in the City.

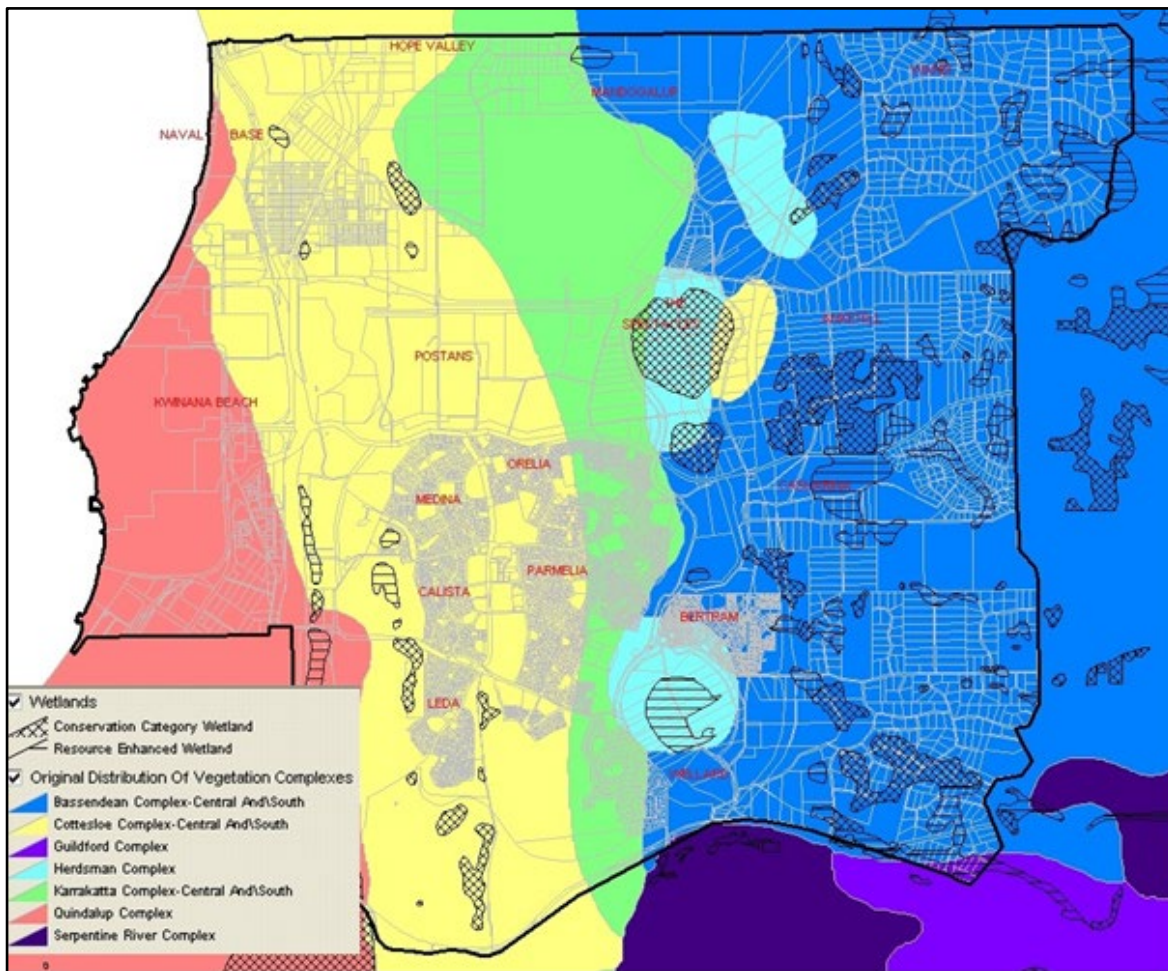


Figure 45: Original extent of vegetation complexes in the City of Kwinana (adapted from the Local Biodiversity Strategy Draft for Public Discussion 2007)

#### [Bassendean Vegetation Complex Central & South](#)

Vegetation complexes within the City are, in general terms, an expression of the underlying geology. The ancient, nutrient leached, free draining Bassendean soils that occur in the central and eastern areas of the City support the Threatened Ecological Communities (TEC) of Banksia Woodlands. Protected under the State Government Environmental Protection Act 1986 (EP Act), and the Federal Government Environmental Protection and Biodiversity Conservation Act 1999, substantial areas of this TEC align with the Jandakot Groundwater Protection Area. Most occurrences of the remaining Bassendean Central and South Vegetation Complex are located within the Rural A, Rural B, Special Rural, and Development zoned areas of the Wandi, Anketell, Casuarina and Wellard suburbs.

Typically, the Bassendean Central and South Complex is made up a number of different Floristic Community Types (FCT) including Banksia-dominated woodlands, wetlands with flooded gums (*Eucalyptus rudis*) and paperbarks (*Melaleuca preissiana*), or heathlands of *Pericalymma ellipticum* and *Hypocalymma angustifolium* (Clarke & Keighery 2005).

The opportunity to conserve and protect these diminishing and iconic Floristic Community Types within the City are becoming increasingly limited. Urban development has resulted in the clearing of significant remnant areas of Banksia Woodland. LPS2 therefore seeks to maximise the retention of this vegetation complex through local planning instruments, particularly within Casuarina where large, intact, good condition areas of this vegetation remain. Of particular importance, Lot 123 Mortimer Road, Casuarina is a regionally significant, 45 ha remnant, privately owned Banksia Woodland containing Conservation Category and Resources Enhancement Wetlands. The sites

ecological values are locally and regionally acknowledged (pers.com. DBCA, 2020) and the City, though the planning process, has strongly advocated, and continues to advocate, for the inclusion of the site to the State's Conservation Estate.

#### [Herdman Vegetation Complex](#)

The Herdsman Vegetation Complex is typically associated with the peaty soils of ephemeral and seasonal wetlands. This vegetation type includes woodlands of flooded gums (*Eucalyptus rudis*) through to dense heaths and sedge-lands that seasonally express ground and/or surface water, or are wet all year round. This complex denotes a chain of regionally significant wetlands that run in a north-south orientation along the greater Swan Coastal Plain. In the low-lying depressions at the edges of the Karrakatta and Bassendean Vegetation Complexes, these sedge-lands and damp-lands provide habitat for international migratory birds and an important ecological link to water supplies for fauna.

Many of the Herdsman Complex Wetlands are within Jandakot Regional Park or Beelihar Regional Park. These important wetlands afford protection under State and Federal Government legislation, with some (within the Cities of Cockburn & Armadale) protected under the International RAMSAR Convention on Wetlands 1971. When considering the overall extent in hectares of this floristic community, it falls within the third lowest percentile. With less than half of its original extent remaining, around 250 hectares, only 28% is formally protected. Of the remaining wetlands in the City around 101 hectares is within Local Natural Areas, that is in private property with no formal protection.

Of particular importance are two large areas of Herdsman Vegetation Complex associated with Conservation Category Wetlands, Bollard Bulrush, in Wellard and a series of wetlands within Casuarina. The City, through the planning instrument, community education, stewardship programs and advocacy, seeks to protect and conserve the remaining Herdsman Vegetation Complex within the Local Government area to and work jointly with State Government agencies and community groups to manage for conservation wetlands within Jandakot & Beelihar Regional Parks.

#### [Karrakatta Vegetation Complex Central & South](#)

The Karrakatta Central and South Vegetation Complex consists of predominantly open forest of *Eucalyptus gomphocephala* (Tuart), *Eucalyptus marginata* (Jarrah), *Corymbia calophylla* (Marri) and woodland of *Eucalyptus marginata* (Jarrah) and Banksia species (<https://catalogue.data.wa.gov.au/dataset>, 2020).

This vegetation exists between the Cottesloe and Bassendean Vegetation Complexes and contains Critically Endangered Tuart Woodlands (EPBC Act, 1999). With only around 30% of the pre-European extent of this vegetation type remaining and 7.35% formally protected, it is critical that the City endeavours to protect and conserve the circa one hundred hectares that remain as Local Natural Areas.

#### [Cottesloe Vegetation Complex Central & South](#)

The deep yellow sands over limestone ridges that support the Cottesloe Central and South Vegetation Complex and provides mosaics of woodlands of *Eucalyptus gomphocephala* (Tuart) and open forest of *Eucalyptus gomphocephala* (Tuart), *Eucalyptus marginata* (Jarrah) and *Corymbia calophylla* (Marri) with closed heath on the Limestone outcrops species (<https://catalogue.data.wa.gov.au/dataset>, 2020).

This vegetation exists at the western extent of the Spearwood Dune System and provides a band of distinct north-south oriented Tuart Woodlands that separate the City center and coastal industrial areas. The unique and distinct Tuart Woodland is considered Critically Endangered under the EPBC

Act 1999. More than 40% of the LNA of this vegetation complex is zoned Residential, General Industry, or Town Centre and therefore provides few opportunities for natural area protection.

#### [Quindalup Vegetation Complex](#)

The Quindalup Vegetation complex is typically coastal vegetation and includes vegetation types on dune ridges and the seasonal wetlands that sometimes form in between the dunes. Coastal dune complex consisting mainly of two alliances - the strand and fore-dune alliance and the mobile and stable dune alliance. Local variations include the low closed forest of *Melaleuca lanceolata* (Rottnest Teatree) and *Callitris preissii* (Rottnest Island Pine), the closed scrub of *Acacia rostellifera* (Summer-scented Wattle) and the low closed *Agonis flexuosa* (Peppermint) forest of Geographe Bay. With only 1.43% of this vegetation formally protected, the main threats include clearing for industrial land-use.

#### [Serpentine River Vegetation Complex & Guilford Vegetation Complex](#)

With only around three hectares of both the Serpentine River and Guilford Vegetation Complexes existing as remnant vegetation within the City, none of which is formally protected, there is limited opportunities for conservation of these rare vegetation communities. Existing in the most south-eastern extent of the City, three hectares of Guilford Complex falls within Rural Zoned LNAs, with three hectares of Serpentine River Complex zoned under the MRS for Parks and Recreation.

Table 27: Status of Vegetation Complexes shows the pre-European extent of all vegetation complexes within the City and the remaining percentile as of 2010 of each. Also shown is the percentage of remaining vegetation types that are formally protected and how much of these are within LNAs. Overall, 38 per cent of the pre-European extent of native vegetation remains in the City. Approximately 56 per cent of these remnant vegetation communities are within LNAs, however they do not have formal protection as compared to remnant vegetation in Bush Forever sites, and DBCA and DPLH managed reserves.

<b>Vegetation Complex</b>	<b>Pre-European extent</b>	<b>2010 Remnant vegetation extent</b>	<b>Pre-European extent remaining</b>	<b>Pre-European extent formally protected</b>	<b>Local Natural Area</b>
	Ha	Ha	%	%	Ha
Bassendean Complex - Central and South	4,679	1,948	41.65	6.93	1,337
Cottesloe Complex-Central and South	3,790	1,450	38.25	11.51	621
Guildford Complex	19	3	14.22	0.00	3
Herdsmen Complex	579	274	47.26	28.94	101
Karrakatta Complex-Central and South	1,634	564	34.50	7.35	319
Quindalup Complex	1,274	352	27.66	1.43	196
Serpentine River Complex	4	3	83.39	0.00	0
<b>Total</b>	<b>11,978</b>	<b>4,594</b>	<b>38.35</b>	<b>8.90</b>	<b>2,576</b>

Table 27: Status of Vegetation Complexes

#### 5.15.1.2 Distribution of Areas having Biodiversity Values

The hierarchical arrangement of natural areas with the City, according to biodiversity and conservation values, has been determined by the State and Federal Government. Table 28:

Government Agencies and Biodiversity Conservation identifies these conservation areas and shows their protection status and total area in hectares. This base-line data has been sourced through desktop studies, obtained from reliable sources including; approved Local Structure Plans, the Perth Biodiversity Project, and the Metropolitan Region Scheme. The data excludes Local Natural Areas (LNAs) in Public Open Space (POS) as a result of the subdivisional processes.

Originally, the City had 11,980 hectares of natural areas (i.e. pre-European). Table 28: Government Agencies and Biodiversity Conservation shows that of these natural areas only 4,490 hectares remain. Of these, 2,342 hectares are protected and managed by the State Government, and 2,148 hectares of LNAs remain.

Agencies	Main Categories of Natural Areas	Protection Status Categories	Area in the City (ha)
State Government	Bush Forever Sites Department of Biodiversity, Conservation and Attractions (DBCA) managed Estates: National & Regional Parks Nature Reserves Conservation Parks Parks and Recreation Reserves through the Metropolitan Region Scheme (MRS)	Native Vegetation Exists in DBCA Estate and Regional Parks Vegetated and non-vegetated Conservation Category Wetlands and Resource Enhancement Wetlands Parks and Recreation Reserves	2,342
City of Kwinana	Local Natural Areas, but exclude: DBCA managed estates; Regional Parks; and Bush Forever sites not managed by Local Government or in private ownership.	Public Open Space (POS)	2,148

Table 28: Government Agencies and Biodiversity Conservation

Most LNAs that exist in Parks and Recreation reserves also cater for a range of recreational uses. Such activities have the potential to adversely affect the conservation of biodiversity values that exists in those areas, such as ovals and sporting complexes. In such cases, the City should seek to have these natural areas retrospectively rezoned for Conservation.

### 5.15.1.3 Natural Area Reservation under the Metropolitan Region Scheme

Figure 46: Vegetated Areas within the MRS and Table 29: Extent of Local Natural Areas (by vegetation complexes) based on MRS Zones (2013), provides a spatial and statistical analysis of natural area reservation under the MRS. Notably, large areas of remnant vegetation are being cleared for subdivision and development in Urban and Urban Deferred zoned lands.



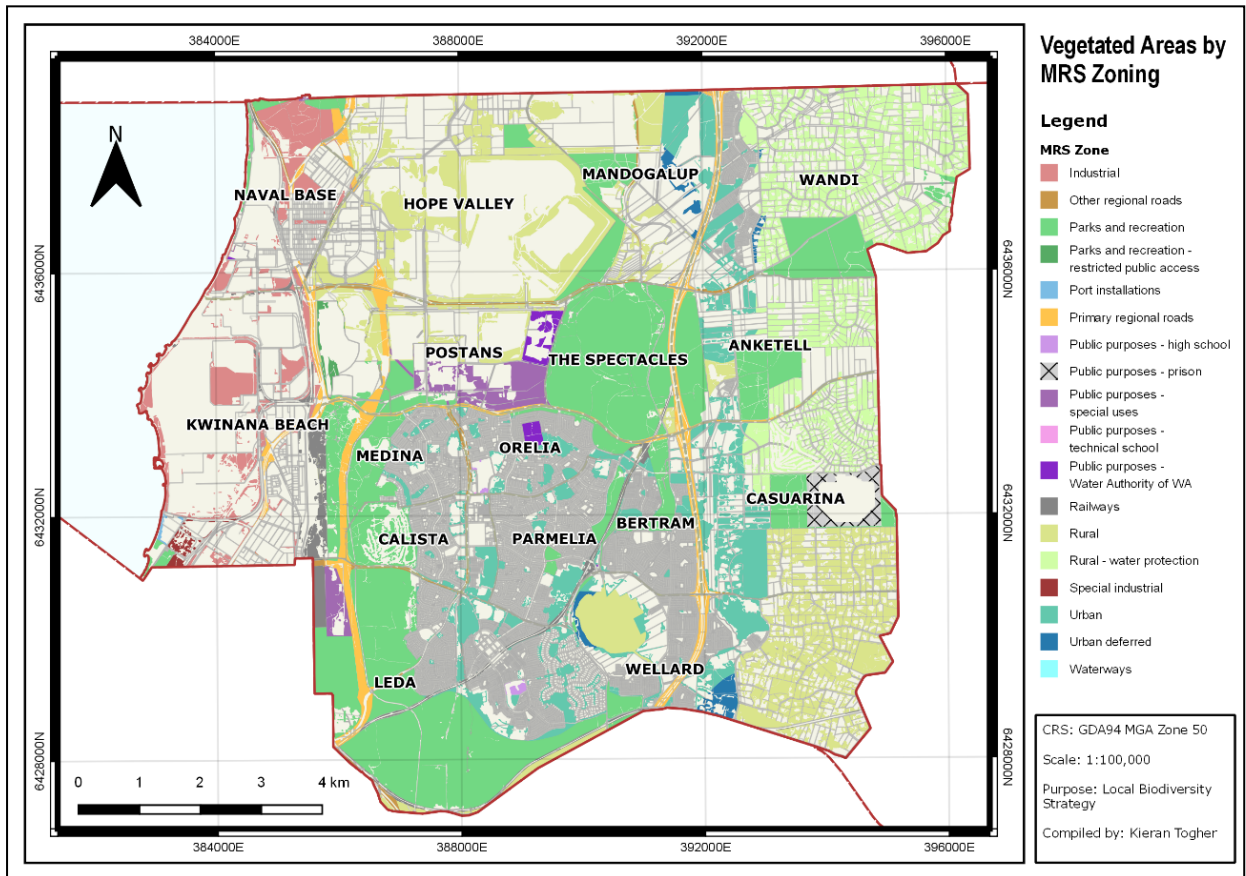


Figure 46: Vegetated Areas within the MRS

Vegetation Complexes	MRS zoned areas																Total
	Parks and Recreation	Parks and Recreation (Restricted)	Port Installation	Primary Regional Roads	Other Regional Roads	Public Purposes High School	Public Purposes Prison	Special Uses (SU)	SU – Water Authority WA	Railways	Rural	Rural - Water Resource	Industrial	Special Industrial	Urban	Urban Deferred	
	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	(ha)	
Bassendean Complex-Central and South	535	-	-	18	4	-	53	-	-	6	365	537	-	-	216	216	1,950
Cottesloe Complex-Central and South	763	8	-	100	15	7	-	37	5	36	172	-	134	-	172	-	1,449
Guildford Complex	-	-	-	-	-	-	-	-	-	-	3	-	-	-	-	-	3
Herdsmen Complex	175	-	-	1	-	-	-	-	-	2	67	-	-	-	19	9	273
Karrakatta Complex-Central and South	218	-	-	1	3	-	-	28	26	3	160	-	-	-	125	-	564
Quindalup Complex	141	-	2	42	2	-	-	28	-	33	-	-	103	2	-	-	353
Serpentine River Complex	3	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	3
Total	1,834	8	2	162	24	7	53	93	31	80	767	537	237	2	532	225	4,585
									2,504								

Table 29: Extent of Local Natural Areas (by vegetation complexes) based on MRS Zones (2013)

### 5.15.2 Bush Forever Sites

*State Planning Policy 2.8 Bushland Policy for the Perth Metropolitan Region, 2010*, through reservation, mandates the protection and management of the State Government's Bush Forever Sites. Similarly, the *Conservation and Land Management Act 1984*, through reservation, provides conservation and protection for regional reserves managed by the Department of Biodiversity Conservation and Attractions (DBCA).

The Bush Forever policy provides a policy and implementation framework that is aimed to ensure bushland protection and management issues in the Perth Metropolitan Region are appropriately addressed and integrated with broader land use planning and decision-making. The policy also recognises the protection and management of significant bushland areas as a fundamental consideration in the planning process, while also seeking to integrate and balance wider environmental, social and economic considerations. Bush Forever Sites, which are representative of regional ecosystems and habitats, play a central role in the conservation of Perth's biodiversity.

The City has 953 hectares of Bush Forever Sites within three major natural areas reserved as Bush Forever Sites within the City. These include, but are not limited to, Jandakot Regional Park, Beeliar Regional Park and Bush Forever Site No. 349 Leda and adjacent bushland. Table 30: Bush Forever Sites and Figure 47: Bush Forever Sites depict Bush Forever Sites within the City of Kwinana.

Regional parks consist of areas of land that have been identified as having regionally significant conservation, landscape and recreation values. These parks may consist of land areas managed by a range of different government agencies and private landowners and provide a unique character and sense of place for the City.

In 1983, the Environmental Protection Authority identified Wandi Nature Reserve, Casuarina Reserve, Modong Nature Reserve and Banksia Nature Reserve as regionally significant in Conservation Reserves for Western Australia (Department of Environment & Conservation, 1983). In 1987, these areas were included in the corridor of regional parks referred to as Jandakot Regional Park. Jandakot Regional Park is an important link in a series of conservation reserves in south-east metropolitan Perth.

Jandakot Regional Park consist of Banksia Woodlands and wetlands and is cooperatively managed by the DBCA, the Cities of Kwinana, Cockburn and Armadale, the Department of Corrective Services and the community. The wetlands within the Park are generally surface expressions of the underlying Jandakot Groundwater Mound. These woodlands and wetland ecosystems provide a range of important habitat for fauna.

Beeliar Regional Park is within the Local Government areas of Melville, Cockburn and Kwinana and are also jointly managed. The Spectacles Wetlands, Bush Forever Site No. 269, is the only section of Beeliar Regional Park within the City, and is managed by the DBCA. Beeliar Regional Park consists primarily of two series of elongated wetlands that run parallel to the coastline and incorporates an area of coastal limestone cliff (Henderson foreshore).

Bush Forever Site No. 349 Leda and adjacent bushlands forms an ecological corridor on the western side of the City and includes the suburbs of Baldivis, Kwinana Beach, Postans, Medina, Calista, Leda, Wellard, and East Rockingham. This vegetation in this corridor consist of Tuart and Banksia Woodlands, wetlands, coastal heath and, importantly, forms a vegetated buffer between the City's urban and industrial areas.

Some Bush Forever Sites remain within private ownership, these remnant bushlands are protected by State Government legislation and the DPLH are systematically acquiring these areas as part of the

conservation estate. The City will support the protection and acquisition of these sites through the planning process.

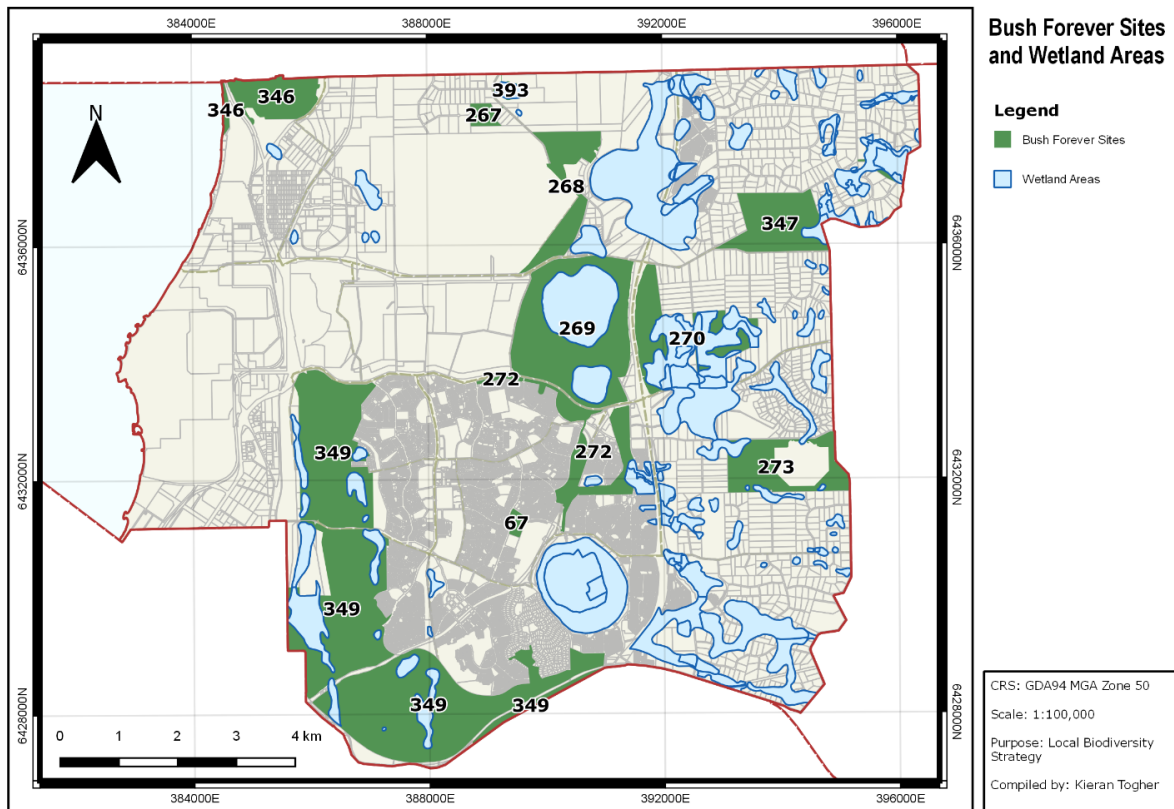


Figure 47: Bush Forever Sites

Bush Forever Sites	Location	Ownership	Management	BF Area (ha)	Vegetated Area (ha)	Water Area (ha)	Cleared (ha)
67	Parmelia Avenue Bushland	WAPC	Crown Reserve vested with the City	8.56	6.96	0.00	1.60
267	Mandogalup Road Bushland, Hope Valley	WAPC	WAPC	16.33	16.05	0.00	0.28
268	Mandogalup Road bushland, Mandogalup	Commercial, Private, State Government	Part no management authority, and part Crown reserve vested with the City	99.97	88.19	0.00	11.78
269	The Spectacles Bushland	Commercial, Private, State Government	Part WAPC, part Department of Water and Environmental Regulation (DWER), part no management agency	422.46	386.92	2.96	32.88
270	Sandy Lake and adjacent Bush land, Anketell	WAPC, Commercial	Part WAPC, part DWER, part no management agency	202.55	188.35	11.34	2.87
272	Sicklemore Road Bushland, Parmelia / Casuarina	Commercial, Private, State Government	Part unallocated crown land, part drain reserve, part Crown reserve vested with the City, part no management agency	107.60	87.01	0.00	20.60
273	Casuarina Prison Bush land	State Government	Crown reserve vested in Department of Corrective Services	113.43	112.35	0.00	1.08
346 (partial)	Brownman Swamp, Mt Brown Lake and adjacent Bushland, Henderson / Naval Base	State Government, Water Corporation, WAPC	Part DBAC conservation Park, part Crown Reserve vested in Local Government, part unallocated Crown land, part no management agency (Beeliar Regional Park)	95,878	xx	xx	xx
347 (partial)	Wandi Nature Reserve and adjacent Bush Land, Wandii / Oakford	WAPC, State Government	Part DBAC Nature Reserve, part WAPC (Jandakot Regional Park)	170.231	xx	xx	xx
349	Leda and adjacent Bushland	Commonwealth, WAPC, State Government	Part DWER nature reserve, part Vacant Crown Land, part WAPC part Crown Reserve vested with the City, part no management agency	1,107.51	917.95	4.86	184.70
393	Wattleup Lake and adjacent Bush Land, Wattleup / Mandogalup	Commercial, Private, State Government	No Management Agency	12,963	xx	xx	xx

Table 30: Bush Forever Sites

### 5.15.3 Local Natural Areas

The term Local Natural Area (LNA) refers to natural areas that exist outside Bush Forever Sites, DBCA Managed Estates (National Parks, State Forest, Nature Reserves, and Conservation Parks), and Regional Parks. The LNAs represent the biodiversity assets of a local government. They are broadly categorised by vegetation complex and, to a large extent, contain Threatened Ecological Communities (TECs), Priority Ecological Communities (PECs), Declared Rare and Priority Flora and Fauna.

Many LNAs in the City have been cleared or modified for farming, industrial and residential development purposes. As of 2013, the City had approximately 2,148 hectares of LNAs remaining. Currently, these LNAs are under threat from various sources, predominantly urbanisation.

The protection of LNAs is important for maintaining a suite of representative vegetation complexes that exist within the City and providing connectivity, critical in maintaining biodiversity, between protected areas. The LNAs are also beneficial for local communities and provide places of passive recreation, local environmental services and a sense of place and amenity values.

#### 5.15.3.1 Implications for LNAs

The City's Biodiversity Strategy (Ironbark Environmental, 2013) identifies LNAs of high ecological significance as a priority for formal protection through future development. Many of these areas are of regional ecological significance when assessed against the policies of the Environmental Protection Authority (e.g. EPA Guidance Statement No.33, EPA, 2000). The Strategy identifies 957 hectares of high ecological significance and recommends the protection of at least 662 hectares. These areas consist of a representative cross-section of vegetation complexes (broad vegetation types) typical of the City's original vegetation.

#### Casuarina & Mandogalup

There are 129 hectares of LNA's within Rural Zones. In the main, these lands are planned or proposed to be rezoned for residential purposes under the Local Planning Scheme. They occur either side of the Kwinana Freeway and include natural areas in the localities of Mandogalup and Casuarina, including Bollard Bulrush Swamp and Lot 123 Mortimer Road, Casuarina.

The City should consider design options for the integration of large protected natural areas in future urban development in Mandogalup and Casuarina. These design options should be made available to State and Federal Government and potential applicants with a view to determining a negotiated outcome.

#### Special Rural and Rural Water Resource Zones

62 hectares of LNAs are within Special Rural and Rural Water Resource zones, these areas are found in the eastern parts of the City and represent the most significant examples of the remaining wetlands and banksia woodlands. They support vegetation in good or better condition that have been less fragmented and disturbed than surrounding natural areas.

#### Hope Valley Wattleup Redevelopment Area

Planning, development and conservation in the Hope Valley Wattleup Redevelopment Area (HVWRA) is determined under the redevelopment project's Masterplan (Western Australian Land Authority, 2008). A Biodiversity Strategy has been finalised to guide how natural areas will be considered and protected as part of future development applications within the redevelopment area (Western Australian Land Authority, 2007).

The areas proposed for protection within the Biodiversity Strategy include approximately 48 hectares of vegetated natural areas. It is important that the City, as part of the administration of the HVVRA Masterplan, ensures that protected natural areas are included within reserves for conservation, and that management plans are prepared and implemented for these reserves.

### [Postans](#)

The Postans locality is bounded by Anketell Road, McLaughlin Rd, Thomas Road and Rockingham Road in the west. Planning for the locality is underway and a Draft Postans Precinct Structure Plan has been prepared (Urbanplan, 2011). There are 126 hectares of natural areas in this precinct proposed for formal protection. Most of the areas recommended for protection are within regional or local reserves, including: Parks and Recreation regional reserves, a local reserve for Parks Recreation and Drainage, Public Purpose reserves and a Primary Regional Road reservation. Part of the eastern extent of areas proposed for protection are zoned Rural. These areas are of regional ecological significance and provide:

- A regional level north-south ecological linkage
- Regionally and nationally significant vegetation
- Habitat for black cockatoos

Given the large extent of publicly owned land in the precinct, the City should proactively work with government agencies to secure protection of the site's conservation values. Given their ecological significance, these natural areas may also have value as potential future biodiversity offset sites for government (Ironbark Environmental, 2013).

#### 5.15.4 Protected Flora & Fauna

Specially protected flora and fauna are at risk of extinction in their natural habitat and include:

- Threatened Ecological Communities
- Priority Ecological Communities
- Threatened Fauna
- Declared Rare Flora
- Priority Flora and Fauna

They are afforded protection status under a variety of State and Federal legislation that includes, the *Environmental Conservation and Biodiversity Conservation Act 1999*, the *Environmental Protection Act 1986* and the *Biodiversity Conservation Act 2016*.

##### 5.15.4.1 Threatened Ecological Communities (TECs)

An ecological community is a naturally occurring group of plants, animals and other organisms that interact in a unique habitat. Its structure, composition and distribution are determined by environmental factors such as soil type, position in the landscape, altitude, climate, and water availability.

In Australia, three categories exist for TECs under the EPBC Act - Critically Endangered, Endangered, and Vulnerable. City's TECs include:

- Assemblages of plants and invertebrate animals of Tumulus (organic mound) Springs of the Swan Coastal Plain (Critically Endangered)
- Banksia Woodlands ecological community of the Swan Coastal Plain (Endangered)
- Sedgeland in Holocene dune swales of the southern Swan Coastal Plain (Endangered)

- Tuart (*Eucalyptus gomphocephala*) woodlands and forests of the Swan Coastal Plain (Critically Endangered)
- Honey myrtle (*Melaleuca huegelii* - *M. systema*) on limestone ecological community within the Cottesloe Central and South vegetation complex

A number of reasons including: clearing of native vegetation, altered fire regimes, non-native or invasive species, climate change, water diversion, and urban development have had significant detrimental impacts on ecological communities within the City. Rapid and significant reduction in the geographic distribution and/or ecological function of Tuart and Banksia Woodlands has contributed to these ecological communities becoming at risk of extinction. That is, the natural composition and function of the ecological community have been significantly depleted across its full range.

Of particular concern, are the Tuart (*Eucalyptus gomphocephala*) woodlands and forests on the Swan Coastal Plain, listed in 2019 as a Critically Endangered Ecological Community under the EPBC Act 1999. The Swan Coastal Plain is the only place in the world where Tuart woodlands and forests are found. Very little of the original area of Tuart woodlands and forests remain, and much of what remains is degraded (DoEE, 2017).

#### 5.15.4.2 Priority Ecological Communities

Priority Ecological Communities (PECs) are ecological communities that are likely to be under threat and are awaiting further information to establish their status of threat. There are four PECs, which are known to occur in the City:

- Coastal shrub lands on shallow sands on the Swan Coastal Plain;
- *Banksia ilicifolia* woodlands, southern Swan Coastal Plain; and
- Low-lying *Banksia attenuata* woodlands or shrublands.

#### 5.15.4.3 Declared Rare Flora & Threatened Fauna

At present, the City requires that flora and fauna surveys be carried out to determine the presence of any declared rare flora (DRF) or Threatened fauna as part of development proposals. This normally occurs during the structure planning process or as a part of a scheme amendment. All areas and associated buffers that contain DRF or Threatened fauna are generally retained but not necessarily protected. Table 31: Declared Rare Flora and Threatened Fauna lists DRF and Threatened Fauna known to occur in the City.

Declared Rare Flora	Specially Protected Fauna
<i>Diuris micrantha</i> (Swamp Donkey Orchid) – Bertram	<i>Calyptorhynchus latirostris</i> (Carnaby’s Black Cockatoo) – Banksia Woodlands
<i>Caladenia huegelii</i> (King Spider Orchid) – Wandi, Bertram	
<i>Drakaea elastica</i> (Glossy-leaved Hammer Orchid) - Anketell	<i>Calyptorhynchus Banksii-naso</i> – Forest Red-tailed Black Cockatoo Tuart & Banksia Woodlands

Table 31: Declared Rare Flora and Threatened Fauna

#### 5.15.4.4 Priority Flora and Fauna

Priority Flora and Fauna are native species that are either under consideration as threatened species and require further surveys to adequately determine their status, or are adequately known and



require monitoring to ensure there is no further decline in populations. Table 32: Priority Flora and Fauna lists species of Priority Flora and Fauna are known to be present in the City.

Priority Flora	Location	Priority
Aponogeton hexatepalus (Stalked Water Ribbons)	Bertram	4
Dodonaea hackettiana (Hackett's Hopbus)	The Spectacles	4
Priority Fauna	Location	Priority
Isodon obesulus fusciventer (Quenda)	Hope Valley / Oakford / Bertram / Wellard East / Wellard / Leda	4
Macropus irma (Western Brush Wallaby)	Wellard / Leda / Casuarina	4

Table 32: Priority Flora and Fauna

#### 5.15.4.5 Conservation of Protected Species

To ensure that protected species persist in their natural habitat within the City it is critical that their habitat is conserved by reservation and connectivity between these habitats is maintained. Fundamental environmental planning should prioritise habitat protection for specially protected species, with an emphasis of the creation of ecological linkages between remnant patches of natural areas. Connectivity is essential in maintaining genetic biodiversity and mitigating localised extinction. Through the planning process, the City should ensure that these habitats are identified, protected and connected to other natural areas. This can best be achieved through consideration at the MRS amendment or structure plan stages. Additionally, the City has a role to play in the protection of habitat and ecological linkages in private ownership, the provision of support and incentives through stewardship programs will assist in the conservation of these areas.

#### 5.15.4.6 Ecological Linkages

Ecological Linkages connect locally and regionally significant natural areas and provide stepping stones for flora and fauna. They allow for movement of fauna across landscapes and maintain long-term movement of genes between the former ranges of species.

Fauna species need to move across the landscape to meet feeding and breeding needs and deal with threats such as fires and feral animals. Many of these species are important in moving pollen and seeds between natural areas that might otherwise be isolated within developments.

To be effective, Regional Ecological Linkages (REL) should incorporate the major variation in plant communities and fauna habitat typical of the region so that the widest range of flora and fauna possible can use the links (WALGA & PBP 2004). Similarly, Local Ecological Linkages (LEL) play an important role in improving the viability of localised natural areas that are too small or too degraded to survive on their own.

The 'Towards Establishing a Green Network' Study (SWG, 2014) identified RELs and LELs within the City, these are show at Figure 48: Regional and local ecological linkages over native vegetation extent prioritisation criteria in the City (adapted from Towards Establishing a Green Network, LBP, 2014 and described in Table 33: New Local Ecological Linkages identified (source: 'Towards establishing a Green Network' Study (2014). Ecological linkages can be protected and restored through recognition of existing linkages in the land use planning process, formal protection of

natural areas on these linkages, and management and restoration of natural areas wherever possible.

The Local Government Biodiversity Planning Guidelines suggest a maximum distance of between 500m and 1000m (but state a preference of 500m) to maintain a linkages function between natural areas (WALGA & PBP, 2004). Landscape elements that contribute to ecological corridors in urban environments include parks, golf courses, street trees, vegetated road reserves and gardens. Importantly, ecological linkages provide:

- Conduits between ecosystems so that flora and fauna may pass through the linkage from one habitat patch to another, ensuring continued survival;
- re-colonisation of habitats by fauna and flora that have become locally extinct from events such as fire, disease or poor breeding success;
- seasonal movement of fauna between temporarily available resources (food, water); and
- Genetic sustainability of flora and fauna between natural areas.

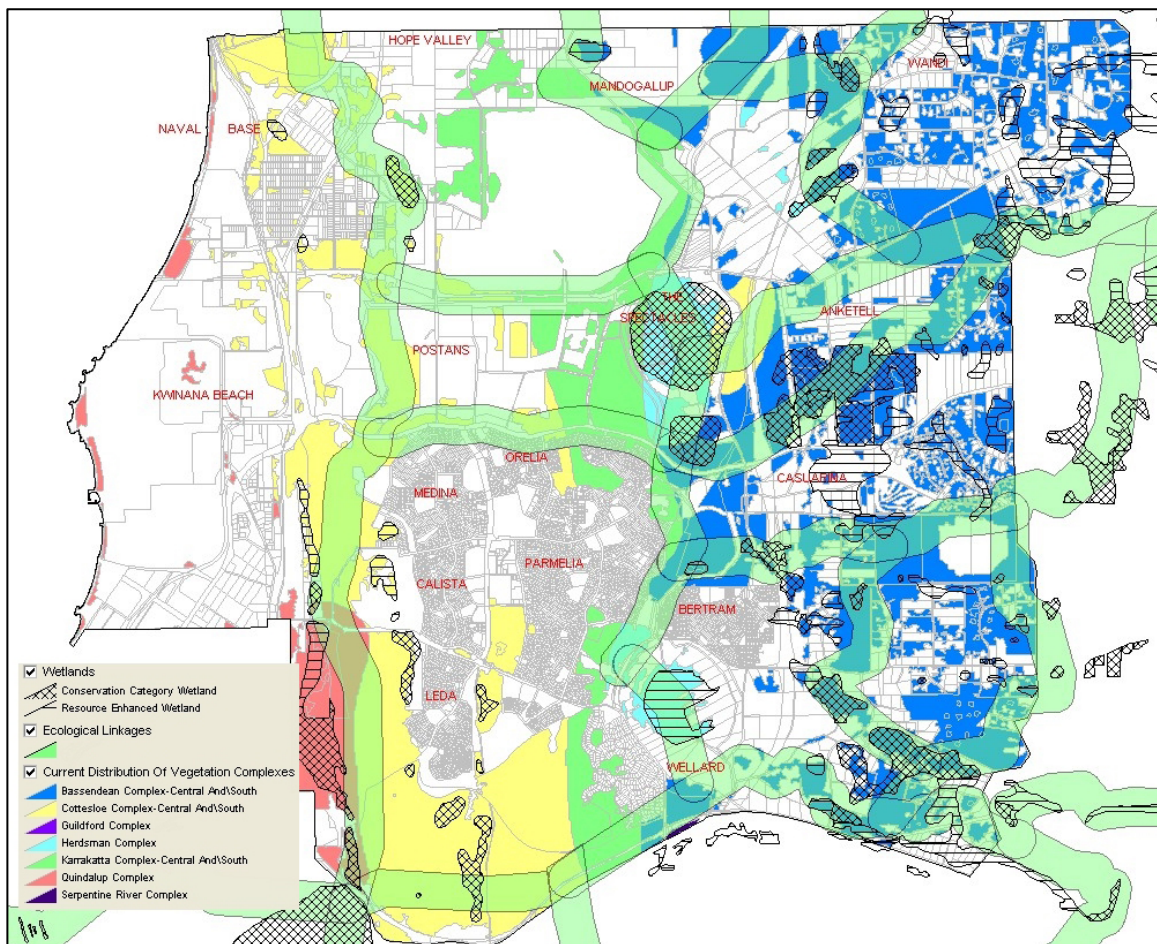


Figure 48: Regional and local ecological linkages over native vegetation extent prioritisation criteria in the City (adapted from Towards Establishing a Green Network, LBP, 2014)

Conservation Values	Recommended Actions
Local linkage between K7 through K37, K34 to C32	Due to the alignment of Kwinana Freeway, East-West movement of terrestrial fauna is limited in this part of the landscape, however, there are several patches retained within new subdivisions within 500m from each other and the linkages follows a drain reserved in the LPS as Parks, Recreation and Drainage. Manage the Parks and recreation reserve and all future POS areas to maximise fauna habitat provision.
Local linkage between K23 (BF270) and K25 (BF 273)	Two opportunities can be explored, one utilising vegetation retained within the Special Rural properties (as mapped in the EPT). Another opportunity exists within Rural Water Resource lands east of Bombay Boulevard and adjoining a golf course. Improvement of vegetation condition within the golf course and the Rural Water Resource properties will improve connectivity between the two regionally significant bushland areas.
East-west linkage between two major north-south regional linkages	There are no opportunities to establish a good linkage between the coastal foreshore reserves and bushland reserves in the eastern parts of the City. K38 and K12 (part of Latitude 32 Structure Plan) are closest to the coast but separated by the industrial development. There are three parallel linkages: one along Thomas Road consisting of BF 272, local recreation reserves vegetation along Anketell Road connecting wetlands to be retained as part of Latitude 32 structure plan to BF 269 East-west linkage formed by vegetation within K9, K8 and K7.

Table 33: New Local Ecological Linkages identified (source: 'Towards establishing a Green Network' Study (2014))

#### 5.15.4.7 City Managed Reserves

The City's Natural Areas Management Plan 2014 - 2024 (Management Plan), through appropriate management practices, preserves and improves biodiversity values existing in City managed reserves. The City actively manages 21 natural area reserves comprising coastal, woodland and wetland communities. In total an area of 240 hectares are under the City's management, ranging from smaller reserves in urban areas to larger reserves in rural and semi-rural areas.

The Management Plan prioritises threats impacting bushland, wetland and other reserves, and investigates and implements suitable methods to control these threats. The management programs include:

- weed control;
- illegal access and rubbish dumping;
- retention, regeneration and revegetation;
- fire management and fuel reduction;
- pest and disease control; and
- community education and volunteer programs.

Through the subdivision process new reserves continue to be added to the conservation estate within the City. It is important that the LPS acknowledges the extent of new natural areas coming under the City's management and that these reserves meet the certain standards and criteria prior to becoming the responsibility of the City. Recurrent municipal resources and budgets must also be considered and implemented to ensure appropriate maintenance regimes of the City's natural area reserves. Table 34: City Managed Reserves shows City managed reserves.

Reserve Name	Area (Ha)	MRS	TPS	Vesting	Bush Forever Site
Clementi Road Reserve	33.6	Rural	Rural A	Parks and Recreation	Section of BF268
Challenger Beach	3.3	Parks and Recreation	MRS	Recreation and Dune Protection	Section of BF268
Postans	1.1	HVWP Redevelopment Area	MRS	Public Recreation	-
Hendy Road	4.9	HVWP Redevelopment Area	MRS	Public Recreation	-
Depot Swamp	6.3	Parks and Recreation	MRS	Council Depot	Section of BF349
Gentle Road /Golf Course	101.2	Parks and Recreation	MRS	Recreation	Section of BF349
Sloans Reserve	12	Parks and Recreation	MRS	Parks and Recreation	Section of BF349
Wildflower Reserve	15.5	Urban 'C' Class Reserve	Parks & Recreation	Parks and Recreation	BF272
Homestead Ridge (x 3)	7.9	Urban	Park, Recreation and Drainage	Parkland	
Millar Road South	19.3	Rural		Public Recreation	Section of BF349
Banksia Road	26.7	Rural 'C' Class Reserve	Rural A Landscape Protection Zone NE corner	Public Recreation and Drainage Rifle Range	
Lake Magenup	27.1	Parks and Recreation and Water Catchment	MRS	Public Recreation	
Chalk Hill	0.7	Parks and Recreation	MRS	Chalk Hill Tourist Lookout	Section of BF349
Bertram Sanctuary	6.8	Conservation and Parks and Recreation	Residential	Conservation and Public Recreation	
Belgravia Dampland	8.4	Parks and Recreation	Residential	Public Recreation	
Henley Reserve	25	Parks and Recreation	Residential	Public Recreation	
Parmelia Reserves (x2)	8.2	Parks and Recreation	MRS	Public Recreation	BF67
Kwinana Beach (Wells Park)	0.9	Parks and Recreation	MRS	Crown Land	
Squires Ave	0.8	Parks and Recreation	Residential	Parks, Recreation and Drainage	-
Thomas Oval	21.7	Parks and Recreation	MRS	Parks and Recreation	Section of BF349

Table 34: City Managed Reserves

#### 5.15.5 Key issues

To be effective, Regional Ecological Linkages (REL) should incorporate the major variation in plant communities and fauna habitat typical of the region so that the widest range of flora and fauna possible can use the links (WALGA & PBP 2004). Similarly, Local Ecological Linkages (LEL) play an important role in improving the viability of localised natural areas that are too small or too degraded to survive on their own. Identifying these linkages through the planning framework will enable opportunities for negotiation.

Some Bush Forever Sites remain within private ownership, these remnant bushlands are protected by State Government legislation and the DPLH are systematically acquiring these areas as part of the conservation estate. The City will support the protection and acquisition of these sites through the planning process.

Clearing of vegetation is one of the greatest threats to biodiversity in the City. Vegetation clearing isolates, displaces and removes species. It also destroys habitat and food resources for a wide range of species not only those that would live permanently in the vegetation but also those that rely on it for food and shelter seasonally or during crisis times. Clearing also interferes with the ability of populations to disperse and recolonise areas after disturbance or mortality. It has been established that for the effective conservation of biodiversity, a standard level of native vegetation retention of at least 30 per cent of the pre-clearing (pre-European) extent of the ecological communities on the Swan Coastal Plain has to be promoted. The EPA in 2000 recognised a 30 per cent threshold level for native vegetation retention target, below which species loss appears to accelerate exponentially at an ecosystem level.

The impact of human intervention and development is such that native vegetation will become increasingly scarce, to a point where ecosystem and biodiversity functions fail, unless protection measures are implemented that regulate urban development through planning and regulatory frameworks. The cumulative impacts of human activity and development to date in Kwinana need to be recognised, with effective policies needed to protect LNA'S where possible. It is evident that the most significant areas of LNA's are within the City's eastern areas which are primarily occupied by Rural and Special Rural land uses.

LNA's are not necessarily considered at the wider metropolitan level however and much of its protection needs to take place by the City within the planning framework. In this context, extensive clearing of native vegetation in the City as a result of subdivision and development approval has been one of the biggest threats to LNA's. At present, there are areas within the City zoned for urban use, which have not been fully developed. In some areas, it will be necessary to develop structure plans for future urban and industrial development.

Currently there are no set criteria for choosing conservation areas during the structure planning phase. It is important that high quality bushland remnants are not lost by addressing biodiversity conservation early in structure planning. The development of appropriate planning mechanism will provide a strong position for the City when assessing new developments and structure planned areas.

#### 5.15.6 Response to issues

The City should adopt objectives for the protection of remnant natural areas and biodiversity conservation. These objectives are designed to achieve the highest level of environmental protection that is possible under current legislation and government policy settings. The objectives are:

- Formally protect a representative cross-section of vegetation complexes (broad vegetation types) typical of the city's original vegetation through appropriate zoning;
- Formally retain Bassendean Central and South vegetation complex in the eastern parts of the city, largely within rural residential areas through vegetation retention policies and strategies;
- Protect natural areas with special biodiversity features such as threatened flora and fauna, threatened and priority ecological communities and wetlands;
- Maintain and enhance natural area connectivity and ecological linkages within the city as part of any development of sites; and
- Maintain and actively manage biodiversity within the City's local reserves.

The City should also seek to review and update its draft Local Biodiversity Strategy in order to better monitor local natural areas within the City and provide further guidance around environmental priorities and appropriate mechanisms to retain and enhance the City's environmental assets.

## 5.16 Climate Change

When considering mitigation and adaptation actions, it is important to first consider the risks posed by climate change in the region. This following information was collated in 2009 by GHD and the Southern Metropolitan Regional Council for the southern metropolitan councils (including the City of Kwinana) as part of the Local Adaptation Pathways (LAP) program (GHD & SMRC 2009). The implications of climate change are increasing in impact and frequency in Australia and throughout the world. These impacts represent the greatest and most urgent of direct risks to the City.

The LAP identifies the following matters as major climate change impacts for the City:

- Water availability – groundwater availability for parks;
- Heatwaves/Urban Heat Island – new and urban areas;
- Sea level rise – Wells Park; and
- Extreme weather events – drainage planning, emergency response.

More frequent and severe bushfires – fuel reduction in reserves and emergency response, the major risks and their relevance to the City's operations are described below.

### 5.16.1 Temperature change

The average annual global temperature has already risen by 0.8 degree Celsius over the past century and it is predicted to rise by 0.6 to 1 degree Celsius by 2030 (Climate Commission, 2011; GHD & SMRC, 2009).

This increase in temperature has, and will, result in an increase in the number and severity of heat waves. This, in turn, has increased heat stress related deaths through heart attack, stroke and heat exhaustion. Heat wave deaths are less prominent than some of the other effects of climate change but are currently the deadliest of the natural disasters in Australia (Climate Council, 2014).

The retention of vegetation within new subdivisions considerably reduces the heat island effect and can reduce localised temperature increase.

The other major impact of an increase in temperature is an increase in high fire risk days and potential severe bush fires.

### 5.16.2 Reduced Rainfall

The southwest corner of the State has become markedly drier, with a 15% reduction in rainfall since the mid-1970s. There is strong evidence in southwest Western Australia that climate change is making a significant contribution to the drying trend. (Climate Commission, 2011; GHD & SMRC, 2009).

Combined with increased evaporation due to increased temperatures, the drying climate has reduced the availability of water for use in the City's buildings, parks and gardens. The City will need to continue to become more efficient in its use of water if parks and gardens are to be maintained to the current standards. The City has developed a Sustainable Water Management Plan (City of Kwinana, 2013) to address these challenges.

The combination of increasing temperatures and reduced rainfall is also likely to have a significant impact on the City's biodiversity. Drought deaths of trees and drying wetlands have already been observed in many reserves and parks. Changing climatic conditions may mean that the ecological

communities that currently exist, which are already under pressure from land clearing, will be unable to adapt and survive.

### 5.16.3 Sea Level Rise

Sea levels along the west coast of Australia have been rising between 7.1 and 7.4mm per year since the early 90's, approximately double the global average. Global sea levels are predicted to rise a further 0.5 to 1 meter this century. A sea level rise of 50cm will lead to significant increases in the frequency of coastal flooding, flooding that is currently considered to be a 1 in 100-year event would occur every year (Climate Commission, 2011).

The City manages a relatively small area of coastline but the financial impact of rising sea levels to the City will be significant. Sea level rise is already threatening millions of dollars of infrastructure at Kwinana Beach and Challenger Beach including sea walls, boat ramps, offshore breakwaters, roads, toilet blocks and car parking.

The City participated in the Cockburn Sound Coastal Vulnerability Study to assess sea level rise impacts and determine the best course of action. The final stage of this project was the completion of the Kwinana Coastal Adaptation Plan in 2015 (CAP) (GHD, 2015). This comprehensive report includes sea level rise predictions and recommended adaptation actions for the entire Kwinana coastline. The coastline was divided into management units, two of which are managed by the City, Wells Park and Challenger Beach. Both sites have been identified as vulnerable to erosion and loss of beach area from coastal actions and sea level rise (GHD, 2015).

State Planning Policy 2.6, 2013 (SPP 2.6) sets out adaptation options associated with coastal hazards resulting from the impacts of climate change. Public and private assets along the West Australian coastline are increasingly at risk from rising sea levels. The option of 'planned or managed retreat' may be the most efficient, effective (including cost) and equitable response to this risk (DPLH, 2013). A strategy of planned or managed retreat should consider social, environmental and economic impacts consistent with SPP 2.6 objectives. This approach adheres to the principles of sustainable development and adaptive risk management in accordance the Planning and Development Act 2005 and SPP 2.6.

The City, having given regard to SPP 2.6 and the CAP has adopted the response of 'planned or managed retreat' for land and infrastructure owned or vested with the City and identified as vulnerable to sea level rise hazards.

This approach will underpin the City's forthcoming policy on sea level rise. It is however recommended that the City remains flexible in its approach given the long-term forecasts of Coastal Hazard and Risk Management Adaption Planning (CHRMAP), which is used to inform the extent of future sea level rise. Incorporating flexibility into its strategic planning allows future communities the ability to make decisions based on current values and risk. Strategic planning should avoid putting new development at risk whilst facilitating changed settlement patterns overtime. The above planning approach allows existing development to remain in-situ until risk becomes unacceptable. Removal of 'at risk' development occurs once the triggers are reached, which are articulated through the City's CHRMAP.

The recommended action at Wells Park is a staged retreat of facilities. Challenger Beach is part of the coastal unit that includes some of the industrial area. Interim protection measures such as a sea wall were recommended to protect the industrial facilities along the coast rather than the beach itself. Any protection measures would therefore have to be undertaken together with industry (GHD, 2015).



The CAP also recommends the incorporation of a special control area and development controls such as time and/or event limitations of new development in the City's Local Planning Scheme to avoid development that will be adversely affected by sea level rise (GHD, 2015). In addition, the City is considering a sea level rise policy along with accompanying advocacy and education.

#### 5.16.4 Extreme Weather Events

In addition to the above risks, extreme weather events have increased ever since the adoption of the original plan (Climate Council, 2017). In particular, two very large summer rainfall events which occurred in 2017 and 2018 being the first and fourth wettest Perth rainfall days on record respectively (WA Today, 2018).

While identified in the 2009 Climate Change Risk Assessment Report (GHD, 2009), extreme weather events were assessed as being one of the lower priority risks possibly due to the fact that local governments only play a part role in emergency management, and other impacts like reduced water availability are currently more disruptive and pressing. However, this impact should continue to be monitored and future revisions of the plans consider specific adaptation measures to extreme weather events.

#### 5.16.5 Bushfire

The *State Planning Policy 3.7 (SPP 3.7) Planning in Bushfire Prone Areas 2015* mandates Bushfire management in the local government context. The DPLH has released a factsheet to assist in the preparation of a Local Planning Strategy that is in accordance with SPP 3.7 and the Guidelines for Planning in Bushfire Prone Areas (Guidelines).

Local planning strategies should assess bushfire risk alongside other relevant planning matters including environmental, economic and social considerations to holistically inform and shape future expansion, as a precursor to Local Planning Scheme zoning and reservations. The bushfire regulatory planning requirements only apply to development applications in areas which are covered by a local planning scheme.

In accordance with policy measure 6.3 of SPP 3.7, any strategic planning proposal that requires a bushfire assessment should be accompanied by:

- a Bushfire Hazard Level (BHL), which should be prepared by an accredited Bushfire Planning Practitioner;
- identification of any bushfire hazard issues arising from the relevant assessment; and
- demonstration that compliance with the bushfire protection criteria can be achieved in subsequent planning stages.

The above information should be prepared in accordance with the Guidelines. The Guidelines provide direction as to how compliance with the bushfire protection criteria can be achieved in subsequent planning stages when preparing a Local Planning Strategy. It should specifically address the following issues:

- the location of bushfire prone areas and the need for further assessment of the hazard in such areas;
- the avoidance of land use and development intensification in any areas likely to maintain or generate a hazard level of extreme;
- firefighting infrastructure;

- the existing and proposed road network, and its likely effectiveness in a bushfire emergency;
- biodiversity issues, their interrelationships with bushfire prone areas and means of protection for areas with high conservation values; and
- the location of any vulnerable or high-risk land uses within identified bushfire prone areas and whether such uses may require management strategies to be prepared.

Bushfire mapping, managed by the Office of Bushfire Risk Management, that applies under SPP 3.7 has been discussed under 3.3.15 SPP 3.7 Planning in Bushfire Prone Areas.

#### 5.16.6 Key issues

Climate change is recognised as a major threat to biodiversity and ecosystem functions. It impacts upon all levels of biodiversity, ranging from biological, ecosystem and ecological impacts through to population level impacts.

Climate change also has the potential to impact on the urban environment, particularly through the heat-island effect. In such instances, temperature increases in urban areas due to the extent of non-permeable surfaces such as roads, buildings and similar hardscapes. Removal of vegetation can have an added adverse effect on urban environments affecting the local community, wildlife and gardens.

The impact of climate change on biodiversity can be direct or indirect through interactions with other species that are affected by climate change. This leads to changes in competition for food, habitat, and predation patterns and processes.

For some species, the indirect impacts may be stronger than direct impacts. This could also lead to interaction with other human pressures on biodiversity such as habitat degradation and loss, water extraction, pollution, and introduction and spread of pest species. Not only do climate change impacts add to these other pressures, they also interact, altering the way species and ecosystems would otherwise respond and adapt.

#### 5.16.7 Response to issues

Since the original Climate Change Mitigation and Adaptation Plan was adopted in 2015, a number of initiatives have been implemented to address these impacts. These are listed below:

- Developed and adopt the Local Planning Policy No.1 Landscape Feature and Tree Retention in September 2016
- Continue to participate in the Cockburn Sound Coastal Alliance
- Continued implementing the Sustainable Water Management Plan
- Increase in the green canopy by installation of trees
- Planned Urban Forest Strategy
- Implementation of a policy of replacing trees that have died in public areas
- Delivery a local native 'Seedling Subsidy Scheme' along with an 'Adopt a Verge' program to encourage urban greening
- City of Kwinana Bushfire Management Plan 2020

# Infrastructure



## 5.17 Transport

### 5.17.1 Journey to Work

Understanding the patterns of travel into, out of, and within the City is vital to understanding of how the system functions so an efficient and well-integrated land use and transport network can be designed.

As shown in Table 35: Employment Location of the City's Residents, more than 76 per cent of the employed residents of the City travel outside of the area to work.

<b>Employment location of Kwinana resident workers by industry</b>		
All industries	2016	
	Number	%
Live and work in the area	3,092	18.5
Live in the area, but work outside	12,799	76.5
No fixed place of work	850	5.1
<b>Total employed residents in the area</b>	<b>16,727</b>	<b>100.0</b>

Table 35: Employment Location of the City's Residents

Top five local government areas the residents worked in were Cockburn (14.4%) Rockingham (10.5%), Perth (10.0%), Melville (6.0%), Canning (5.6%).

Table 36: Method of travel to work – Resident Workers shows the method of travel to work adopted by the resident workers both within and out of Kwinana. Overall, the resident workers in the City have a higher proportion of people using the train to get to work than Western Australia as a whole.

<b>City of Kwinana - Resident workers method of travel</b>	2016		
	Number	%	Western Australia %
Car, as driver	10,627	63.2	63.3
Car, as passenger	802	4.8	4.9
Train	1,865	11.1	4.9
Bus	413	2.5	4.3
Ferry	4	0.0	0.0
Tram	5	0.0	0.0
Truck	119	0.7	0.7
Motorbike/Motor scooter	81	0.5	0.5
Bicycle	38	0.2	1.0
Taxi/Other	22	0.1	0.2
Other - multiple methods	302	1.8	2.0
Walked only	210	1.2	2.8
Worked at home	408	2.4	4.3
Did not go to work	1,711	10.2	10.1
Not stated	209	1.2	1.1
<b>Total</b>	<b>16,816</b>	<b>100.0</b>	<b>100.0</b>

Table 36: Method of travel to work – Resident Workers

The largest changes in the method of travel of work of the resident workers in the City between 2011 and 2016 were:

- Car, as driver (+2,695 people)

- Train (+767 people)
- Did not go to work (+229 people)
- Worked at home (+168 people)

Thus ‘journey to work’ impacts upon planning and advocacy for roads and public transport provision, as well as economic development strategies to develop local employment which fits the skills and qualifications of the resident workers.

### 5.17.2 Freight Network

The freight network within the City is characterised by both road and rail infrastructure, servicing the Kwinana Industrial Area, Naval Base Industrial area, Latitude 32 and destinations to the south (e.g. Rockingham) and the north (e.g. Fremantle).

The Kwinana Freeway and Rockingham Road are the two primary north-south routes in the City, supported by east-west connections along Anketell Road and Thomas Road. Main Roads’ Restricted Access Vehicle (RAV) Network classifications define the size and scale of freight transport along these corridors and are shown in Figure 49: RAV Network Map.

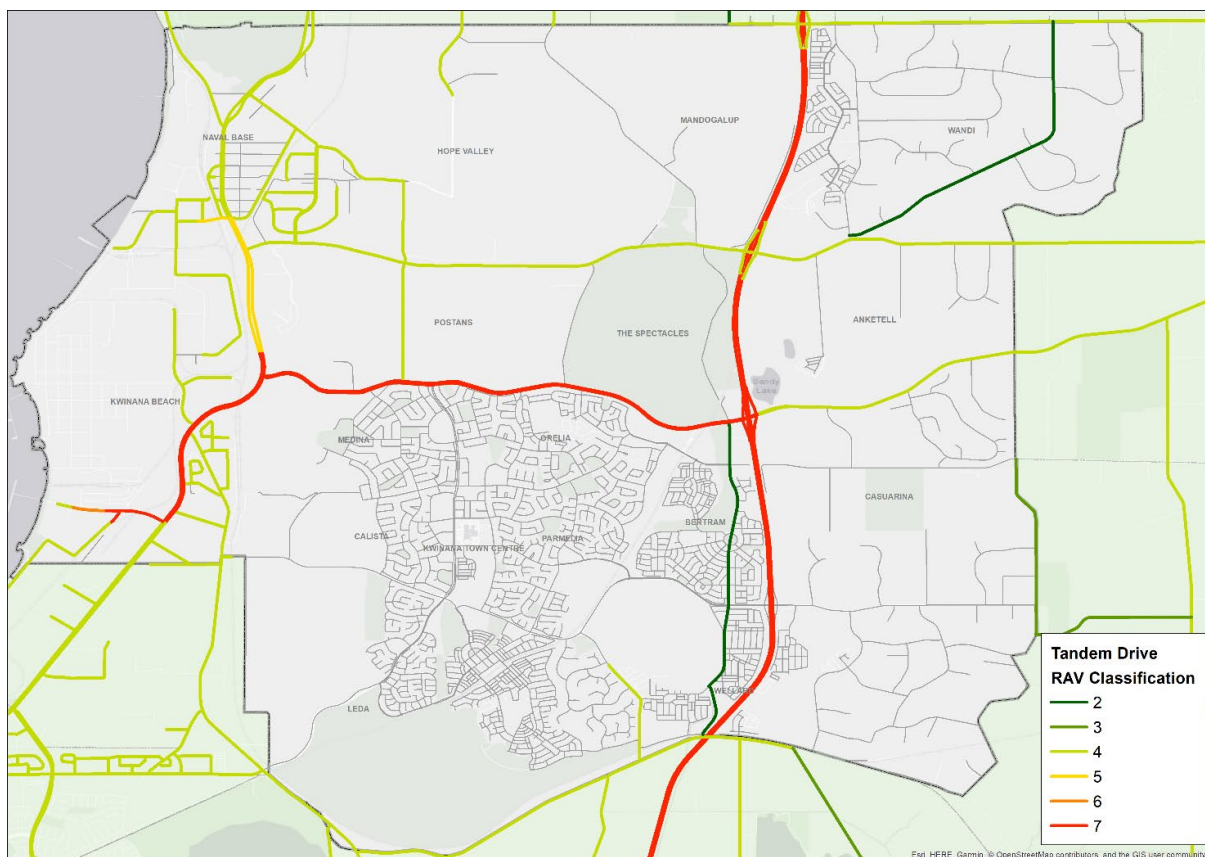


Figure 49: RAV Network Map

Rockingham Road, Patterson Road and Thomas Road are all critical for multiple transport modes. Conflicts in road priority consequently occur, representing inefficiencies in operation and high-risk interactions during peak demand periods. Thomas Road particularly provides the main access to residential development in the Kwinana area. The needs of local access are at odds with the desire for free-flowing through traffic. Importantly, noise pollution from heavy vehicles will affect residents in the northern portions of Orelia and Medina.

The State’s strategic planning for the region is defined by the South Metropolitan Peel Sub-Regional Planning Framework and shown below in Figure 50: Future transport network

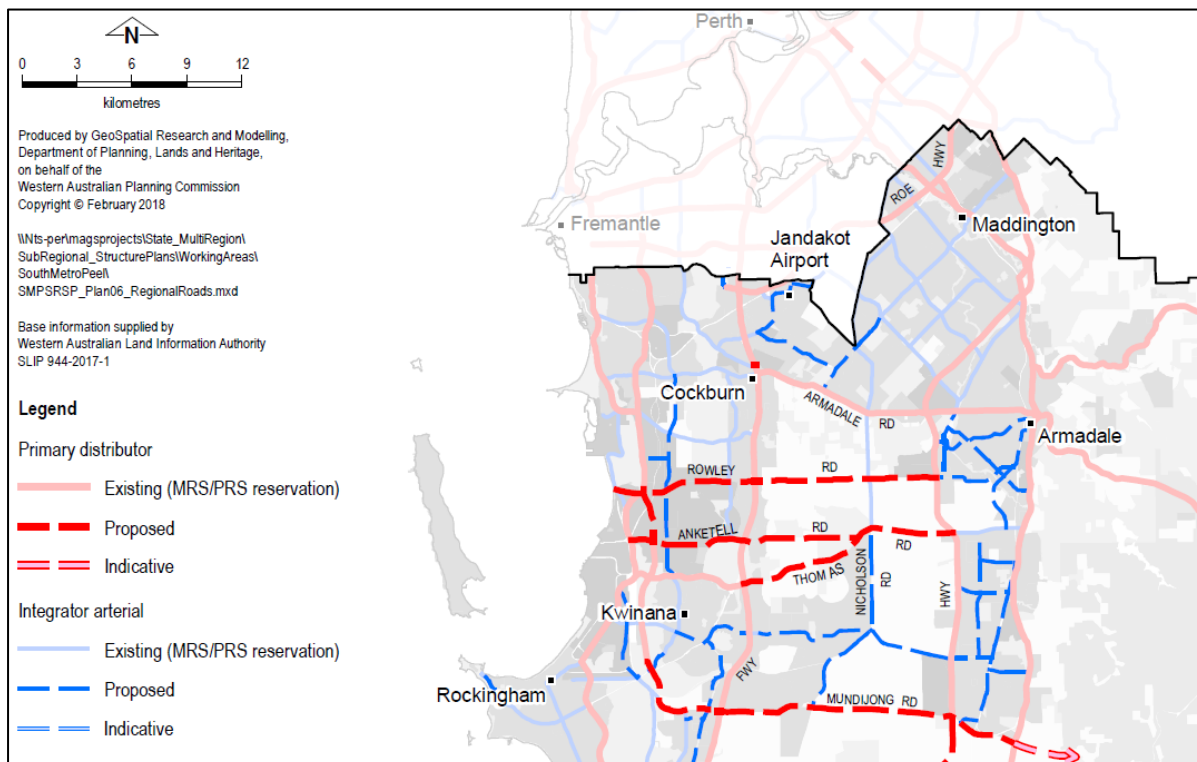


Figure 50: Future transport network

The following key changes to the network are likely to impact the form and function of private vehicle transport in the City.

Regional East-West Roads - Rowley Road, Anketell Road, Thomas Road and Mundijong Road - are a part of Westport’s considerations and the status of a number of east-west road links will be reconstructed as key freight corridors. The latest announcements from Westport’s taskforce is that Anketell Road will become the primary freight route for the future outer harbour with Rowley Road and Thomas Road also playing key roles.

The reconfiguration of Anketell Road as a 4-lane freight corridor will support efficient access to Latitude32 and the Kwinana Industrial Area. These freight network decisions will result in a greater traffic capacity along certain alignments, and will support higher private vehicle mode shares than the current network can sustain.

Fremantle-Rockingham Controlled Access Highway (FRCAH) - Mundijong - Rockingham Road proposes a free-flow freight corridor intended to provide efficient, high-capacity, free-flow connection from the Kwinana Freeway through the Kwinana Industrial Area and connecting to the Port (both existing and future). In doing so, it will alleviate pressure along the existing Patterson Road/Rockingham Road alignment.

One consequence of the FRCAH design is the modification of the Mundijong Road – Kwinana Freeway connection to a Freeway-Freeway Interchange (similar to the Tonkin/Roe Highway Interchange). The size and scale of this infrastructure and requirements for ramps are expected to require modification of the Baldvis Road/Mundijong Road intersection which may impact upon environmental and cultural assets in the area.

The Sub-regional Framework identifies Gilmore Avenue Extension - Thomas Road to Russell Road as a proposed Integrator Arterial as shown in Figure 50: Future transport network, connecting through to Henderson Road in Munster. This alignment is not currently included in the MRS, however if constructed could have significant impacts on the nature and volume of traffic along Gilmore Avenue.

By directly connecting the residential neighbourhoods in Kwinana with the industrial development zones through Postans, Hope Valley, Wattleup and Munster, these residential locations would become more attractive.

However, the route from industrial areas to the north through Kwinana to Dixon Road creates an attractive alternative alignment for heavy vehicle and industrial traffic, which could be detrimental to the development of the Kwinana City Centre itself, by increasing severance and reducing local amenity in the vicinity of Gilmore Road.

### 5.17.3 Private Transport

Private vehicles have traditionally played a significant role in the transportation network for the broader Perth Metropolitan region, as well as specifically for the City of Kwinana.

Private vehicles comprise the majority of traffic on the road network in the City of Kwinana. However, road capacity is finite, and continuing growth in development has the potential to stress the road network under business-as-usual conditions.

A number of intersections across the City of Kwinana experience substantial congestion during peak periods. These include along the following strategic roads:

- Kwinana Freeway: at the grade separated intersections at Rowley Road, Anketell Road, Thomas Road and Mortimer Road;
- Rockingham Road: at Anketell Road, Thomas Road and Mandurah Road; and
- Thomas Road: at the minor road approaches of Abercrombie Road, Medina Avenue, Gilmore Avenue, Orelia Avenue and Johnson Avenue.
- Gilmore Avenue: at Sulphur Road/Summertown Road and Harlow Road/Chisham Avenue.
- Lambeth Circle: near Wellard Train Station Park 'n' Ride
- Bertram Road: near King's College

Along Kwinana Freeway, the high proportion of freight vehicles reduces the efficiency of the corridor due to conflicts between heavy and light vehicle characteristics and the requirements for signal control suitable for large RAV combinations.

Rockingham Road provides mobility for up to 40,000vpd, which meets or exceeds its theoretical capacity given the 4-lane form. As a consequence, Rockingham Road experiences a high degree of congestion during peak periods, and this extends throughout much of the day.

Thomas Road functions as the primary corridor for connection from the established residential suburbs of Kwinana through to the Freeway, as well as a primary access route for industrial traffic to and from the Kwinana Industrial Area. The lack of signalised intersections along Thomas Road beyond Gilmore Avenue creates free-flowing traffic conditions along its length, but does not well-support turning movements from minor roads.

Duplication of Thomas Road (east of Kwinana Freeway) is currently being planned for by Main Roads WA to facilitate the safe movement of vehicles through the area, which is becoming increasingly important as development on adjoining land is taking place.

Gilmore Avenue is a 4-lane road accommodating 18,000vpd. This road forms the central access spine for the Kwinana City Centre, as well as a key strategic route from Thomas Road towards Rockingham. The configuration of the local arterial road network puts a great deal of pressure on these two signalised intersections. A large number of residential dwellings utilise Gilmore Avenue to provide access north to Thomas Road and south to Dixon Road.

The network connections to Gilmore Avenue are compromised by turning restrictions at Challenger Avenue, Barrick Road and Christmas Avenue/Brownell Crescent, which funnels traffic into these limited signalised intersections. Lane allocation at these intersections allow only split phase function for the minor roads, with consequences for the proportion of green time available for turning movements.

There is a very high demand for commuter parking adjacent to the Wellard Train Station, with all access via Charing Cross. The local street network is oriented around Lambeth Circle, which provides the only local crossing point over the train line to connect with the regional road network. As such, Lambeth Circle is simultaneously the main corridor for peak travel out of the suburb for residents, and the only access to Park 'n' Ride. This dual role creates conditions for congestion even prior to the full build-out of the sub-division.

Bertram Road functions as a primary connection (along with Johnson Road) to the Kwinana Freeway at Mortimer Road. It also is the only access point to King's College. Peak period traffic movement clash and create a congested environment.

Other streets operate as neighbourhood connectors, but access is constrained at peak times due to conflicts with adjacent land uses, the impacts of intersection controls, or other capacity-limiting effects.

There are a number of options to mitigate these impacts, each dependent on land use and multi-modal transport decisions. Some of the key options are:

- Road capacity: can be increased through duplication or intersection improvements. The City currently operates numerous Developer Contribution Schemes which have identified key road project upgrades required to cope with the additional urban development that is occurring. This can forestall congestion along key corridors, but ultimately results in negative traffic impacts in Centres and arterial roads, as well as diminished viability for all other forms of transport. Consistent increases in road capacity are also related to demand growth for parking, perpetuating car-centric development.
- Parking Constraint: Parking is one of the key factors that drive the use of private vehicles. Where residential parking is abundant, vehicle ownership is high and driving makes up a larger proportion of trips. Reducing commercial all-day parking is one of the best ways to induce mode shift by employees.



- **Proximity and Density:** Where destinations are located far away, private car use may be the only reasonable mode for access. For this reason, dense mixed-use developments generate far fewer car trips than disperse single-use neighbourhoods. The City's land use strategy can assist in creating effective communities that minimize travel to reach services. Similarly, the combination of industrial development and nearby residential zones can help to limit travel distances, containing the impacts of trip generation and protecting the wider regional network.
- **Mode Shift:** traffic congestion is one of the primary determinants of mode choice. Land use growth creates both the demand for alternative modes and the incentive (through congestion effects) to shift to them. However, if this demand is left unsatisfied, and sufficient public transport and safe cycling/pedestrian facilities are not provided, then development may stall or networks collapse.

#### 5.17.4 Parking

Parking infrastructure is an essential and inherent component of both the transport and land use system. In general, parking in the City is abundant and free. As outlined earlier, park 'n' ride at train stations supports regional transport by train.

In general, parking in the City is abundant and free. Private parking minimums are in-line with expected demand rates, suggesting that overspill onto the adjacent network would be limited. Parking on-street is unrestricted, or managed by time and duration only. One exception to this is the commuter parking facilities located adjacent to the Kwinana and Wellard Train Stations, which are operated and maintained by the State Government.

Abundant at-grade parking reduces the capacity for pedestrian-scale activation, with corresponding effects on commercial viability and social cohesion. This is exemplified by the significant majority of developments which face internal car parks rather than the street.

Despite local parking 'hotspots' in and around the Aquatic Centre and other specific locations, there is an oversupply of car parking in most areas (on-street and off-street).

#### 5.17.5 Pedestrian Infrastructure

Pedestrian travel is much more localised than other transport modes, but vital for the function of land use and transport systems. Outside of centres, high quality pedestrian facilities support residential travel to shopping and schools, connection to public transport facilities and recreation. An attractive and safe pedestrian realm results in improved health and social outcomes for residents. Attractive pedestrian environments also improve economic outcomes, attracting more residents and businesses.

Pedestrian activity and connectivity are also critical factors in the effectiveness and vitality of Centres and Corridors. For this reason, the pedestrian environment must be carefully considered, particularly along primary pedestrian routes. This includes construction of high quality paths, shade trees and street furniture to provide amenity. By allocating suitable resources to the pedestrian environment, the use of pedestrian modes will grow, reducing the demand for other modes as well as the requirement for parking.

Parking location can be key to determining both traffic and pedestrian movement. The location of car parking towards the periphery of the Centre limits the impact of parking on trip volumes and land consumption, and requires parkers to travel an additional distance to their destination. The acceptance of peripheral car parking will be significantly improved where attractive legible pedestrian facilities are provided.

Higher-traffic areas with a concentration of pedestrians require good quality, connected, covered and shaded paths, but so do paths which connect areas of high demand across relatively long distances, approaching or exceeding the nominal 400m or 800m walkable catchment.

Key pedestrian need has been identified for the existing network, consisting of:

- within activity centres and from peripheral parking to employment destinations;
- along and across activity corridors;
- along and across transit corridors, and along adjoining minor streets within a 200m walking distance;
- within a 400m walking catchment of schools, shopping precincts and recreation venues, oriented towards the destination;
- within 800m of a train station or high-priority transit, oriented towards the node.

Further analysis of the existing pedestrian environment was undertaken and the following points, amongst others, were made:

- Crossing points along major corridors are widely spaced and generally provide no priority for pedestrians. These barriers induce car dependence by local residents, even where destinations are located nearby.
- The pedestrian environment is characterised by long distances and little to no shade or shelter. The introduction of street trees and awnings would benefit pedestrians and cyclists.
- The orientation of development in the Kwinana City Centre and other retail/commercial areas, where shops and restaurants face large at-grade car parks, is generally hostile to pedestrians.
- Pedestrian access to bus stops in many locations is lacking, even where compliant stop infrastructure has been built.

#### 5.17.6 Cycling

The cycling environment of Kwinana is largely captured in the City of Kwinana Bike and Walk Plan, from which Figure 51: Long-Term Cycling Plan shows the primary and secondary routes through the area. This plan reviewed the extent and sufficiency of bicycle infrastructure and recommended changes based on establishing a network for cycling that improved safety, linking communities and facilities for all types of cyclists, as well as pedestrians.

Kwinana benefits from having a Primary Cycling Route in the form of a Principal Shared Path (PSP) running alongside Kwinana Freeway and connecting to Kwinana Train Station. However, the PSP does not provide access to the Wellard Train Station or the Kwinana City Centre.

Recreational Shared Path (RSP) runs along the ocean foreshore to the south (within the City of Rockingham) and the north (within the City of Cockburn). There is a sizeable gap in network which is a result of the Kwinana Industrial Area (KIA). Future expansion of the KIA to the north and west of Kwinana creates the potential for a greater cycling mode share by workers. To ensure cyclists' ease and safety in traversing the area, sufficient cycling infrastructure will need to be implemented, with particular consideration in corridors used for high-volume traffic and freight.

Sealed shoulders running along many of the strategic roads, including Thomas Road, Rockingham Road and Anketell Road provide long-distance connections for high confidence 'strong and fearless' commuter cyclists who are willing to travel alongside heavy vehicles at 80km/hr.

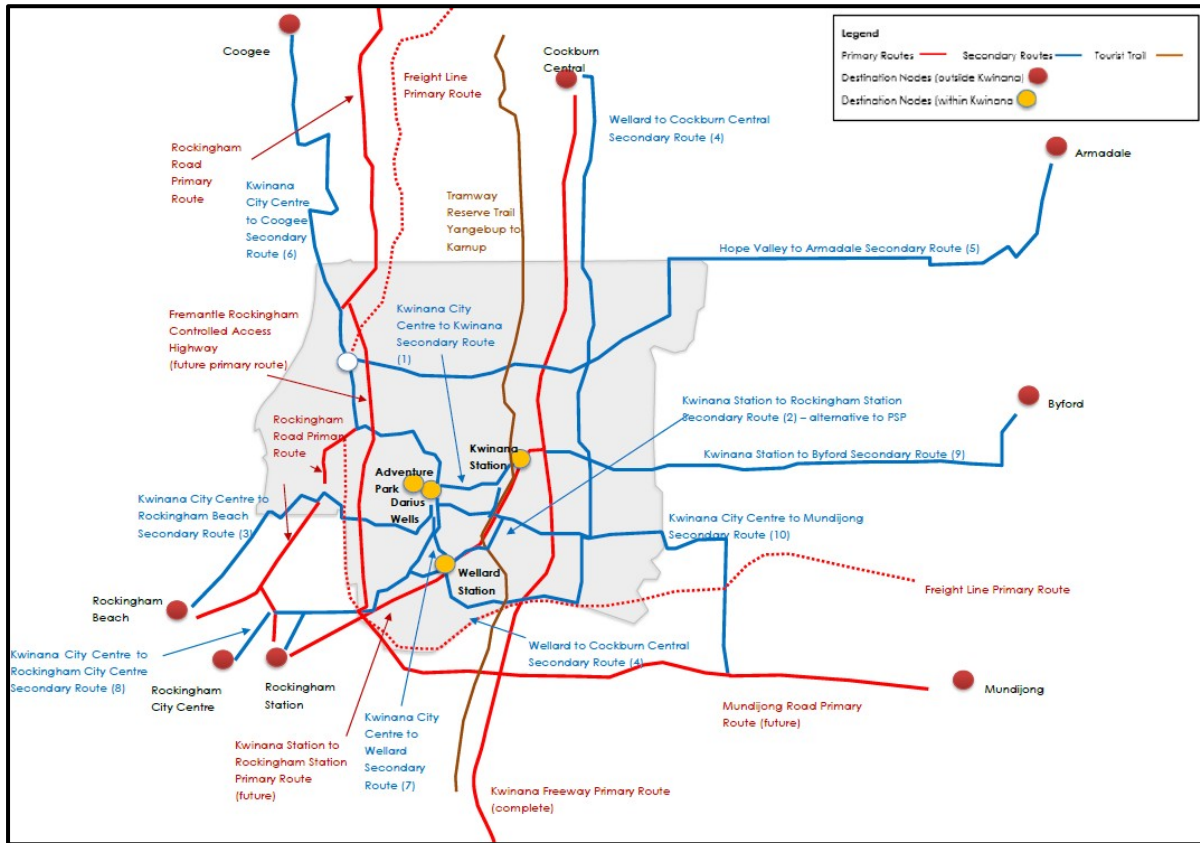


Figure 51: Long-Term Cycling Plan

Cycling lanes exist along much of the local distributor network, along Gilmore Avenue (south of Runnymede Gate), Runnymede Gate, Lambeth Circle, Calista Avenue etc. The form of these cycling facilities generally consists of narrow unprotected lanes adjacent to traffic, and broken up by roundabouts where cyclists must merge into mixed traffic. Again, this form of infrastructure is suitable for high-confidence cyclists, but will not attract more cautious first-time commuters.

Quiet residential neighbourhood streets and slow-speed Activity Centre corridors allow safe cycling in mixed traffic. However, the structure of historic suburban development creates poor legibility for cyclists, as well as for cars. This combined with the distance between residential areas and activity centres greatly reduces the propensity for local trips by active transport modes.

The City's Bike and Walk Plan proposes a network of future off-street and on-street facilities at strategic and local scales. This comprehensive network of paths will support fine-grained cycling connections to destinations in the City, and regional connections to transport hubs and Primary Corridors.

The plan also acknowledges the higher order strategic network and is broadly similar to the Department of Transport's planning for cycling facilities, as expressed by the Bicycle Network Plan for Perth @ 3.5 million. However, the density of key routes as described by the City's Bike and Walk Plan is expected to better support the needs of the community.

The City includes cycling infrastructure in the majority of new developments, improving the connectivity for cyclists traversing the City, but older suburban areas provide limited infrastructure for active modes.

### 5.17.7 Public Transport

Public transport within the City includes a range of local collector bus services and a high capacity train service.

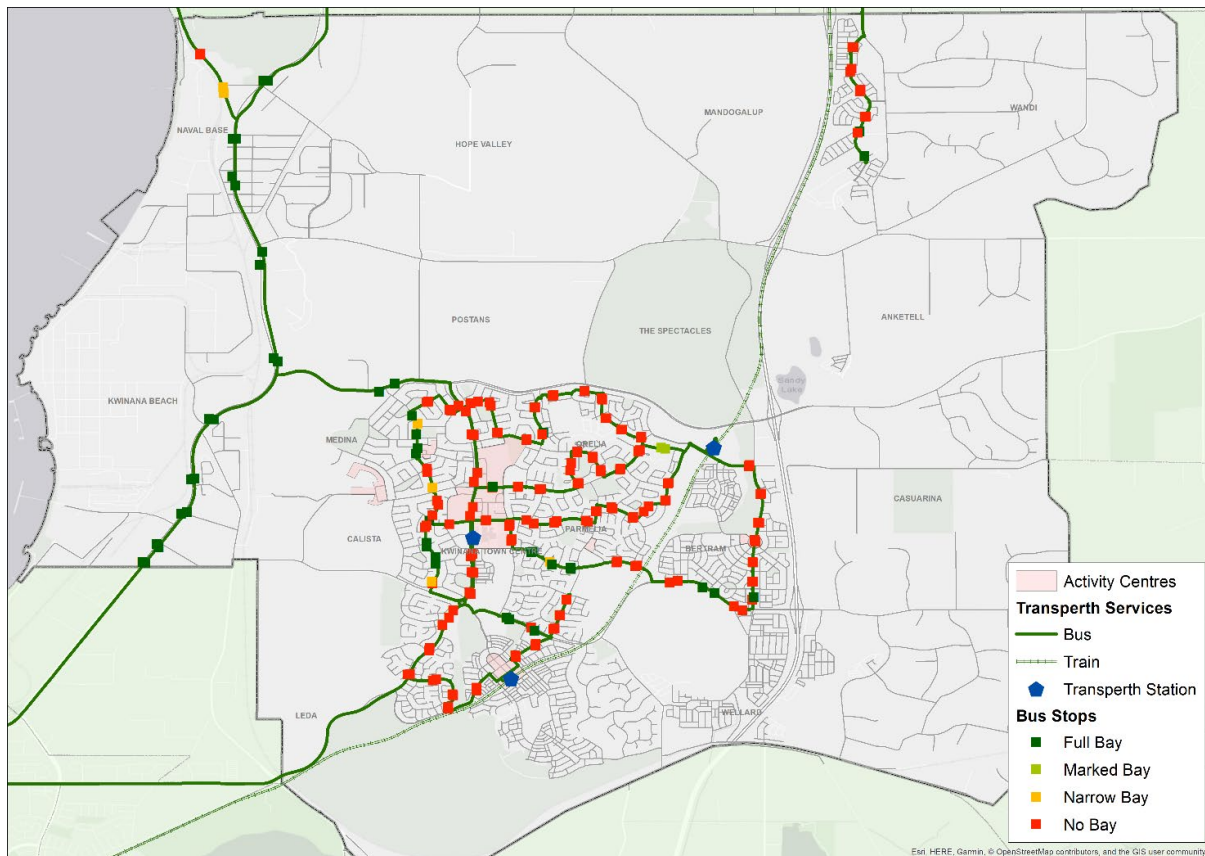


Figure 52: Public Transport Network

Population within the City of Kwinana will approximately double in the next 20 years. This will necessarily increase the demand for transport across the network. Given the overall growth in residential and employment over this period, there is likely to be insufficient road capacity to accommodate the corresponding transport demand under the current paradigm of predominantly peak-hour private vehicle trips.

As such, an alternative framework and infrastructure solution must be considered which prioritises the movement of people and goods over traffic. Public transport is an ideal mode, able to provide regional travel for large numbers of people within a relatively small cross-section.

Current use of public transport in the City is relatively good, in particular the journey to work by train, as shown in Table 36: Method of travel to work – Resident Workers being more than double that of the State.

#### 5.17.7.1 Bus Network

The City of Kwinana is serviced by seven bus routes, as shown in Figure 53: Bus Routes in the City of Kwinana (Central). The majority of these services run as relatively infrequent collector routes, operating approximately every 20 minutes routes during peak periods, and hourly off-peak.

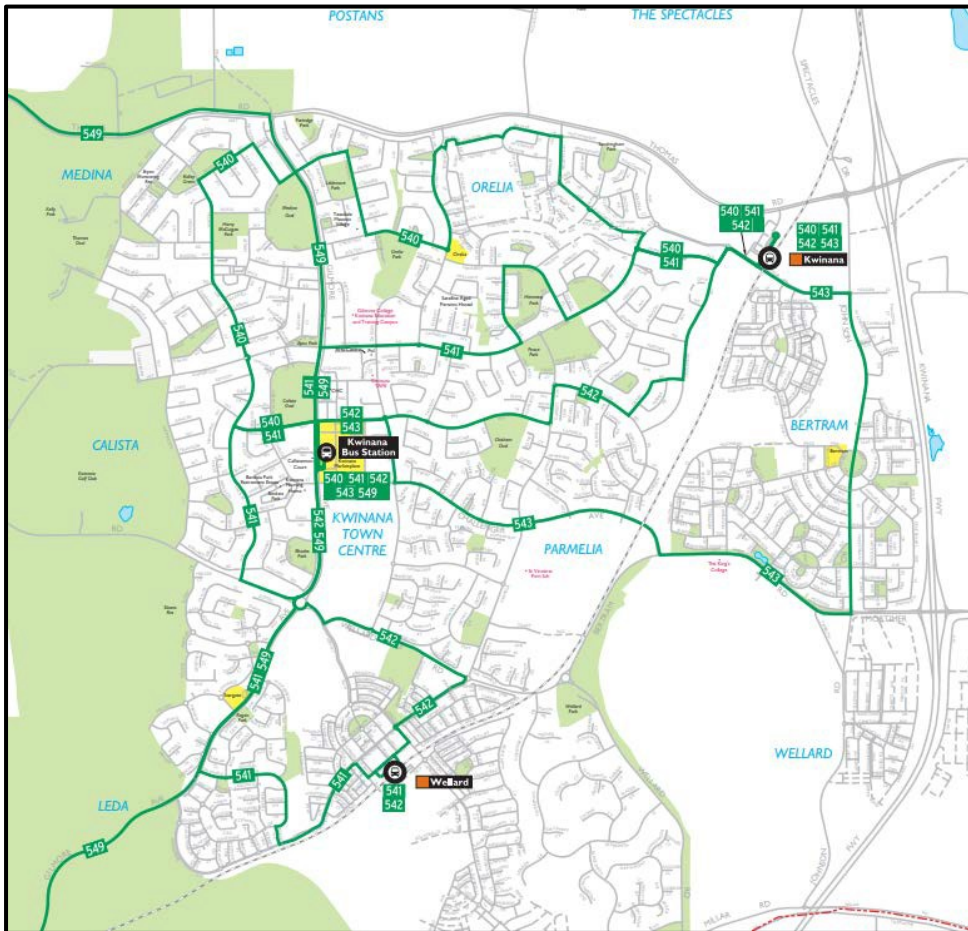


Figure 53: Bus Routes in the City of Kwinana (Central)

The Routes 548 and 549 services provide the sole connection in the area between Kwinana and Fremantle, which is otherwise poorly served by the rail network. With the exception of the coastal Route 548, all buses connect to the Kwinana City Centre via the Kwinana Bus Station.

As a result of the overlap between services, some corridors in the City are already well-served by transit. Gilmore Avenue experiences significantly more bus services than any other corridor, and as a consequence, attracts much higher patronage. The daily patronage along other corridors in residential neighbourhoods ranges from 25 to 170 boardings and alightings per day.

Structure planned areas currently under development generally do not have access to bus transport. The Public Transport Authority (PTA) can be expected to provide services, as dwelling numbers reach a certain threshold, but this is likely to lag development by some years. It will also be key to ensure that structure planning processes seek to link bus networks to planned centres, as destinations, to maximise patronage and reduce the reliance on private vehicle travel.

#### 5.17.7.2 Train Network

The City is serviced by the Mandurah Train Line, stopping at Kwinana and Wellard Train Stations. The service frequency to both Kwinana and Wellard Train Stations is an average of 10 minutes during peak periods and 15 minutes off peak.

The Kwinana Train Station is located well to the north of residential and commercial zones, reducing its local catchment by at least 50 per cent. There is limited development within walking distance of

Kwinana Train Station. This creates a high demand for the park 'n' ride service even from the surrounding residential development.

The ongoing growth of regional demand is likely to overwhelm the capacity of commuter park 'n' ride at the Stations. Alternative arrangements including improved bus transit will be necessary to offset this impact. Otherwise, additional park 'n' ride facilities will be required, that has the potential with a corresponding impact on local traffic volumes, congestion and amenity.

Local residents must compete with regional demand for park 'n' ride at the station. This is exacerbated by the lack of bus transport alternatives in some new development areas. Public transit options and connectivity and improvement of transit options to stations should be enhanced.

Limited car parking capacity exists at Wellard Train Station. If spill over parking issues are noted throughout the Wellard Town Centre, the introduction of time limited parking should be supported.

Previous investigations have been conducted into additional Stations in the Mandogalup/Anketell and Bertram locations.

While these Stations are not currently under consideration by State Government agencies, there may be future opportunities to integrate high-capacity public transport hubs into the future District Shopping Centre at Anketell Road, or to create a Station with better accessibility to the Kwinana City Centre.

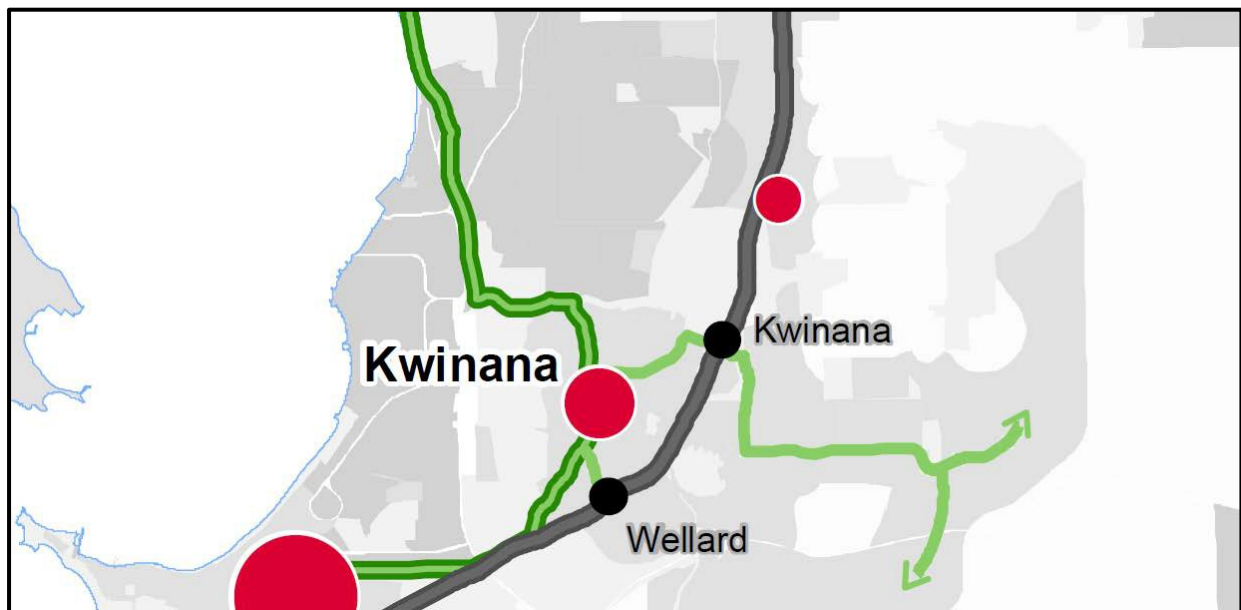


Figure 54: High-Priority and High-Frequency Transit Corridors

The South Metropolitan Peel Sub-Regional Planning Framework identifies two strategic bus corridors through the City, as shown in Figure 54: High-Priority and High-Frequency Transit Corridors:

- A High-Frequency Transit Corridor running between Kwinana Train Station and Wellard Train Station via the Kwinana City Centre.

This could support improvements to the connectivity between the two train stations as a transit corridor.

- A High-Priority Transit Corridor between Rockingham and Fremantle via the Kwinana City Centre.

This would improve direct access to Kwinana, but is primarily intended to be an efficient commuter service delivering employees to destinations along the coast, including the Outer Harbour and supporting industry. This corridor is likely to require bus priority in the form of queue facilities or partial bus lanes to maintain consistent times.

#### 5.17.8 Key Issues

The orientation of development in the Kwinana City Centre and other retail/commercial areas, where shops and restaurants face large at-grade car parks, is generally apathetic to pedestrians. The majority of land development in Kwinana remains in low-density neighbourhoods and commercial/industrial zones. These types of development create a separation between residence, work and activity which perpetuates car-focused transport choices.

Pedestrian environments are often squeezed by the demands of other transport modes with easily to define dimensions.

The majority of on-road cycling facilities cede priority to traffic at critical locations in the network, this reduces their attractiveness and risk performance. In particular, the requirement for cyclists to merge into traffic at numerous roundabout locations.

A major theme arising from the engagement process for this Strategy was that people wanted to cycle, but felt that there was not appropriate infrastructure for them to undertake this safely.

Many areas in the City have coverage but low frequency, for public transport options. High-capacity public transport is enabled by dense development adjacent to the corridor. Where residential areas primarily low-density, then demand is unlikely to support frequent or higher priority public transport.

Local residents must compete with regional demand for park 'n' ride at the station. This is exacerbated by the lack of bus transport alternatives in some new development areas.

Increased traffic volumes in the future are likely to increase along local distributor and collector roads, which has the potential to reduce the utility of these corridors for mixed-traffic cycling and painted cycle lanes.

Good quality streetscapes reinforce the place-making functions of a street, and enhance amenity through appropriate infrastructure, shade and positive accessibility measures. These attributes could also encourage uptake of active transportation modes, increase the land value of adjacent development, and induce additional economic activity.

The City is proposed to be home to the Kwinana outer harbour, a bulk cargo port with strategic significance to Western Australia. Providing enhanced, efficient transport networks and land use relationships will be vital to facilitate these activities.

#### 5.17.9 Response to issues

The recently adopted Kwinana Bike and Walk Plan recommends a range of paths, safe active streets and strategic on-road facilities. This Plan generally describes the specific opportunities inherent in the broader network. As part of road upgrade projects, the City will construct pedestrian and cycling facilities in accordance with the Bike and Walk Plan.

Modification of suburban roundabouts to improve cycling safety requires very little change. The conversion of standard high-speed roundabouts to compact or European-style roundabouts could be of significant benefit along designated cycling corridors.

The South Metropolitan Peel Sub-Regional Planning Framework identifies two strategic bus corridors and the City should support their provision to improve public transport options, reliability and frequency within the City.

There are possibilities for private or public-private partnership transport services to be developed by businesses, Local and State Government partners. Services might consist of branded coaster-sized transit operating in a 'petal route' formation around the central rail hub or bus interchange. This service becomes more viable where multiple businesses see benefit, as electric vehicle technology reduces running costs, and following the advent of automated technology.

The City should aim to enhance connectivity and improvement of transit options to stations through direct provision of cycle and walking infrastructure and through advocacy to relevant state agencies. This will also assist in addressing issues with parking availability at park and ride facilities.

The City recognises and supports the strategic importance of the wider freight objectives required for efficient logistics. The City will avoid inappropriate development and location of sensitive land uses near freight routes.



## 5.18 Service Infrastructure

Utilities such as water supply, waste water, electricity, gas and telecommunications are largely provided by state government agencies and in areas of expansion developers are required to construct infrastructure to a minimum standard as prescribed by these agencies.

The City is currently serviced by Increasing residential density will have a direct impact on the existing Service Infrastructure such as Water, Wastewater, Storm Water, Telephone and National Broadband Network (NBN), Electricity and Gas.

### 5.18.1.1 Water and wastewater infrastructure

The majority of areas within the City of Kwinana are connected to the sewer network, however the rural and industrial areas often require on-site effluent disposal systems to be installed.

Conventional primary treatment septic systems are no longer approved in the City of Kwinana. Only certain approved wastewater systems can be installed within the City of Kwinana environmentally sensitive areas including within the Public Drinking Water Source, Peel Harvey Catchment or Cockburn Sound Catchment areas.

The Water Corporation is currently operates a waste water facility in Postans and is generally comfortable with the water and wastewater infrastructure capacity available within the City. Based on the City's projected infill development target, the Water Corporation is willing to run a model in order to identify whether the existing infrastructure can cope with the projected target and what upgrading is needed to be carried out. This would ensure the water and wastewater infrastructure is not compromised by higher density development or the future urban expansion that is and will continue to occur along the urban corridor.

### 5.18.2 Stormwater Drainage Infrastructure

City's stormwater drainage assets as of 2018 is in shown in Table 37: Stormwater drainage assets.

Asset Category	Dimension
Pipes	191.72km
Pits	8.345 units
Culverts	194 units
Soakwells	137 units

Table 37: Stormwater drainage assets

The creation, acquisition and upgrade of stormwater drainage infrastructure, within the City, is predominantly driven by land development. In the case of key strategic links, these stormwater drains have been identified in planning documents, such as structure plans, and are funded through a developer contribution plan.

#### 5.18.2.1 Telephone, NBN, Electricity and Gas.

The objective is to ensure that expansion areas are capable of being supplied with telephone, NBN, electricity and gas services. The existing areas within Kwinana al have connections to these services including NBN which has been rolled out within the City.

Any new development is required to be provided with these services and this is a consideration as part of the structure planning process for expansion areas.

### 5.18.3 Key Issues

Current population growth, which will be accommodated within expanding greenfield urban areas, will require servicing with all the necessary requirements to ensure the future communities in those locations are well provided.

However, it will be important to ensure that any infill development is undertaken with consultation with service providers to ensure that existing infrastructure has the capacity to cater for an increased population.

### 5.18.4 Response to issues

The City will consult and work with the relevant service providers to ensure infrastructure needs are met by the growing populations both in greenfield and infill areas.

# Tomorrow's Kwinana



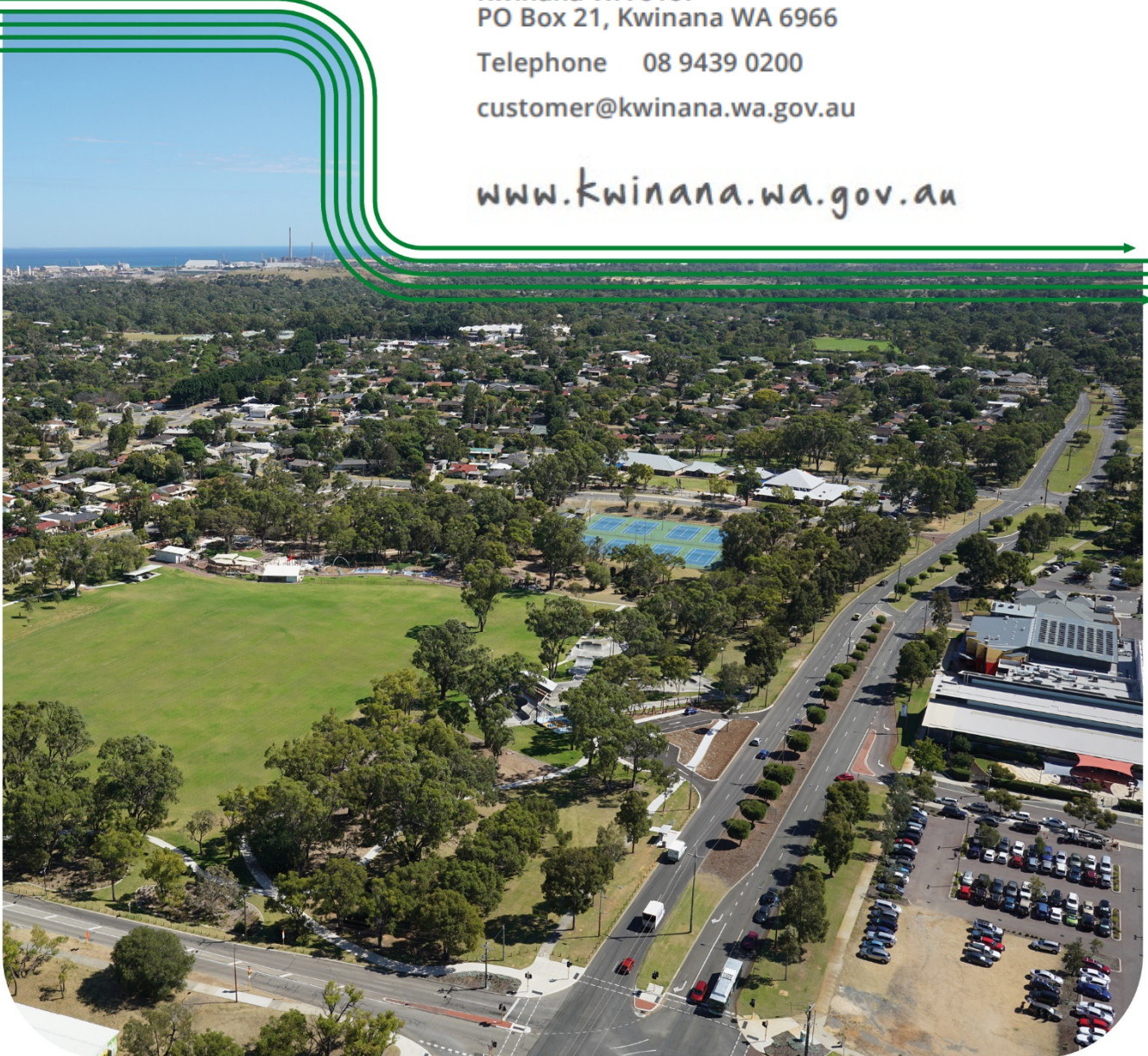
## ADMINISTRATION

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## 17.2 Proposed Road Names for Lots 52 and 682 Rowley Road, Mandogalup (Stage 7 – 17)

### DECLARATION OF INTEREST:

There were no declarations of interest declared.

### SUMMARY:

MNG, the surveyor for the developer of Lots 52 and 682 Rowley Road, Mandogalup, has submitted details of proposed road names for new roads being constructed as part of their development, as indicated in Attachment A. MNG is now seeking Council support for the proposed road names, in order to present the names to the Geographic Names Committee for approval. In addition, MNG is seeking support for alternative road names, as shown in Attachment B.

Geographic Names has granted 'in principle approval' for the use of these road names, via passing preliminary validation on their 'request road name' web page. The listed alternative road names will be used as a substitute if the proposed road name is not approved by Geographic Names Committee. The naming theme for the roads in this subdivision is 'Australian Explorers' which is consistent to the existing 'Explorers and Pioneers of Australia' theme of the surrounding development.

The information regarding the origin of the proposed road names is contained in Attachment B.

### OFFICER RECOMMENDATION:

That Council approve the following road names for use within Lots 52 and 682 Rowley Road, Mandogalup (Stage 7-17) as shown in Attachment A.

Proposed Names:	Alternative Names:
Ainsworth Crescent	Blaxland
Dampier Road	Grimes
Ferdinand Street	Hartog
Hann Way	Kekwick
Ludwig Lane	Lort
Mcmillan Road	Pelsart
Tasman Crescent	Pickmore
Tench Lane	Septimus
Walker Road	Severin
Warburton Street	
Windich Road	
Wynford Road	

### DISCUSSION:

Before the developer of a subdivision can lodge survey diagrams for clearance, all road names need to be approved and shown on the survey diagram. The process for naming roads must adhere to the Geographic Names Committee guidelines to ensure no duplication of road names occurs within the surrounding areas.

### 17.2 PROPOSED ROAD NAMES FOR LOTS 52 AND 682 ROWLEY ROAD, MANDOGALUP (STAGE 7 – 17)

Geographic Names Committee has granted 'in principle approval' for the use of these road names via passing preliminary validation on Landgate's 'request road name' web page. The naming theme for the road in this subdivision is 'Australian Explorers'. This theme is consistent with the existing 'Explorers and Pioneers of Australia' theme in this development.

Nine road names are proposed as alternative road names for use in the event that the any of the proposed names are not approved by the Geographic Names Committee . The information regarding the origins of these road names can be found in Attachment B.

The proposed road names for Lots 52 and 682 Rowley Road, Mandogalup (Stages 7 – 17) are:

<b>Proposed Names:</b>	<b>Alternative Names:</b>
Ainsworth Crescent	Blaxland
Dampier Road	Grimes
Ferdinand Street	Hartog
Hann Way	Kekwick
Ludwig Lane	Lort
Mcmillan Road	Pelsart
Tasman Crescent	Pickmore
Tench Lane	Septimus
Walker Road	Severin
Warburton Street	
Windich Road	
Wynford Road	

#### **LEGAL/POLICY IMPLICATIONS:**

The approval process is in compliance with the Geographic Names Committee Guidelines, and Council Policy – Street Naming.

#### **FINANCIAL/BUDGET IMPLICATIONS:**

No financial/budget implications have been identified as a result of this report or recommendation.

#### **ASSET MANAGEMENT IMPLICATIONS:**

No asset management implications have been identified as a result of this report or recommendation

#### **ENVIRONMENTAL IMPLICATIONS:**

No environmental implications have been identified as a result of this report or recommendation.

17.2 PROPOSED ROAD NAMES FOR LOTS 52 AND 682 ROWLEY ROAD, MANDOGALUP (STAGE 7 – 17)

**STRATEGIC/SOCIAL IMPLICATIONS:**

There are no strategic/social implications as a result of this proposal.

**COMMUNITY ENGAGEMENT:**

There are no community engagement implications as a result of this report

**PUBLIC HEALTH IMPLICATIONS:**

There are no implications on any determinants of health as a result of this report.

**RISK IMPLICATIONS:**

The risk implications in relation to this proposal are as follows:

Risk Event	The approval of the road names is required for titles to be issued for the lots within the subdivision. Should Council or Geographic Names not approve these road names, clearances may be delayed which will have implications for the developer and the owners of these lots
Risk Theme	Errors omissions delays
Risk Effect/Impact	Service delivery
Risk Assessment Context	Operational
Consequence	Insignificant
Likelihood	Possible
Rating (before treatment)	Low
Risk Treatment in place	Reduce (mitigate the risk)
Response to risk treatment required/in place	Work instructions in place for Geographic Names approvals and sufficient information and alternative names for Council approvals.
Rating (after treatment)	Low

**COUNCIL DECISION**

386

**MOVED CR S LEE**

**SECONDED CR P FEASEY**

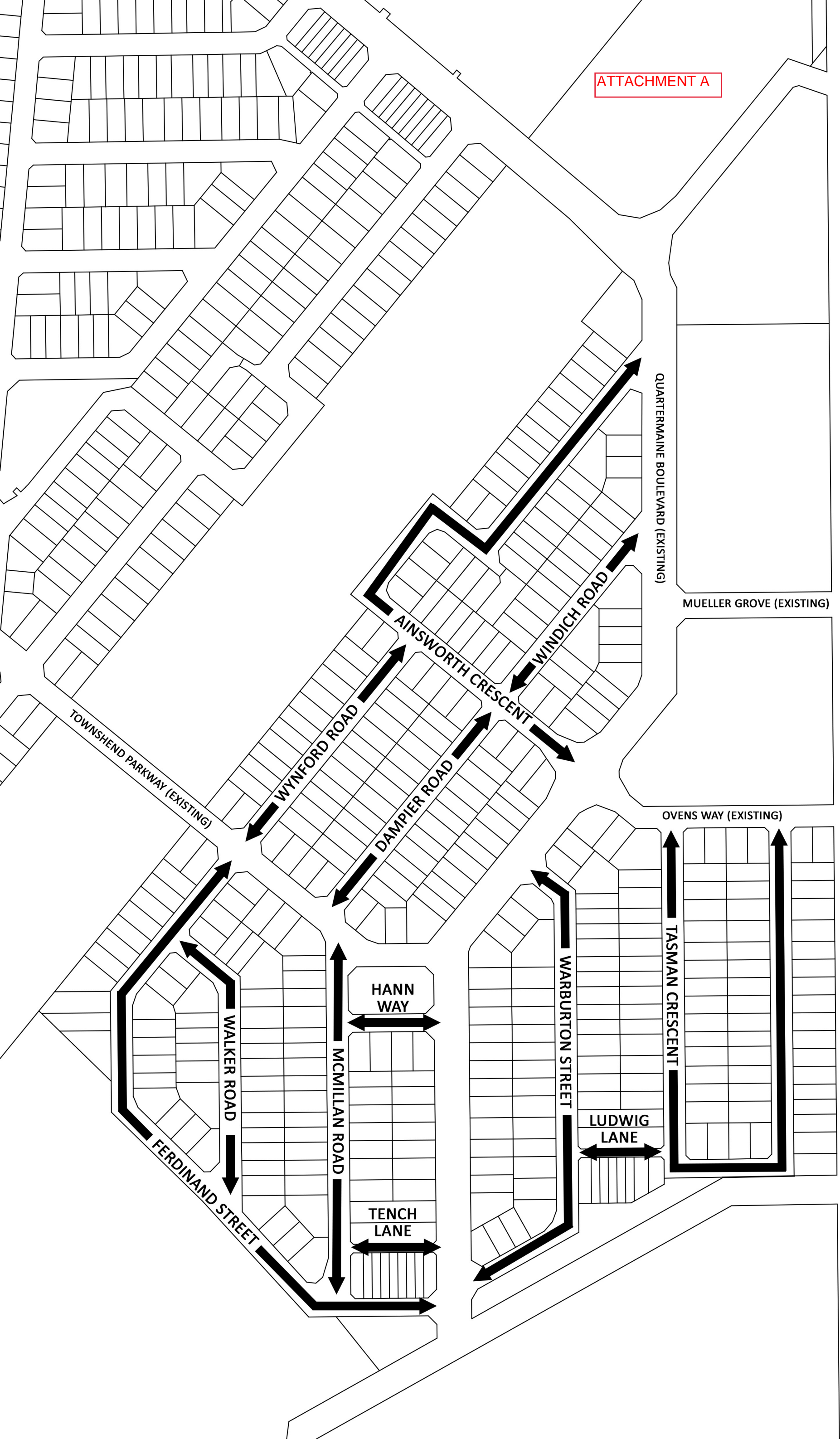
**That Council approve the following road names for use within Lots 52 and 682 Rowley Road, Mandogalup (Stage 7-17) as shown in Attachment A.**

<b>Proposed Names:</b>	<b>Alternative Names:</b>
<b>Ainsworth Crescent</b>	<b>Blaxland</b>
<b>Dampier Road</b>	<b>Grimes</b>
<b>Ferdinand Street</b>	<b>Hartog</b>
<b>Hann Way</b>	<b>Kekwick</b>
<b>Ludwig Lane</b>	<b>Lort</b>
<b>Mcmillan Road</b>	<b>Pelsart</b>
<b>Tasman Crescent</b>	<b>Pickmore</b>
<b>Tench Lane</b>	<b>Septimus</b>

17.2 PROPOSED ROAD NAMES FOR LOTS 52 AND 682 ROWLEY ROAD, MANDOGALUP (STAGE 7 – 17)

<b>Walker Road</b>	<b>Severin</b>
<b>Warburton Street</b>	
<b>Windich Road</b>	
<b>Wynford Road</b>	

**CARRIED  
6/0**





**ATTACHMENT B**

Proposed Name	Proposed Suffix	Source of info	Background/origin/meaning/justification
Ainsworth	Crescent	<a href="http://gutenberg.net.au/pages/horrocks.html">http://gutenberg.net.au/pages/horrocks.html</a>	John Ainsworth Horrocks (1818-1846) was an explorer and pastoralist who explored South Australia. He was possibly the first explorer to utilise camels on expeditions.
Dampier	Road	<a href="http://gutenberg.net.au/pages/dampier.html">http://gutenberg.net.au/pages/dampier.html</a>	Dampier was born in England. His parents both died before he left school, and at his own desire he was sent to sea. In January 1688 Dampier actually landed in northern Australia at King Sound or Collier Bay
Ferdinand	Street	<a href="https://en.wikipedia.org/wiki/Ferdinand_von_Mueller">https://en.wikipedia.org/wiki/Ferdinand_von_Mueller</a>	A botanist and explorer who explored the gascoigne river and parts of Geraldton. (1825-1896)
Hann	Way	<a href="https://adb.anu.edu.au/biography/hann-frank-hugh-3906">https://adb.anu.edu.au/biography/hann-frank-hugh-3906</a>	William Hann (1837-1889), and Frank Hugh Hann (1846-1921), explorers and pastoralists, were the sons of Joseph Hann and his wife Elizabeth, née Sharpe. The family migrated from Wiltshire in 1851 to the Westernport district in Victoria.
Ludwig	Lane	<a href="http://adb.anu.edu.au/biography/leichhardt-friedrich-wilhelm">http://adb.anu.edu.au/biography/leichhardt-friedrich-wilhelm</a>	Friedrich Wilhelm Ludwig Leichhardt, known as Ludwig Leichhardt, was a Prussian explorer and naturalist, most famous for his exploration of northern and central Australia. (1813-1848)
Mcmillan	Road	<a href="http://gutenberg.net.au/ebooks15/1500721h/0-dict-biogMc.h">http://gutenberg.net.au/ebooks15/1500721h/0-dict-biogMc.h</a>	McMILLAN, ANGUS (1810-1865), was born at Glenbrittle, Skye, off the west coast of Scotland, in 1810. His diary, which in 1925 was in private keeping at Sale, Victoria, shows that he left Scotland on 13 September 1837 as a cabin passenger in the Minerva, and arrived at Sydney on 23 January 1838. Millan was a natural leader whose tact, good sense and kindness enabled him to get on well with his men, including the aborigines, and he has long been recognized as one of the great pioneers of Victoria.
Tasman	Crescent	<a href="https://en.wikipedia.org/wiki/Abel_Tasman">https://en.wikipedia.org/wiki/Abel_Tasman</a>	Abel Janszoon Tasman was a Dutch seafarer, explorer, and merchant, best known for his voyages of 1642 and 1644 in the service of the Dutch East India Company. Tasman left Batavia on 30 January 1644 on his second voyage with three ships. He mapped the north coast of Australia, making observations on New Holland and its people.[29] He arrived back in Batavia in August 1644.
Tench	Lane	<a href="http://gutenberg.net.au/pages/tench.html">http://gutenberg.net.au/pages/tench.html</a>	Tench was born in England. He became familiar with Latin and French and spent some time in France during his early years. He served in the Royal Navy during the American War of Independence and in 1786 volunteered for three years service with the First Fleet.
Walker	Road	<a href="http://gutenberg.net.au/pages/walker.html">http://gutenberg.net.au/pages/walker.html</a>	Frederick Walker was born in England around 1820 and died of gulf fever in Floraville, Queensland on 19 September 1866. Walker emigrated to Australia as a young man. He held the position of Clerk of Petty Sessions in Tumut, NSW, before he was appointed as the first Commandant of the Native Police on the recommendation of William Charles Wentworth and Augustus Morris, two members of the Legislative Council.
Warburton	Street	<a href="http://gutenberg.net.au/ebooks15/1500721h/0-dict-biogWa.h">http://gutenberg.net.au/ebooks15/1500721h/0-dict-biogWa.h</a>	Born at Northwich, Cheshire, England, on 15 August 1813. Educated largely in France he joined the royal navy in 1826. In 1829 he entered the East India Company's military college at Addiscombe, and in 1834 went to India. He remained in the East India Company's service until 1853, when he retired with the rank of major and emigrated to Western Australia. After a short stay he went to Adelaide, and at the close of the year was appointed commissioner of police. About this time he did some exploring in the country west of Lake Torrens, and made an unfavourable report on it. In the following year he was able to determine the size and shape of Lake Torrens. Warburton carried out his duties as commissioner of police until 1867, and two years later became colonel commandant of the South Australian volunteer forces.
Windich	Road	<a href="http://adb.anu.edu.au/biography/windich-tommy-4871">http://adb.anu.edu.au/biography/windich-tommy-4871</a>	Tommy Windich 1840-1876 was an Aboriginal Tracker and explorer that assisted John Forrest, and others, on numerous expeditions in Western Australia.
Wynford	Road	<a href="http://adb.anu.edu.au/biography/carnegie-david-wynford-550">http://adb.anu.edu.au/biography/carnegie-david-wynford-550</a>	David Wynford Carnegie was an explorer and gold prospector who charted territories from Coolgardie to the Gibson Desert. (1871-1900)

Proposed Backup Name	Source of info	Background/origin/meaning/justification
Blaxland	<a href="http://gutenberg.net.au/ebooks15/1500721h/0-dict-biogBe-B">http://gutenberg.net.au/ebooks15/1500721h/0-dict-biogBe-B</a>	Born in Kent, England, in 1771 (Burke's Colonial Gentry 1891). In 1805, with his brother John Blaxland (q.v.), he arranged with the government to go to Australia as a free settler.
Grimes	<a href="http://gutenberg.net.au/ebooks15/1500721h/0-dict-biogG.ht">http://gutenberg.net.au/ebooks15/1500721h/0-dict-biogG.ht</a>	Surveyor General of New South Wales (1790) and discoverer of the Yarra. (1772-1858)
Hartog	<a href="https://en.wikipedia.org/wiki/Dirk_Hartog">https://en.wikipedia.org/wiki/Dirk_Hartog</a>	Dirk Hartog was a 17th-century Dutch sailor and explorer. Dirk Hartog's expedition was the second European group to land in Australia and the first to leave behind an artefact to record his visit, the Hartog plate.
Kekwick	<a href="http://gutenberg.net.au/pages/kekwick.html">http://gutenberg.net.au/pages/kekwick.html</a>	Kekwick was second in command on John McDouall Stuart's expeditions. On the 22nd of April 1862, Stuart camped in the centre of Australia, and one of his hopes was accomplished; about two miles and a-half to the N.N.E. was a tolerable high mount, which he called Central Mount Stuart.
Lort	<a href="https://www.portrait.gov.au/portraits/2013.24.1/captain-john-lort-stokes">https://www.portrait.gov.au/portraits/2013.24.1/captain-john-lort-stokes</a>	John Lort Stokes was an explorer and hydrographer who navigated and surveyed the Western Australian coast. (1812-1885)
Pelsart	<a href="https://en.wikipedia.org/wiki/Francisco_Pelsaert">https://en.wikipedia.org/wiki/Francisco_Pelsaert</a>	(c. 1595 – September 1630) was a Dutch merchant who worked for the Dutch East Indies Company, who became most famous as the commander of the ship <i>Batavia</i> , which ran aground in the Houtman Abrolhos off the coast of Western Australia in June 1629.
Pickmore	<a href="http://adb.anu.edu.au/biography/bussell-alfred-pickmore-312">http://adb.anu.edu.au/biography/bussell-alfred-pickmore-312</a>	Alfred Pickmore Bussell was a settler and pastoralist who settled land in the Augusta/ Vasse Area. (1816-1882)
Septimus	<a href="http://www.wanowandthen.com/John-Septimus-Roe.html">http://www.wanowandthen.com/John-Septimus-Roe.html</a>	John Septimus Roe was the first Surveyor General Of Western Australia. He undertook at least 16 explorations of the State. (1797-1878)
Severin	<a href="https://lostkatanning.com/severin/">https://lostkatanning.com/severin/</a>	(1835-1898) Carl Severin migrated from Prussia to Australia as a 19 yr old. He and his family were an early pioneering family in Katanning.

### 17.3 Development Application – Honeywood Community Pavilion – Lot 8008 Lyon Road and Lot 9034 Honeywood Avenue, Wandí

#### DECLARATION OF INTEREST:

There were no declarations of interest declared.

#### SUMMARY:

The City of Kwinana is proposing to construct a new facility at its existing playing fields in Wandí (Attachment A). The proposal is for a single storey, multipurpose facility at Lot 8008 Lyon Road and Lot 9034 Honeywood Avenue, Wandí (the subject site). The community pavilion is intended to be a multipurpose community hub that provides facilities for sport clubs, social, recreational, cultural and learning activities, programs and services to meet the collective needs of the immediate local area. The site is zoned Special Rural under the Local Planning Scheme No. 2 (LPS2) and Rural Water Protection under the Metropolitan Region Scheme (MRS).

As the use does not fall neatly into any one land use in the Zoning Table of LPS2 due to its multipurpose nature, it is assessed as a Use Not Specified. City officers do not have delegation for these uses and the application is therefore required to be referred to Council for its determination. More importantly, as the site falls within the Jandakot Underground Water Pollution Control Area (UWPCA), the input of the Department of Water and Environmental Regulation (DWER) is extremely important in assessing the appropriateness of a development in this location. DWER has confirmed that it does not object to the proposed development subject to a number of management conditions which are detailed further in the report.

The City consulted with owners and occupiers of surrounding lots within a 200m radius of the subject site and received one (1) submission objecting to the proposal. It is noted that the City has previously undertaken substantial consultation on the project during the design phase.

The application has been assessed against relevant planning legislation and is considered to meet the requirements of LPS2. The development is therefore recommended for conditional approval.

#### OFFICER RECOMMENDATION:

That Council approve the development application for the Use Not Specified (Honeywood community pavilion) (Attachment A) at Lot 8008 Lyon Road and Lot 9034 Honeywood Avenue, Wandí subject to the following conditions in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*:

1. Pursuant to clause 26 of the Metropolitan Region Scheme (MRS), this approval is deemed to be an approval under clause 24 (1) of the MRS.
2. Prior to the lodgement of a building permit application, a Noise Management Plan (NMP) prepared by a suitably qualified acoustic consultant shall be submitted to the City of Kwinana for approval, detailing measures that will be undertaken to ensure noise levels generated by the development are kept within levels prescribed in the *Environmental Protection (Noise) Regulations 1997*.

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

3. Prior to occupation of the development, the facility shall be connected to sewer in accordance with the requirements of the Water Corporation to the satisfaction of the City of Kwinana.
4. Prior to occupation of the development, vehicle crossovers shall be constructed of concrete and located to the specifications and satisfaction of the City of Kwinana.
5. Prior to the completion of crossover works, the applicant shall remove the existing 4 on-street parking bays on Honeywood Avenue through which the proposed car park crossover extends. The applicant will extend the crossover to the carriageway edge, reinstating the verge and landscaping to tie in with existing, to the specification and satisfaction of the City of Kwinana.
6. Car parking areas are to be sealed and drained as per the City of Kwinana Trafficable Areas specification to the satisfaction of the City of Kwinana.
7. Stormwater drainage from roofed and paved areas being contained and disposed of on site at all times, incorporating Water Sensitive Urban Design principles to utilise garden bed and landscape areas for water quality treatment and infiltration, to the satisfaction of the City of Kwinana.
8. Prior to occupation of the development, a dual use footpath shall be installed along the north side of Litoria Drive to a minimum width of 2.5m, linking the existing Honeywood Avenue dual use path to the facility, to the specification and satisfaction of the City of Kwinana.
9. Prior to occupation, the landscaping plan (dated 13 July 2020 as amended), shall be implemented to the satisfaction of the City of Kwinana.
10. During construction, no chemicals or fuels should be kept on site within the Public Drinking Water Source Area on advice from the Department of Water and Environmental Regulation, to the satisfaction of the City of Kwinana.
11. No chemicals shall be kept on the premises, unless residing within a bunded storage area as advised by the Department of Water and Environmental Regulation, to the satisfaction of the City of Kwinana.

Advice notes:

- i. If the development the subject of this approval is not substantially commenced within a period of two (2) years, or another period specified in the approval after the date of the determination, the approval will lapse and be of no further effect.
- ii. The Minister for Planning has issued a formal notice extending the deadline for substantial commencement by an additional two (2) years for all applications approved during the current State of Emergency. In effect, this means that the timeframe for substantial commencement is now four (4) years from the date of this determination.

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

- iii. The occupant shall submit an application for notification/registration of a food premises to the City of Kwinana for approval prior to building occupancy. The food business shall comply with *Food Act 2008*, *Food Regulations 2009* and *Australian New Zealand Food Standards Code*.
- iv. The carpark design shall incorporate best practice water sensitive urban design to utilise garden beds to manage minor rainfall event runoff (first 15mm) for water quality treatment and reduce drainage requirements.
- v. To assist in creating a fauna corridor to facilitate safer movement of Quenda, it is recommended that the landscape plan incorporate low, dense, endemic herbs and shrubs within the median of Honeywood Avenue, carpark swales and associated road reserves and carparks.
- vi. If an applicant or owner is aggrieved by this determination there is a right of review by the State Administrative Tribunal in accordance with the Planning and Development Act 2005 Part 14. An application must be made within 28 days of the determination.
- vii. The applicant is advised that this conditional development approval is not a building permit giving authority to commence construction. Prior to any building work commencing on site a Building Permit must be issued and penalties apply for failing to adhere to this requirement.

**DISCUSSION:****The proposal**

The City of Kwinana is proposing to construct a community pavilion at the existing Wandii sporting ground. A development application was submitted in January 2021 which includes the following:

- Single storey clubrooms
  - Community function room
  - Kitchen
  - Chair and function store
  - Toilets
  - 2 x change rooms with showers
  - Sports store area
  - First aid room
  - Umpire change room
  - Bin store
- Car park
  - 164 standard bays + 4 disabled bays
  - Bicycle racks
  - 2 x entry/exit points

The building is proposed to be 3.8m in height, with a raised ceiling above the community function room that goes to 5.5m.

### 17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI

The pavilion is intended to be a multipurpose community hub that provides facilities for sport clubs, social, recreational, cultural and learning activities, programs and services to meet the collective needs of the immediate local area. The facility will cater for all age groups and will provide opportunities to bring the local community together in order to build relationships, and develop local identity and community spirit. Examples of facility use include sporting club functions and fundraiser activities, fitness classes, children's birthday parties, community gatherings and workshops.

The City of Kwinana will manage and operate the pavilion, including undertaking building maintenance and facilitating community access to the hireable spaces in the building.

The proposed hours of availability are:

- Monday to Thursday 7am to 10pm
- Friday to Sunday 7am to 12am (midnight)

#### **Background**

The development is proposed on Lot 8008 Lyon Road and Lot 9034 Honeywood Avenue, Wandi (the subject site). The pavilion is proposed to be on the south western portion of the existing playing fields, at the end of Litoria Drive. The subject site is currently occupied by existing sporting grounds which abut Honeywood Primary School. A lot on the western side of the subject site is proposed to be developed as the car park for the facility. To the south is a playground and single dwellings. The zoning for the site is as follows:

- **Lot 9034 Honeywood Avenue**

The car park for the pavilion is proposed to be located on Lot 9034 Honeywood Avenue. The site is zoned Development under Local Planning Scheme No. 2 (LPS2) and Urban under the Metropolitan Region Scheme (MRS). The site is also affected by the Wandi North Local Structure Plan (LSP), which designates the site as Parks, Recreation and Drainage.

- **Lot 8008 Lyon Road**

The pavilion will be located on Lot 8008 Lyon Road, which is currently occupied by multi use sporting fields. The site is zoned Rural-Water Protection under the Metropolitan Region Scheme (MRS) and Special Rural under the City's Local Planning Scheme No. 3 (LPS2). The site is not zoned by the LSP, however it does identify the site as being district open space (local playing fields).

- **Road reserve**

Currently a road reserve runs through the site, from Litoria Drive to Lyon Road. The road reserve is not currently used as a road and forms part of the oval. The proposed location for the building straddles the road reserve and Lot 8008, a freehold lot (see figure 1 below). This creates a complication under the Building Codes as buildings are generally not approved over two lots.

A recommendation was adopted by Council on 10 February 2021 to vest the playing fields as a reserve for recreation, converting the road reserve to a recreation reserve and amalgamating the two parcels of land. This process will occur concurrently with the approvals and construction period, ensuring that the lots are amalgamated before occupation of the facility.

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**



Figure 1 – Site plan

### Community Infrastructure Plan

The City's Community Infrastructure Plan 2011-2031 sets out the City's priorities to provide community infrastructure until 2031. The CIP has long identified the Wandí Playing Fields and associated pavilion as part of its forward planning. These facilities are also included and funded by the City's developer contribution scheme (Development Contribution Plans 5 and 9). They will form a valuable service to the Wandí community with the playing fields also serving the primary school.

### External referral comments

- Department of Water and Environmental Regulation  
The application was referred to the Department of Water and Environment Regulation (DWER) as the site is located within the Priority 2 public drinking water source area (PDWSA) of the Jandakot Underground Water Pollution Control Area (UWPCA) and also resides within a wellhead protection zone (WHPZ). The advice provided by DWER is as follows:
  - In accordance with Water Quality Protection Note No. 25 a "recreation oval" within the Jandakot UWPCA is considered "compatible with conditions". As such the proposed development of a clubroom premises is directly associated with the primary, and previously approved, land use of an oval.
  - Onsite wastewater disposal within the PDWSA is not proposed, with connection to sewer confirmed.
  - The associated carpark, that has the potential to impact ground water resources as a result of hydrocarbon export, has been located outside of the PDWSA and WHPZ.

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

DWER has therefore supported the proposal, subject to the following conditions of approval:

- During the construction phase no chemicals or fuels should be kept on site within the PDWSA.
- Once constructed any chemicals kept on the premises should reside within a bunded storage area.
- It is recommended that the opportunity be sought for best practice water sensitive urban design within the carpark design to utilise garden bed areas to manage minor rainfall event runoff (first 15mm) for water quality treatment and reduce drainage infrastructure requirements.

Conditions of approval have been recommended in accordance with DWER advice.

**Internal referral comments**

- **Engineering**

The City's engineers have reviewed the proposal and support the application subject to conditions relating to the following:

- Crossovers being constructed;
- The car park to be drained and sealed;
- Stormwater drainage to consider Water Sensitive Urban Design;
- The installation of a dual use footpath on the northern side of Litoria Drive to connect with the existing dual use path on Honeywood Avenue; and
- Remove on street car bays where there is a clash with the crossover.

- **Environment**

The local area is a habitat for the Quenda. The following comments are provided in relation to the development:

- The City has been working with the community for around a year to support the local Quenda population that occur in the adjacent Darling Chase reserve.
- Evidence and advice has been provided that Quenda frequently cross Honeywood Avenue to the oval and remnant bushland and there have been significant fatalities on Honeywood Avenue.
- It is suspected that these fauna are foraging for larvae and other invertebrates on the oval, which is common practice for Quenda.
- To assist in creating a fauna corridor to facilitate safer movement of Quenda the landscape plan should incorporate low, dense endemic herbs and shrubs within the median of Honeywood Avenue, carpark swales and associated road reserves and carparks.

Advice and conditions are also recommended, to advise of further legislative obligations under environmental health and building legislation. Specifically:

- A condition to require the development be connected to sewer.
- A condition to require a noise management plan be provided to ensure any noise levels generated are kept within prescribed levels.
- Advice regarding obligations for a food premises; and
- Advice regarding obligations under the Building Act.

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

**Neighbour notification**

The proposed development was advertised via letters to owners and occupiers within 200m of the subject site. After the conclusion of the fourteen day period, one (1) submission was received. The summarised submitters issues and officer response is included below:

Submission	Officer response
Unacceptable to have a venue like this so close to homes.	The facility was considered in the design of the area and has been reviewed having regard to the amenity of the site in the report below.
Parking is not dealt with currently.	A new, large car park is part of this project.
If alcohol is on the premises, it will be a problem.	A permanent licence is not included in this proposal.
Noise needs to be considered.	An acoustic report is recommended to be provided to ensure the facility complies with noise regulations.
Traffic already bad with primary school.	Parking and traffic is managed sufficiently and the design has involved the City's engineers.
Do not want to see it go ahead.	Noted.

**Planning assessment**

• Zoning and land use permissibility

The lot where the car park for the facility is proposed is reserved as Parks, Recreation and Drainage under the LSP. The installation of a public carpark on this site is consistent with the intent of the reserve, noting that in addition to being available for users of the new building, it will be available for visitors to the existing playing fields and park.

The proposed facility is located on a Special Rural Zone (SR16) which is designated by LPS2 and sits just outside of the LSP. The facility is not easily classified into a land use that is listed in the scheme's zoning table. The variety of activities and events that will occur on site through permanent and occasional hiring, as well as the mixture of public use and private club use makes it difficult to classify. The City's City Engagement team has confirmed that the following will occur on site:

- Facilities and storage for sport clubs
- Social, recreational, cultural and learning activities
- Programs, workshops and services for the local community
- Fitness classes
- Community gatherings

Given the multipurpose nature of the facility, it is appropriate to classify it as a use not specified as it doesn't clearly fit in to any one land use. Further information on the assessment of the use not specified is included below.

In addition to being zoned Special Rural under the LPS2, under the Metropolitan Region Scheme (MRS) the site is zoned Rural-water protection. This is due to the area falling within the Jandakot UWPCA. The proposal is assessed against SPP2.3 Jandakot Groundwater Protection below. Advice has been sought from DWER, who have supported the use in this location.



17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI

- Use not specified

Where a land use cannot reasonably be determined as falling within a use listed in the LPS2 zoning table, Council is able to approve an unlisted use, provided that advertising is undertaken and the use is consistent with the objectives and purposes of the zone. The application has been advertised to neighbouring owners and occupiers, and additional consultation was undertaken in the months preceding lodgement.

The provisions for SR16, detail limitations on livestock, provision of effluent disposal systems and restrictions on further clearing. Essentially, the provisions are built around protecting the groundwater resource. The proposed development is considered to meet the requirements of SR16 as a use not specified, for the following reasons:

- There is no intensification of residential development or subdivision proposed. Rather, the lots are being amalgamated to protect the use as a public facility.
- There is no livestock proposed.
- No groundwater is proposed to be extracted.
- DWER has reviewed the proposal and intended activities and is supportive subject to conditions.
- Stormwater and drainage, specifically for the carpark, is designed with best practice water sensitive urban design in mind.
- The development will be connected to reticulated sewer, meaning no onsite effluent disposal that has the potential to contaminate groundwater.
- The land is already cleared for the purposes of a recreational facility.

- Jandakot Groundwater Protection

The key objectives of the policy are to protect one of the key drinking water sources for Perth. The policy outlines principles to ensure that development is compatible with the long term protection of groundwater. The subject site is located within a P2 area, which means that land use acceptability is based on minimising risks. Supporting Water Quality Protection Notes 25 (WQPN 25) elaborates on which uses are acceptable where and provides guidance for approval agencies in assessing new development proposals.

In reviewing the application, DWER has concluded that subject to minor management conditions, the proposed development is acceptable as it is consistent with the existing playing fields, there is no onsite wastewater disposal proposed (i.e. it is connected to sewer which restricts the potential for contamination) and the carpark associated with the facility is located outside of the protection area.

- Matters to be considered

Clause 67 of the *Planning and Development (Local Planning Schemes) Regulations 2015* (the Regulations) outlines general matters that should be given due regard in the assessment of planning applications. In reviewing this application, the following comments are made:

- While a use not specified, the facility is considered to be appropriate for its setting at existing playing fields next door to a primary school.
- The development does not jeopardise the existing groundwater resource and meets the intent of the relevant state planning policy. Additionally, the carpark for the site, which has the potential for the greatest risk to ground water due to stormwater runoff, is located outside of the water mound.
- The site sits outside of the bushfire prone area.
- There is no clearing proposed.

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

- The building is designed to look out across the playing fields, protecting the privacy of nearby residents.
- The use of the building will provide a significant community benefit by encouraging the establishment of sporting clubs, creating a meeting place for residents and opportunities for engagement between groups.
- There are 168 car parking bays proposed to be installed for the facility, noting that it will also be available for users of the existing playground and playing fields.
- The carpark has been designed for two entry and exit points to assist with traffic flow at the conclusion of an event when the carpark may be full.

**LEGAL/POLICY IMPLICATIONS:**

For the purpose of Councillors considering a financial or impartiality interest only, the owner and developer of the site is the City of Kwinana.

The following legislation is applicable to this item:

- *Planning and Development (Local Planning Schemes) Regulations 2015*
- City of Kwinana Local Planning Scheme No. 2
- SPP 2.3 Jandakot Groundwater Protection
- Wandi North Local Structure Plan

**FINANCIAL/BUDGET IMPLICATIONS:**

Nil. Any budget implications for this project are not related to the development application.

**ASSET MANAGEMENT IMPLICATIONS:**

The facility will be managed by the City.

**ENVIRONMENTAL IMPLICATIONS:**

The development's potential impact on the drinking water source are discussed throughout the report.

**STRATEGIC/SOCIAL IMPLICATIONS:**

This proposal will support the achievement of the following outcomes and objectives detailed in the Strategic Community Plan and Corporate Business Plan.

Plan	Outcome	Objective
Strategic Community Plan	A unique identity	1.4 A healthy and active community with services for everyone's needs

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

Strategic Community Plan	A beautiful environment	3.1 Improve conservation of biodiversity and protection of native vegetation whilst achieving high levels of environmental protection in new developments
--------------------------	-------------------------	---

**COMMUNITY ENGAGEMENT:**

Community Engagement has taken place in the following forms prior to the application being submitted:

- The City has undertaken various forms of community engagement in order to assess and meet the local community's needs through the provision of the Honeywood Community Pavilion. In August 2020, the City conducted engagement to update the community on the progress of the project and seek feedback on the proposed design. Of 132 surveys received from the community, 64% rated the design as either 'Excellent' or 'Good', with the average rating being 3.6/5. Online commentary and conversations with other residents and groups has also been positive.
- It was through this most recent engagement the City received a petition which was at odds with the majority of community feedback. This was received and noted by Council at the time. City Officers have been working with the petitioner to resolve some outstanding concerns unrelated to the design of the pavilion.

In addition to the above consultation which was undertaken prior to lodgement of the application, the development application was advertised via letter to owners and occupiers within a 200m radius of the site. One submission objecting to the proposal was received.

**PUBLIC HEALTH IMPLICATIONS:**

The recommendation/proposal has the potential to help improve the following determinants of health -

- Built Environment – Environmental Quality; Neighbourhood Amenity; Disease Prevention;
- Health Behaviours –Diet and Exercise; Participation
- Socio-economic Factors – Education; Family and Social Support; Community Safety

**RISK IMPLICATIONS:**

The risk implications in relation to this proposal are as follows:

Risk Event	Local community is not fully notified of proposed development.
Risk Theme	Inadequate engagement practices Inadequate environmental management

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

Risk Effect/Impact	Service Delivery Environment
Risk Assessment Context	Project
Consequence	Moderate
Likelihood	Possible
Rating (before treatment)	Low
Risk Treatment in place	Reduce - mitigate risk
Response to risk treatment required/in place	Significant consultation held with the community about the facility and its purpose, and full consideration about the environmental management of the facility on the Jandakot Groundwater Mound and impact on Quenda population.
Rating (after treatment)	Low

**COUNCIL DECISION**

387

**MOVED CR S LEE**

**SECONDED MAYOR C ADAMS**

**That Council approve the development application for the Use Not Specified (Honeywood community pavilion) (Attachment A) at Lot 8008 Lyon Road and Lot 9034 Honeywood Avenue, Wandi subject to the following conditions in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*:**

1. Pursuant to clause 26 of the Metropolitan Region Scheme (MRS), this approval is deemed to be an approval under clause 24 (1) of the MRS.
2. Prior to the lodgement of a building permit application, a Noise Management Plan (NMP) prepared by a suitably qualified acoustic consultant shall be submitted to the City of Kwinana for approval, detailing measures that will be undertaken to ensure noise levels generated by the development are kept within levels prescribed in the *Environmental Protection (Noise) Regulations 1997*.
3. Prior to occupation of the development, the facility shall be connected to sewer in accordance with the requirements of the Water Corporation to the satisfaction of the City of Kwinana.
4. Prior to occupation of the development, vehicle crossovers shall be constructed of concrete and located to the specifications and satisfaction of the City of Kwinana.

**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

- 5. Prior to the completion of crossover works, the applicant shall remove the existing 4 on-street parking bays on Honeywood Avenue through which the proposed car park crossover extends. The applicant will extend the crossover to the carriageway edge, reinstating the verge and landscaping to tie in with existing, to the specification and satisfaction of the City of Kwinana.**
- 6. Car parking areas are to be sealed and drained as per the City of Kwinana Trafficable Areas specification to the satisfaction of the City of Kwinana.**
- 7. Stormwater drainage from roofed and paved areas being contained and disposed of on site at all times, incorporating Water Sensitive Urban Design principles to utilise garden bed and landscape areas for water quality treatment and infiltration, to the satisfaction of the City of Kwinana.**
- 8. Prior to occupation of the development, a dual use footpath shall be installed along the north side of Litoria Drive to a minimum width of 2.5m, linking the existing Honeywood Avenue dual use path to the facility, to the specification and satisfaction of the City of Kwinana.**
- 9. Prior to occupation, the landscaping plan (dated 13 July 2020 as amended), shall be implemented to the satisfaction of the City of Kwinana.**
- 10. During construction, no chemicals or fuels should be kept on site within the Public Drinking Water Source Area on advice from the Department of Water and Environmental Regulation, to the satisfaction of the City of Kwinana.**
- 11. No chemicals shall be kept on the premises, unless residing within a bunded storage area as advised by the Department of Water and Environmental Regulation, to the satisfaction of the City of Kwinana.**

**Advice notes:**

- i. If the development the subject of this approval is not substantially commenced within a period of two (2) years, or another period specified in the approval after the date of the determination, the approval will lapse and be of no further effect.**
- ii. The Minister for Planning has issued a formal notice extending the deadline for substantial commencement by an additional two (2) years for all applications approved during the current State of Emergency. In effect, this means that the timeframe for substantial commencement is now four (4) years from the date of this determination.**
- iii. The occupant shall submit an application for notification/registration of a food premises to the City of Kwinana for approval prior to building occupancy. The food business shall comply with *Food Act 2008*, *Food Regulations 2009* and *Australian New Zealand Food Standards Code*.**

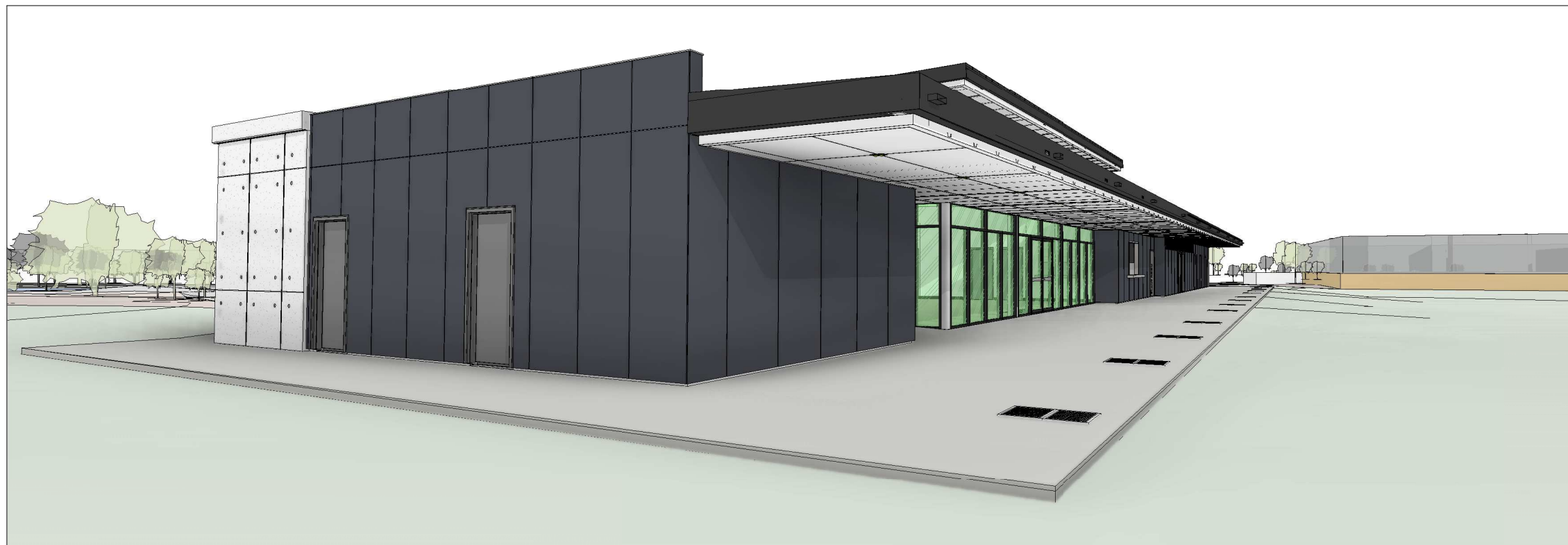
**17.3 DEVELOPMENT APPLICATION – HONEYWOOD COMMUNITY PAVILION – LOT 8008 LYON ROAD AND LOT 9034 HONEYWOOD AVENUE, WANDI**

- iv. The carpark design shall incorporate best practice water sensitive urban design to utilise garden beds to manage minor rainfall event runoff (first 15mm) for water quality treatment and reduce drainage requirements.**
- v. To assist in creating a fauna corridor to facilitate safer movement of Quenda, it is recommended that the landscape plan incorporate low, dense, endemic herbs and shrubs within the median of Honeywood Avenue, carpark swales and associated road reserves and carparks.**
- vi. If an applicant or owner is aggrieved by this determination there is a right of review by the State Administrative Tribunal in accordance with the Planning and Development Act 2005 Part 14. An application must be made within 28 days of the determination.**
- vii. The applicant is advised that this conditional development approval is not a building permit giving authority to commence construction. Prior to any building work commencing on site a Building Permit must be issued and penalties apply for failing to adhere to this requirement.**

**CARRIED  
4/2**

# HONEYWOOD COMMUNITY PAVILION

DEVELOPMENT APPLICATION ISSUE



NORTH-EASTERN VIEW



SOUTH-EASTERN VIEW

**NOTE:**  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION WITH ARCHITECTURAL SPECIFICATION.

d	20 10 12	DEVELOPMENT APPLICATION ISSUE
c	20 08 04	ISSUED TO CONSULTANTS
b	20 07 01	ISSUED PRELIMINARY COSTING
a	20 02 16	SCHEMATIC DESIGN ISSUE

No. DATE AMENDMENT

**donovan payne**

HONEYWOOD PAVILION

CITY OF KWINANA

TITLE  
COVER SHEET

BUILDER MUST CHECK ALL DIMENSIONS, HEIGHTS, AND LEVELS ON SITE BEFORE COMMENCING ANY WORK.  
DIMENSIONS TO GOVERN SCALE.  
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SCALE	DWG NUMBER	REV.
DRAWN BGW	A0.1	d
CHECKED CP		
JOB NO. 2003	DATE	

20/10/12



OVAL VIEW



ENTRANCE VIEW

NOTE:  
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a	20 02 16	SCHEMATIC DESIGN ISSUE

No. DATE AMENDMENT



HONEYWOOD PAVILION

CITY OF KWINANA

TITLE  
IMAGERY SHEET CONT.

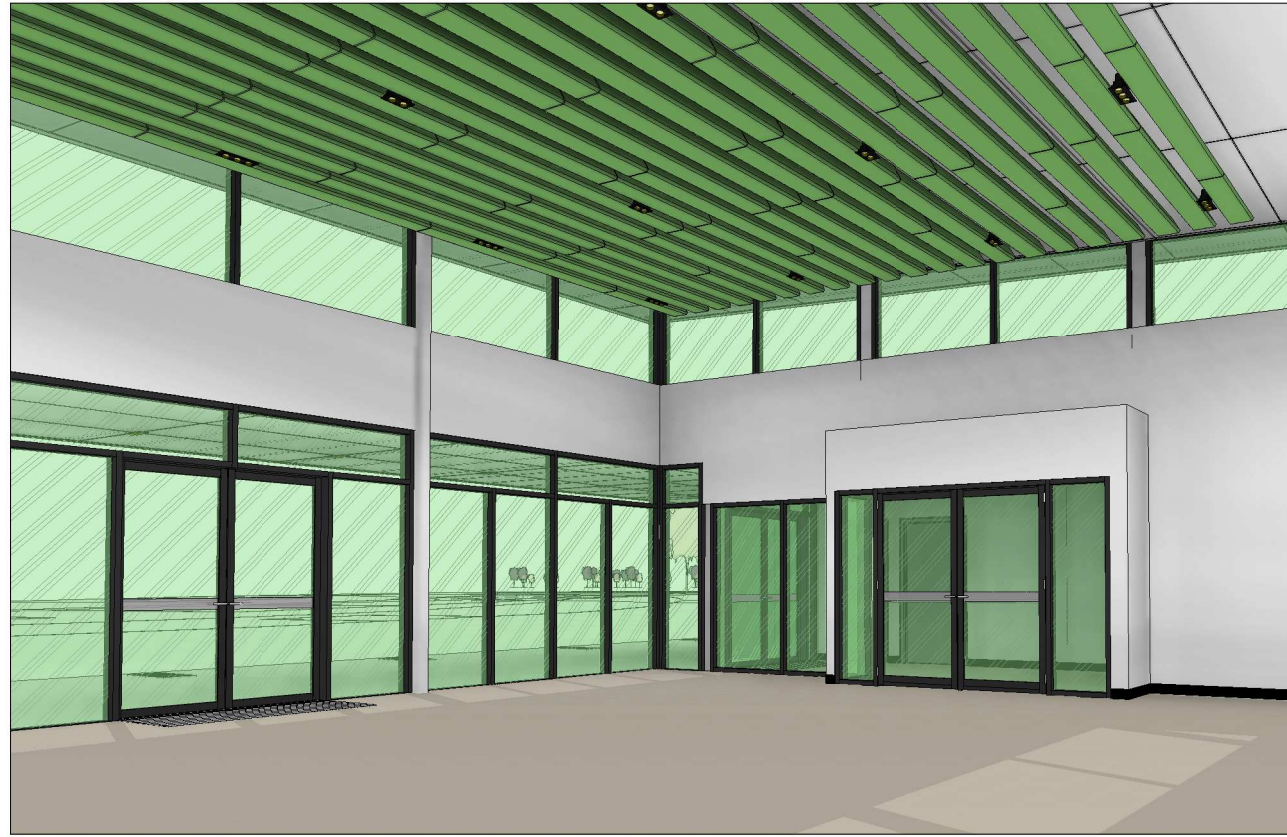
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SCALE	DWG NUMBER	REV.
DRAWN BGW	A0.2	d
CHECKED CP		
JOB NO. 2003	PLOT DATE	19/08/18

PLEASE REFER TO  
CHECK PRINT SCALE





FUNCTION ROOM VIEW



FOYER VIEW



CHANGE ROOM VIEW



KITCHEN VIEW

NOTE:  
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No. DATE AMENDMENT



HONEYWOOD PAVILION

CITY OF KWINANA

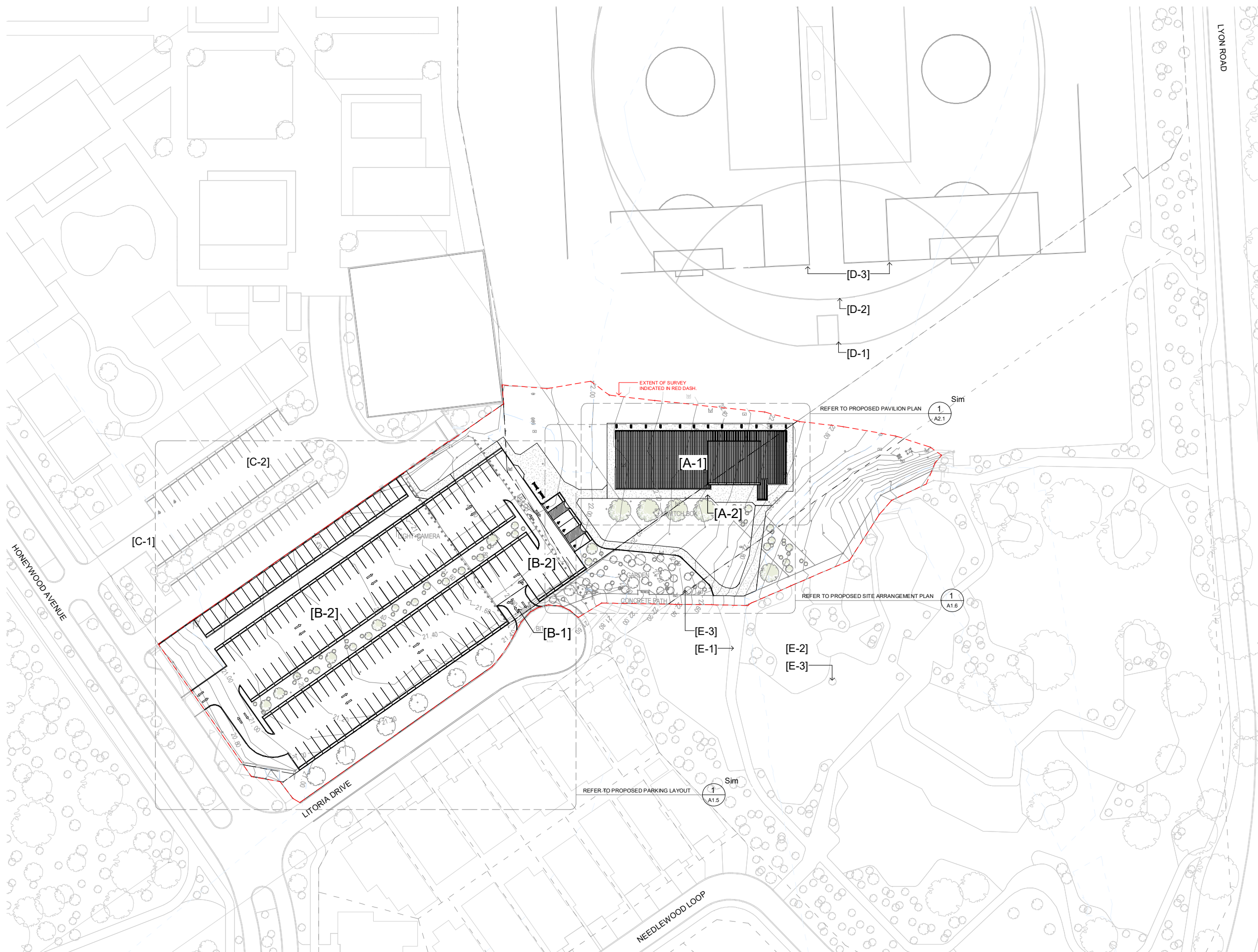
TITLE

IMAGERY SHEET CONT.

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SCALE	DWG NUMBER	REV.
DRAWN BGW	A0.3	d
CHECKED CP		
JOB NO. 2003	PLOT DATE	19/08/2013

PLEASE REFER TO  
CHECK PRINT SCALE



**LOCATION KEY LEGEND**

- [A-1] HONEYWOOD COMMUNITY PAVILION
- [A-2] PAVILION USER CONCOURSE
- [B-1] PAVILION VEHICLE PARKING ACCESS
- [B-2] PAVILION PRIMARY PARKING
- [C-1] EXISTING SCHOOL ACCESS
- [C-2] EXISTING SCHOOL PARKING
- [D-1] EXISTING AFL FIELD ALIGNMENT
- [D-2] EXISTING CRICKET FIELD ALIGNMENT
- [D-3] EXISTING SOCCER FIELD ALIGNMENT
- [E-1] EXISTING PARKLAND PATH
- [E-2] EXISTING PARKLAND RECREATION
- [E-3] EXISTING PARKLAND VEGETATION

**NOTE:**  
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e	20 10 12	DEVELOPMENT APPLICATION ISSUE
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b	20 07 01	ISSUED PRELIMINARY COSTING
a	20 06 16	SCHEMATIC DESIGN ISSUE

No.	DATE	AMENDMENT
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**HONEYWOOD PAVILION**

CITY OF KWINANA

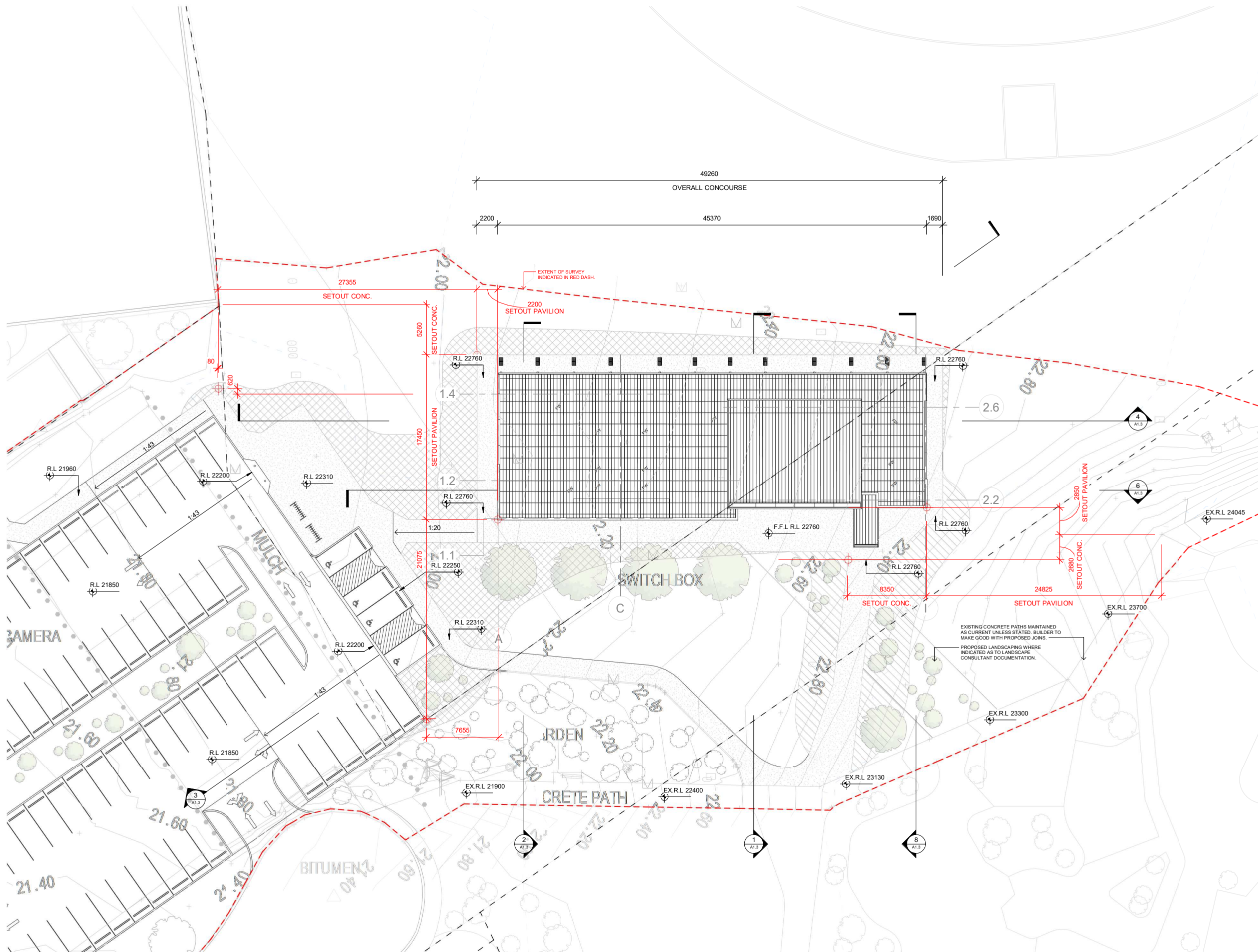
**LOCATION PLAN**

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SCALE	As indicated	DWG NUMBER	REV.
DRAWN	BGW	A1.1	e
CHECKED	CP		
JOB NO.	2003	DATE	2020/10/12

**1** LOCATION PLAN  
A1.3 1 : 500

EARTHWORKS KEY LEGEND



NOTE: REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION WITH ARCHITECTURAL SPECIFICATION.

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NO.	DATE	AMENDMENT

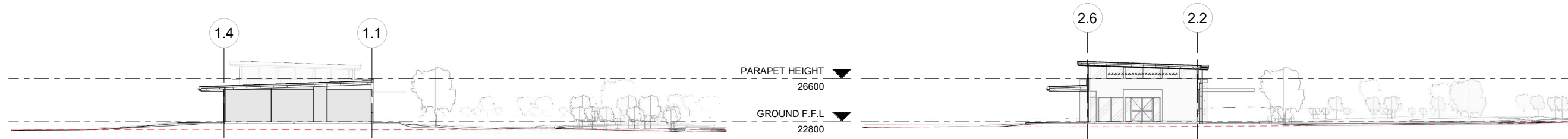


HONEYWOOD PAVILION  
CITY OF KWINANA  
TITLE  
SITE PLAN

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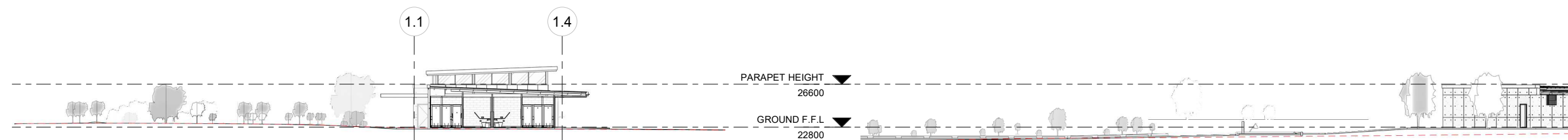
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DRAWN	BGW	A1.2	e
CHECKED	CP		
JOB NO.	2003	PLOT DATE	19/08/2016

1 SITE PLAN  
A1.3 1 : 200



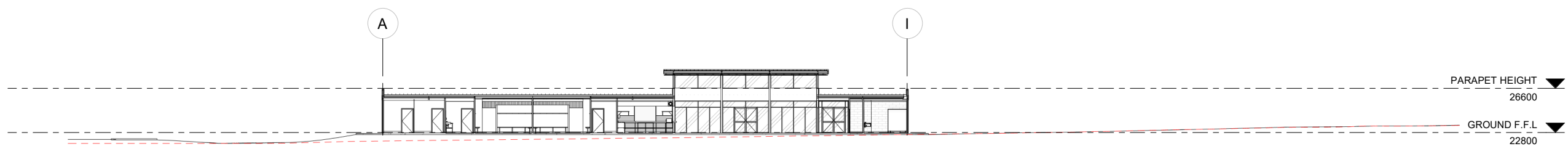
2 SITE SECTION A  
A1.2 1 : 200

1 SITE SECTION B  
A1.2 1 : 200

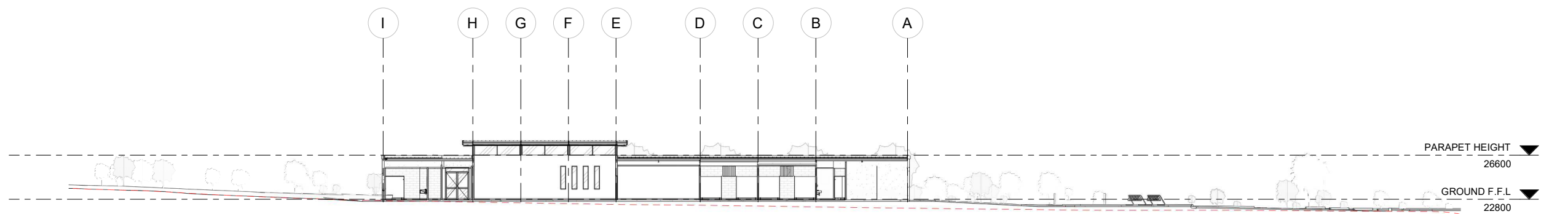


8 SITE SECTION C  
A1.2 1 : 200

3 SITE SECTION D  
A1.2 1 : 200



4 SITE SECTION E  
A1.2 1 : 200



6 SITE SECTION F  
A1.2 1 : 200



NOTE:  
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DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION  
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20 10 12 DEVELOPMENT APPLICATION ISSUE

No. DATE AMENDMENT



HONEYWOOD PAVILION

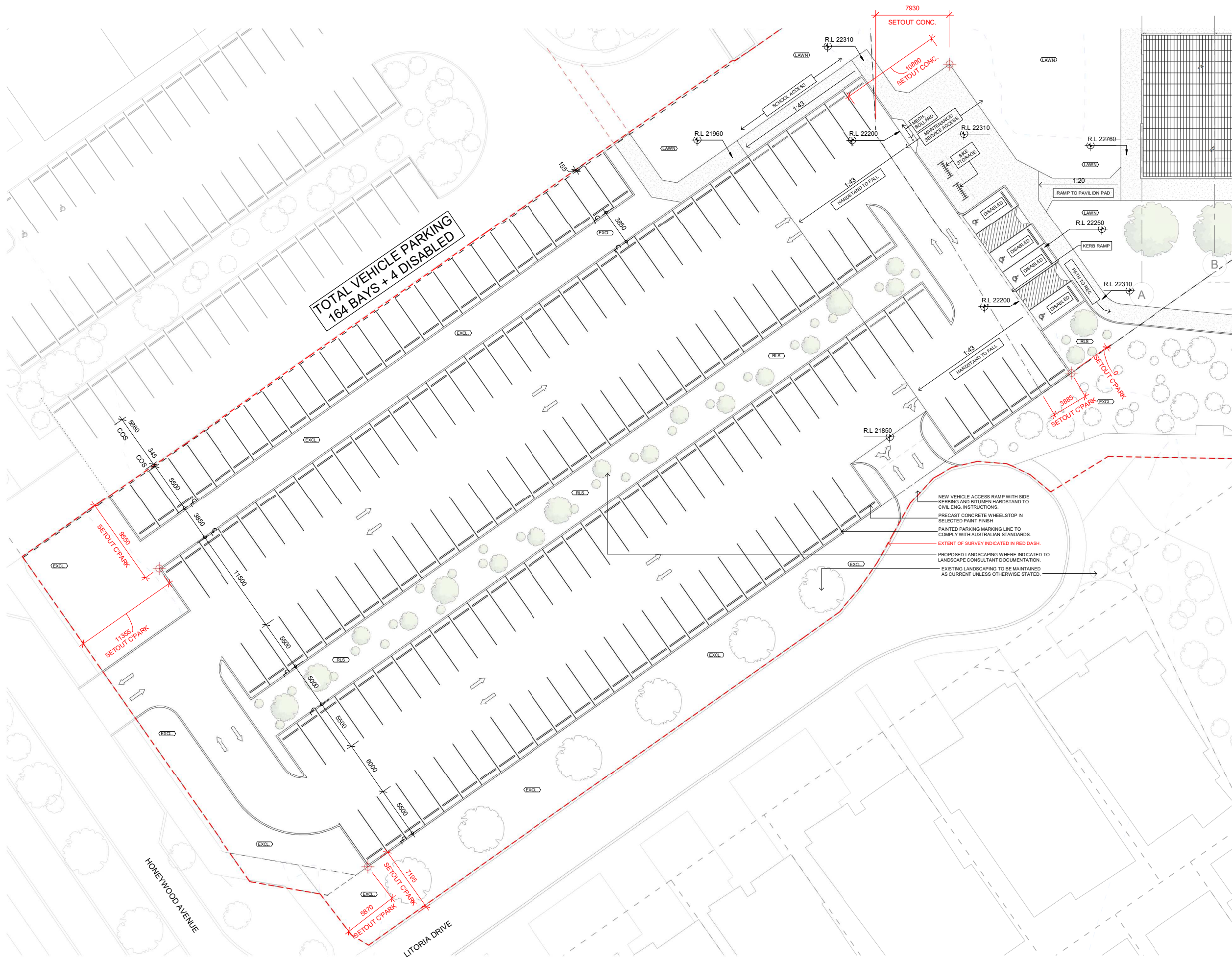
CITY OF KWINANA

TITLE  
SITE SECTIONS

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SCALE	1 : 200	DWG NUMBER	A1.3	REV.	a
DRAWN	BGW	CHECKED	CP		
JOB NO.	2003	PLOT DATE	9/20/2012 10:46 AM		

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**TOTAL VEHICLE PARKING  
164 BAYS + 4 DISABLED**

- NEW VEHICLE ACCESS RAMP WITH SIDE KERBING AND BITUMEN HARDSTAND TO CIVIL ENG. INSTRUCTIONS.
- PRECAST CONCRETE WHEELSTOP IN SELECTED PAINT FINISH
- PAINTED PARKING MARKING LINE TO COMPLY WITH AUSTRALIAN STANDARDS.
- EXTENT OF SURVEY INDICATED IN RED DASH.
- PROPOSED LANDSCAPING WHERE INDICATED TO LANDSCAPE CONSULTANT DOCUMENTATION.
- EXISTING LANDSCAPING TO BE MAINTAINED AS CURRENT UNLESS OTHERWISE STATED.

**1 SITE ARRANGEMENT - CARPARK LAYOUT**  
A1.1 1 : 200



**NOTE:**  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION WITH ARCHITECTURAL SPECIFICATION.

a	20 10 12	DEVELOPMENT APPLICATION ISSUE
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b	20 07 01	ISSUED PRELIMINARY COSTING
a	20 06 16	SCHEMATIC DESIGN ISSUE

No.	DATE	AMENDMENT
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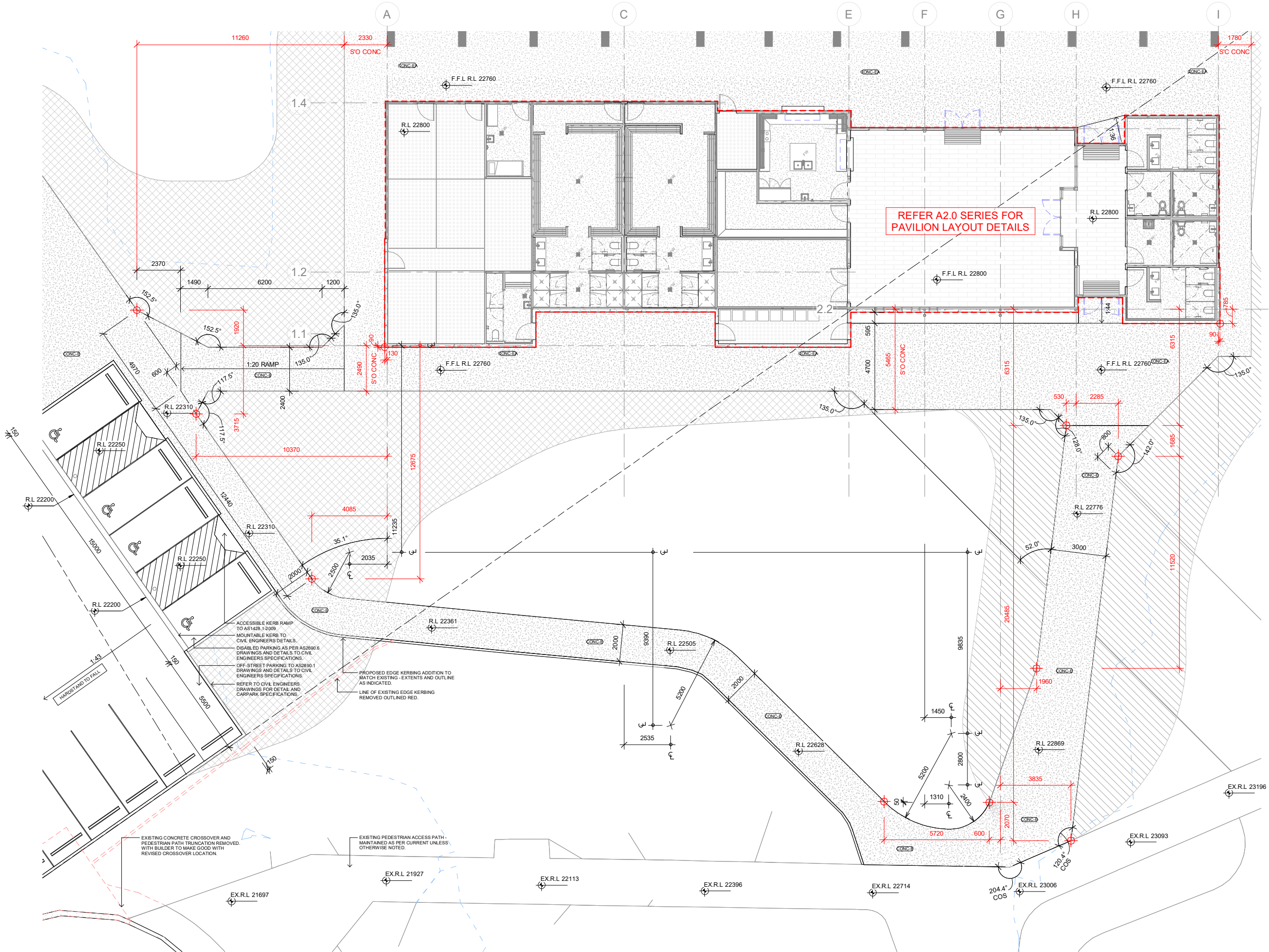


**HONEYWOOD PAVILION**

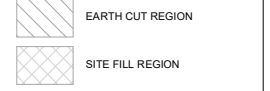
**CITY OF KWINANA**  
**TITLE**  
**PARKING LAYOUT**

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SCALE	1 : 200	DWG NUMBER	REV.
DRAWN	BGW	<b>A1.5</b>	<b>e</b>
CHECKED	CP		
JOB NO.	2003	PLOT DATE	19/08/2024



**EARTHWORKS KEY LEGEND**



NOTE:  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION WITH ARCHITECTURAL SPECIFICATION.

No. DATE AMENDMENT



**HONEYWOOD PAVILION**

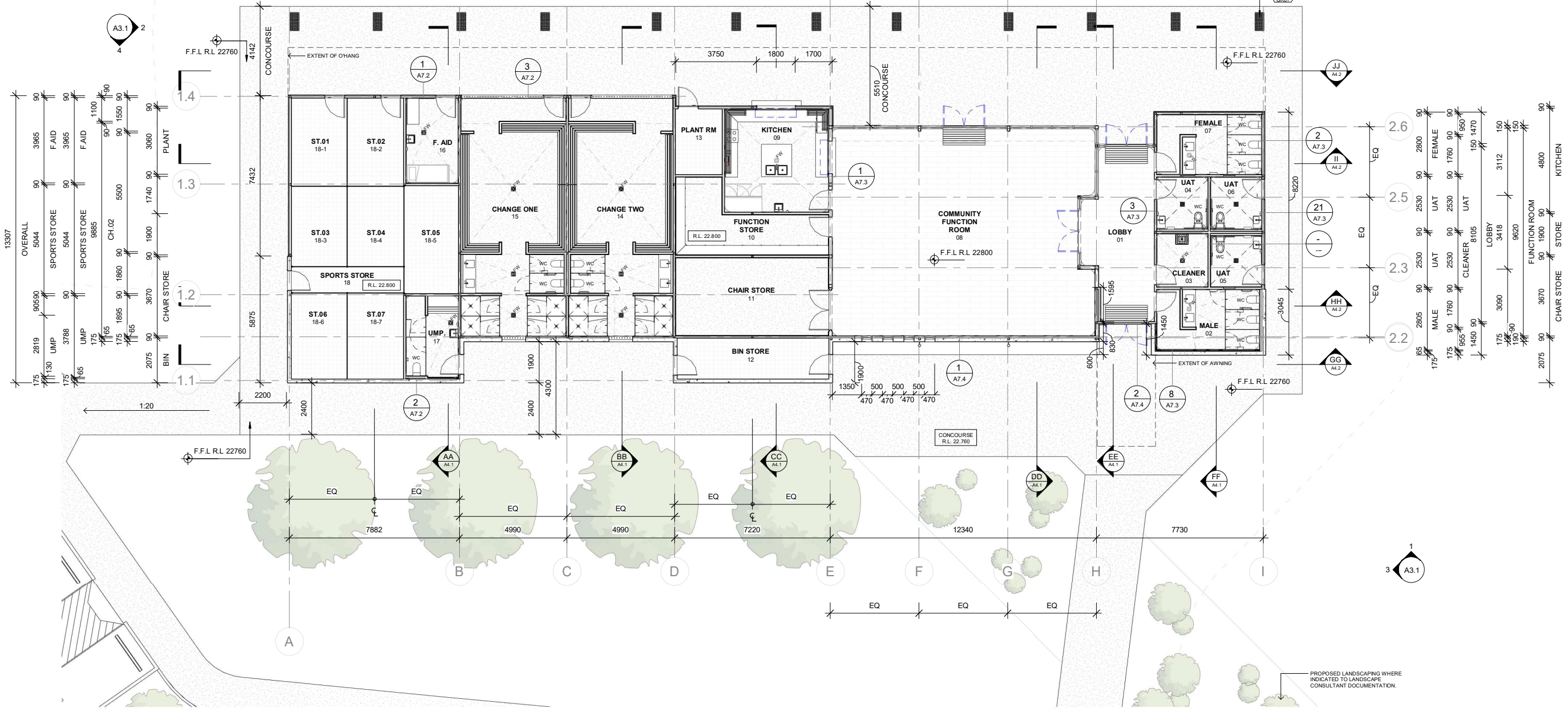
CITY OF KWINANA  
TITLE  
**PAVILION LAYOUT**

BUILDER MUST CHECK ALL DIMENSIONS, HEIGHTS AND LEVELS ON SITE BEFORE COMMENCING ANY WORK.  
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SCALE	DWG NUMBER	REV.
As indicated		
DRAWN Author	<b>A1.6</b>	
CHECKED Checker		
JOB NO. 2003	PLOT DATE	

**1 SITE ARRANGEMENT - PAVILION LAYOUT**  
A1.1 1 : 100

45202																											
OVERALL																											
17952				7250					12340		2600		5060														
90	5250	90	2452	90	1800	90	3010	90	3010	90	1800	90	2180	90	4830	150	12190	150	2600	90	1100	90	3690	90			
90	SPORTS STORE	7792	F.AID	90	4900	90	4900	90	4900	90	4900	90	PLANT	90	KITCHEN	7100	150	COMM. FUNCTION ROOM	11390	150	LOBBY	3400	90	2395	90	2395	90
175	SPORTS STORE	5250	90	2453	90	4900	90	4900	90	4900	90	4900	90	FUNCTION STORE	7100	150	COMM. FUNCTION ROOM	12190	150	LOBBY	2450	175	4790	175	UAT	90	
175	SPORTS STORE	5250	90	2453	90	4900	90	4900	90	4900	90	4900	90	CHAIR STORE	7100	150	COMM. FUNCTION ROOM	12190	150	LOBBY	2500	90	1100	90	MALE	90	
175	SPORTS STORE	5250	90	2453	175	CH.01	9745	CH.02	9745	90	4900	90	CHAIR STORE	7360	150	COMM. FUNCTION ROOM	12359	150	LOBBY	2406	90	1100	90	MALE	175		
175	SPORTS STORE	5250	90	2453	175	UMP. 65	9745	90	4900	90	4900	90	CHAIR STORE	7360	150	COMM. FUNCTION ROOM	12359	150	LOBBY	2406	90	1100	90	MALE	175		



**1** GROUND FLOOR LAYOUT  
A1.1 1 : 100

NOTE:  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION WITH ARCHITECTURAL SPECIFICATION.

REV.	DATE	DESCRIPTION
a	20 10 12	DEVELOPMENT APPLICATION ISSUE
b	20 08 04	ISSUED TO CONSULTANTS
c	20 07 30	ISSUED FOR REVIEW
d	20 07 01	ISSUED PRELIMINARY COSTING
e	20 06 18	SCHEMATIC DESIGN ISSUE

No. DATE AMENDMENT



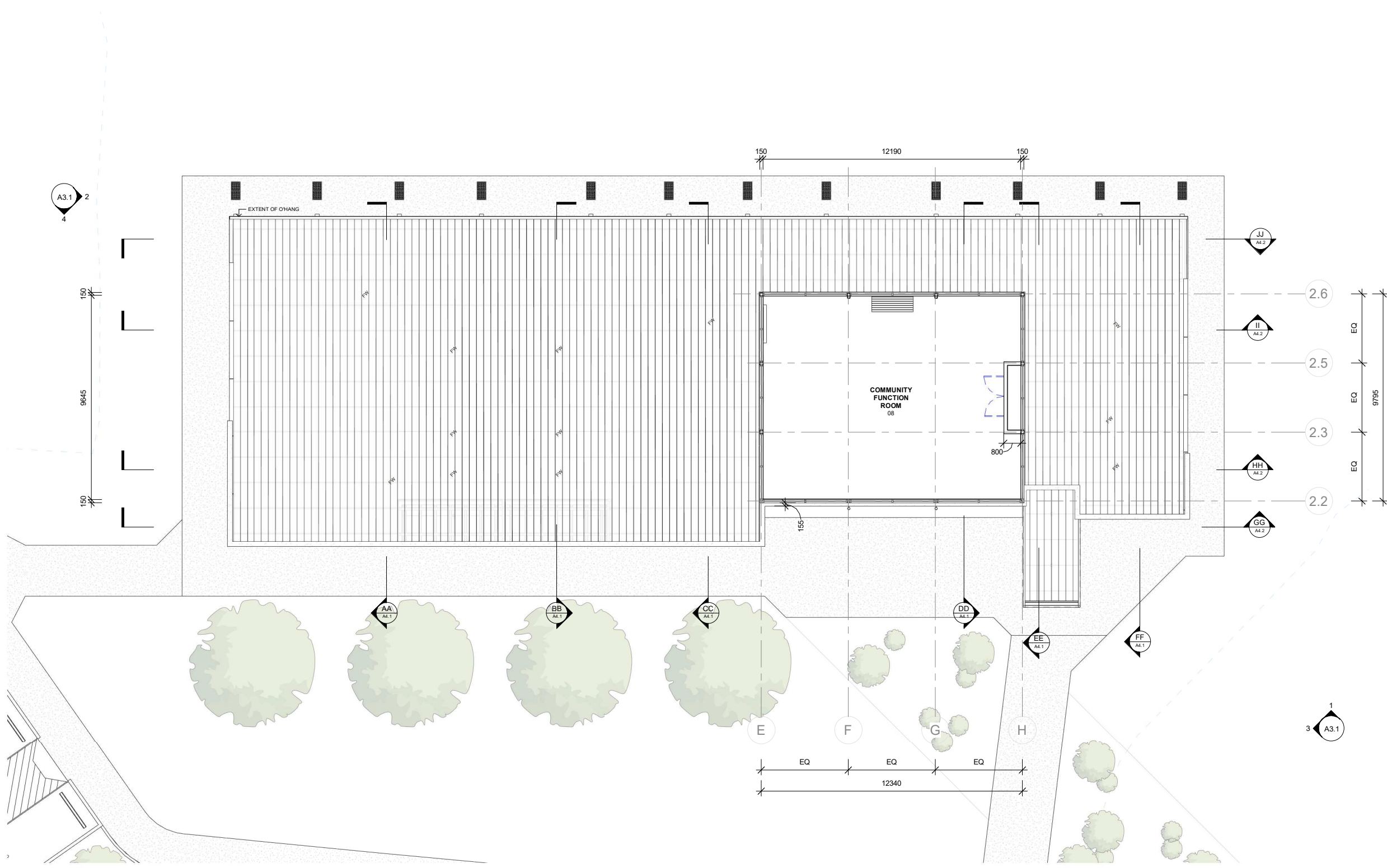
HONEYWOOD PAVILION

CITY OF KWINANA  
TITLE  
GROUND FLOOR PLAN

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SCALE	DWG NUMBER	REV.
1 : 100	A2.1	e
DRAWN BGW		
CHECKED CP		
JOB NO. 2003	DATE 18/01/20	

CHECK PRINT SCALE



1 GROUND UPPER LAYOUT  
A1.3 1 : 100

NOTE:  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER  
DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION  
WITH ARCHITECTURAL SPECIFICATION.

20 10 12 DEVELOPMENT APPLICATION ISSUE  
20 08 04 ISSUED TO CONSULTANTS

NO. DATE AMENDMENT

donovan payne

HONEYWOOD PAVILION

CITY OF KWINANA

TITLE  
FIRST FLOOR PLAN

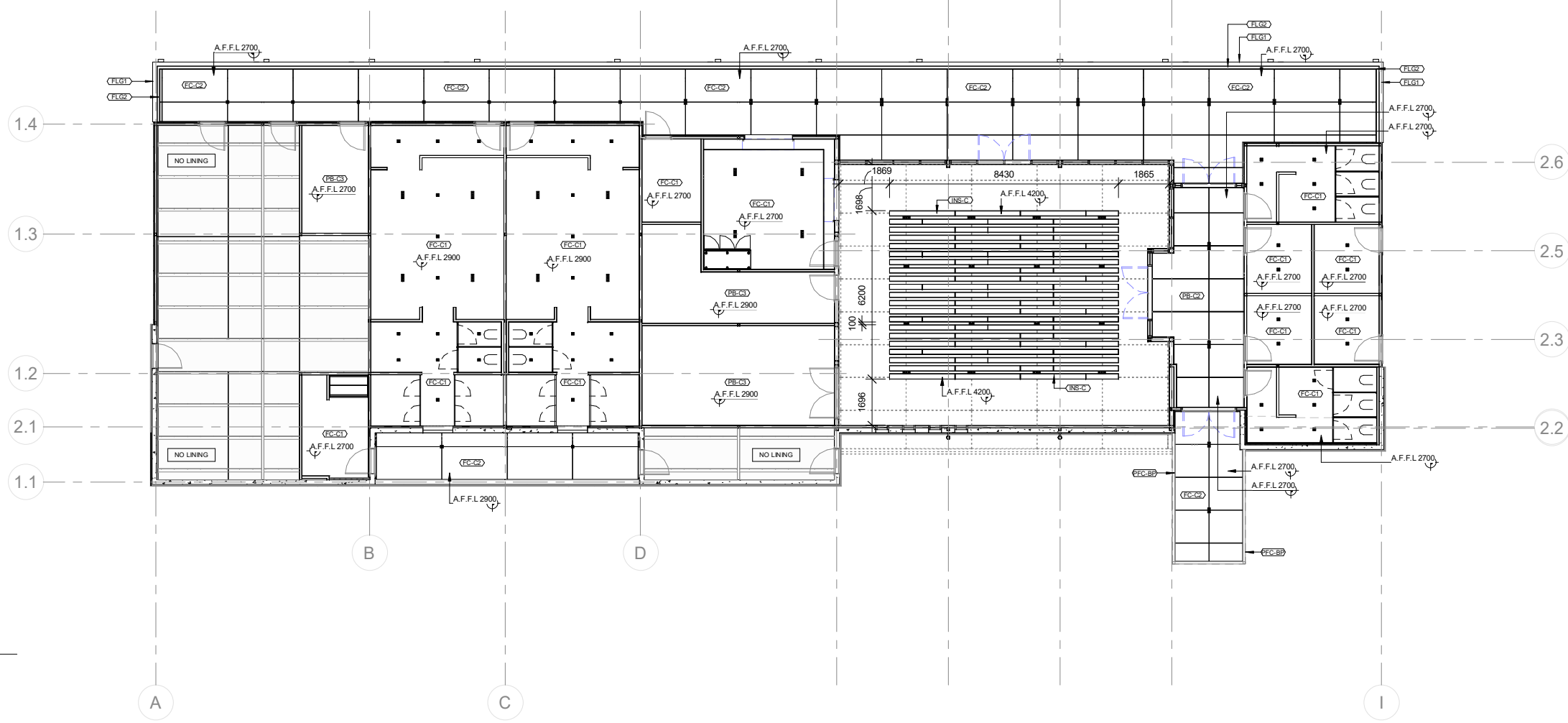
BUILDER MUST CHECK ALL DIMENSIONS, HEIGHTS, AND LEVELS  
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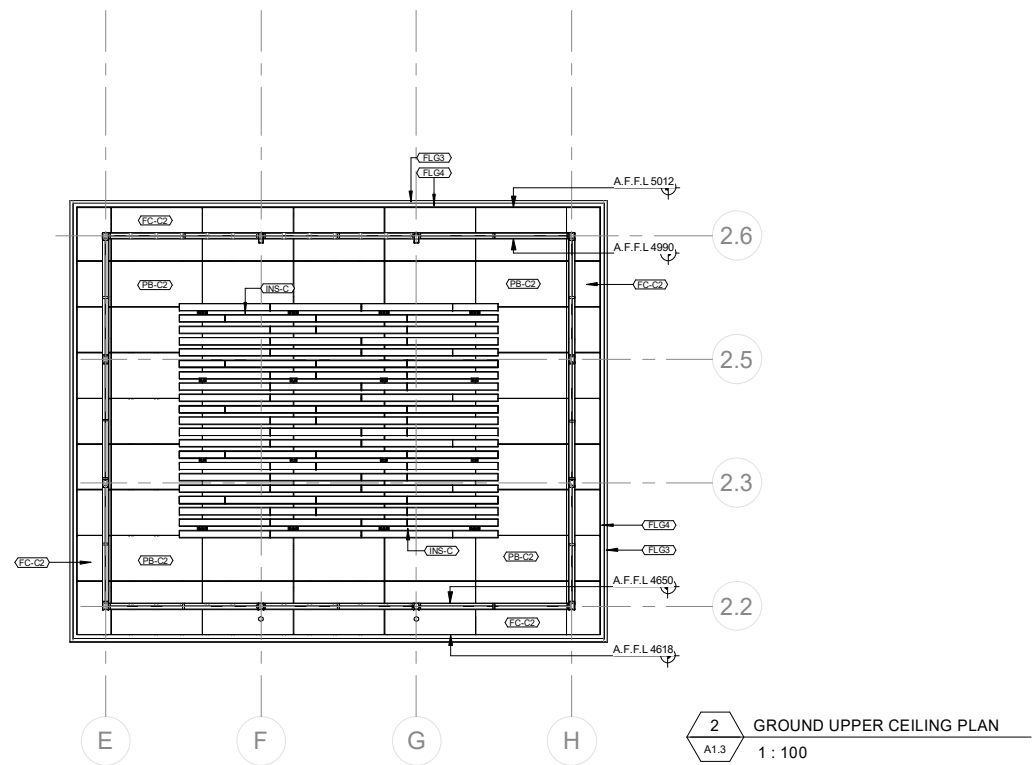
SCALE	1 : 100	DWG NUMBER	REV.
DRAWN	Author	A2.2	b
CHECKED	Checker		
JOB NO.	2003	PLOT DATE	20/10/12

CHECK PRINT SCALE





1 GROUND FLOOR CEILING PLAN  
A1.3 1 : 100



2 GROUND UPPER CEILING PLAN  
A1.3 1 : 100

CEILING LEGEND	
<i>(READ THIS SCHEDULE IN CONJUNCTION WITH ALL OTHER SCHEDULES ON THE A10 SERIES AND ALL OTHER DOCUMENTS)</i>	
FC-C1	9mm THICK VILLABOARD CEILING LINING FIXED TO FURRING CHANNELS. P51 SHADOWLINE COMBINATION SET BEAD. PAINT FINISH - WHITE.
FC-C2	9mm THICK JAMES HARDIE VERSILUX LINING. EXPRESS JOINTED WITH JAMES HARDIE BLACK BACKING GASKET. PAINT FINISH - DULUX VIVID WHITE.
INS-C	12mm AUTEX FRONTIER SUSPENDED ACOUSTIC CEILING RAFT TRAPEZOID. COLOUR AS AUTEX BOSCO.
PB-C2	13mm THICK GYPROCK EC08 COMPLETE PLASTERBOARD LINING. EXPRESS JOINTED WITH BLACK BACKING STRIP. PAINT FINISH - DULUX VIVID WHITE.
PB-C3	13mm THICK GYPROCK EC08 COMPLETE PLASTERBOARD LINING. FLUSH JOINTED. PAINT FINISH - DULUX VIVID WHITE.

FLASHING LEGEND	
<i>(READ THIS SCHEDULE IN CONJUNCTION WITH ALL OTHER SCHEDULES ON THE A10 SERIES AND ALL OTHER DOCUMENTS)</i>	
FLG1	PROFILED EDGE FASCIA IN COLORBOND MONUMENT WITH DOWNSPOUT POP TO RAIN-GARDEN BELOW.
FLG2	PROFILED EDGE FASCIA IN COLORBOND WINDSPRAY.
FLG3	PROFILED EDGE FASCIA IN COLORBOND MONUMENT.
FLG4	PROFILED EDGE FASCIA IN COLORBOND WINDSPRAY.

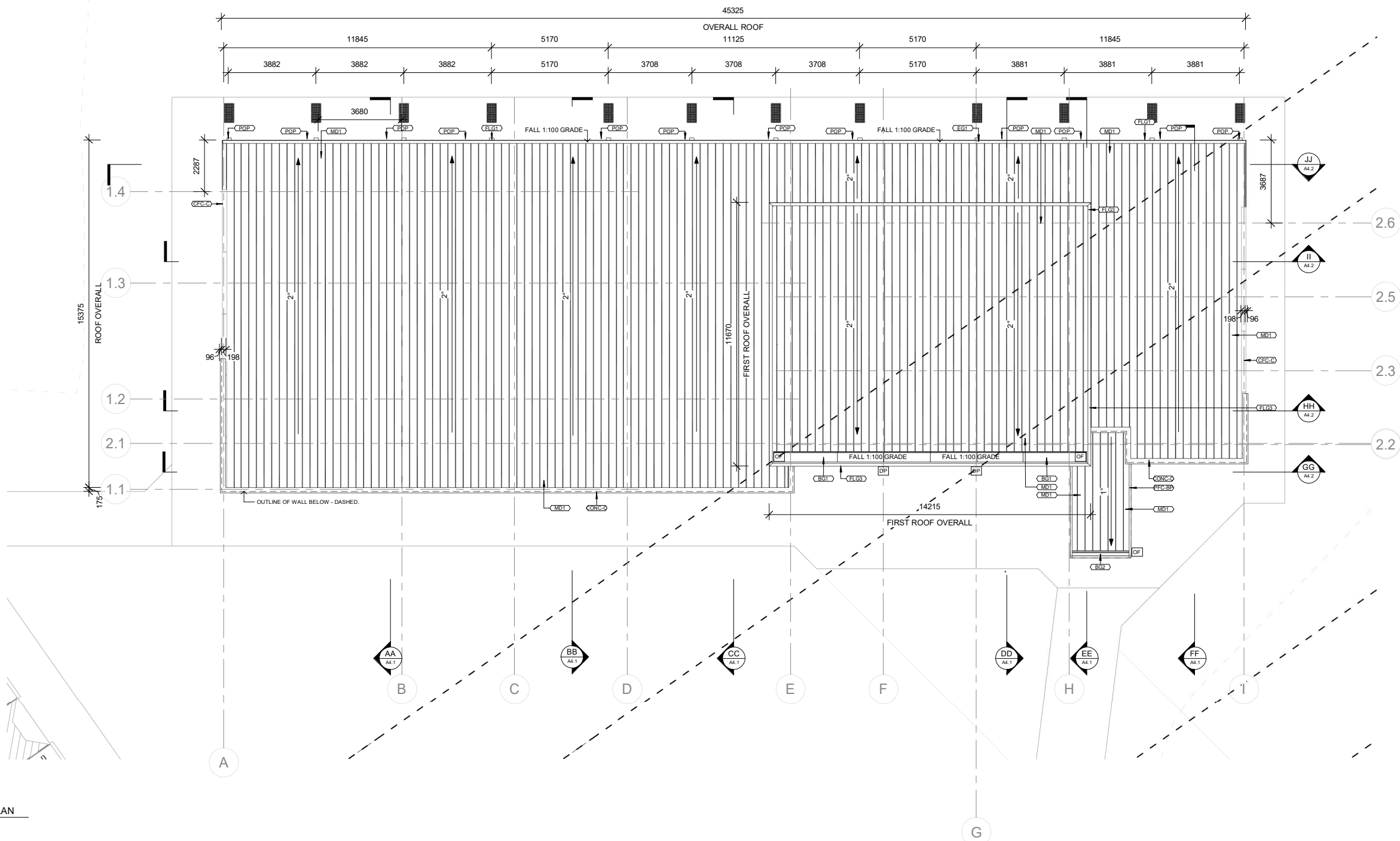
NOTE:  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION WITH ARCHITECTURAL SPECIFICATION.

REV.	DATE	DESCRIPTION
a	20 10 12	DEVELOPMENT APPLICATION ISSUE
d	20 08 04	ISSUED TO CONSULTANTS
c	20 07 30	ISSUED FOR REVIEW
b	20 07 01	ISSUED PRELIMINARY COSTING
a	20 06 16	SCHEMATIC DESIGN ISSUE



HONEYWOOD PAVILION  
CITY OF KWINANA  
TITLE  
CEILING PLAN

DRAWN BGW		A2.5	e
CHECKED CP			
SCALE 1 : 100	DWG NUMBER	REV.	
JOB NO. 2003	PLOT DATE	© 2003 DONOVAN PAYNE ARCHITECTS	



1 ROOF PLAN  
A1.3 1 : 100

**ROOFING LEGEND**  
(READ THIS SCHEDULE IN CONJUNCTION WITH ALL OTHER SCHEDULES ON THE A10 SERIES AND ALL OTHER DOCUMENTS)

BG1	600x100 DEEP 0.55BMT COLOURBOND SHEET BOX GUTTER. COLOUR TO MATCH ROOF SHEETING
BG2	300x100 DEEP 0.55BMT COLOURBOND SHEET BOX GUTTER. COLOUR TO MATCH ROOF SHEETING
EG1	200x200 DEEP 0.55BMT COLOURBOND SHEET EAVE GUTTER. COLOUR TO MATCH ROOF SHEETING
MD1	REVOLUTION ROOFING MAXLINE ROOF SHEETING PITCHED 2° TO MANUFACTURERS INSTRUCTIONS. COLORBOND FINISH - MONUMENT.
POP	PROFILED FASCIA IN COLORBOND MONUMENT WITH DOWNSPOUT POP TO RAIN-GARDEN BELOW.
RR1	BRADFORD ASHGRID 120mm GALV. ROOF INSULATION SPACERS INSTALLED TO MANUFACTURERS RECOMMENDATIONS AND INSTALLATION INSTRUCTIONS

**FLASHING LEGEND**  
(READ THIS SCHEDULE IN CONJUNCTION WITH ALL OTHER SCHEDULES ON THE A10 SERIES AND ALL OTHER DOCUMENTS)

FLG1	PROFILED EDGE FASCIA IN COLORBOND MONUMENT WITH DOWNSPOUT POP TO RAIN-GARDEN BELOW.
FLG2	PROFILED EDGE FASCIA IN COLORBOND WINDSPRAY.
FLG3	PROFILED EDGE FASCIA IN COLORBOND MONUMENT.
FLG4	PROFILED EDGE FASCIA IN COLORBOND WINDSPRAY.

**NOTE:**  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION WITH ARCHITECTURAL SPECIFICATION.

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d	20 08 04	ISSUED TO CONSULTANTS
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b	20 07 01	ISSUED PRELIMINARY COSTING
a	20 06 16	SCHEMATIC DESIGN ISSUE

NO.	DATE	AMENDMENT
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HONEYWOOD PAVILION

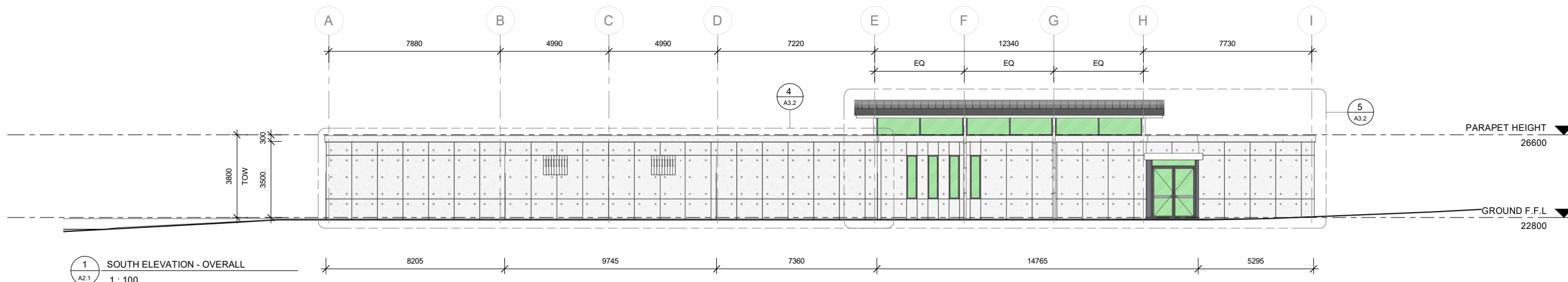
CITY OF KWINANA

TITLE  
ROOF PLAN

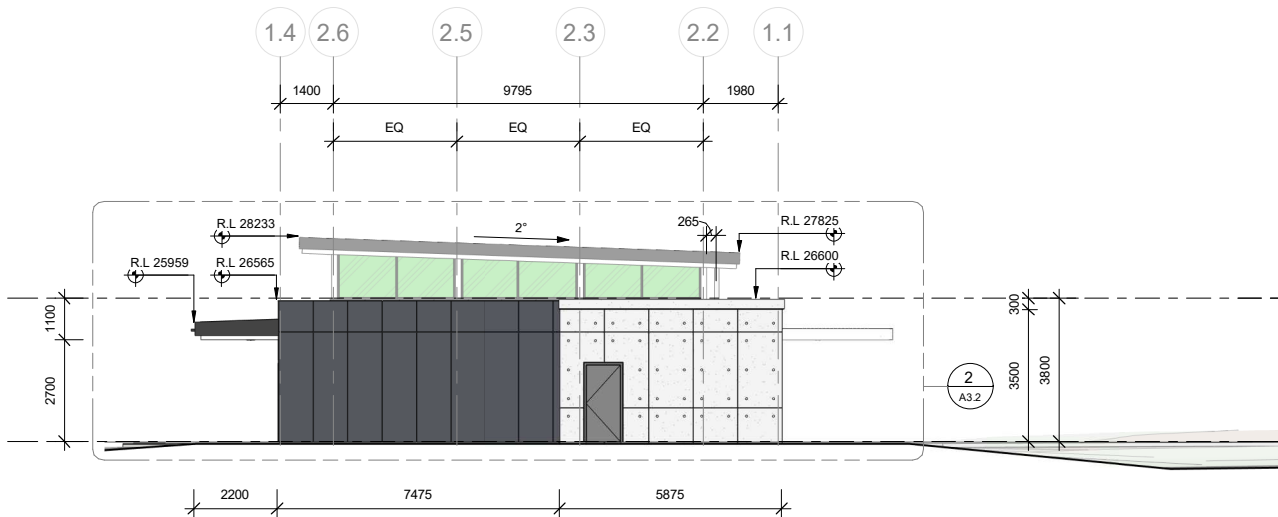
BUILDER MUST CHECK ALL DIMENSIONS, HEIGHTS, AND LEVELS ON SITE BEFORE COMMENCING ANY WORK.  
DIMENSIONS TO GOVERN SCALE.  
ALL REFERENCES TO ELECTRICAL AND MECHANICAL SERVICES ARE INDICATIVE ONLY. REFER TO THE RELEVANT ENGINEERS' DRAWINGS AND SPEC. FOR FINAL SELECTIONS AND LAYOUT.

SCALE	1 : 100	DWG NUMBER	REV.
DRAWN	BGW	A2.6	e
CHECKED	CP		
JOB NO.	2003	PLOT DATE	19/01/2024

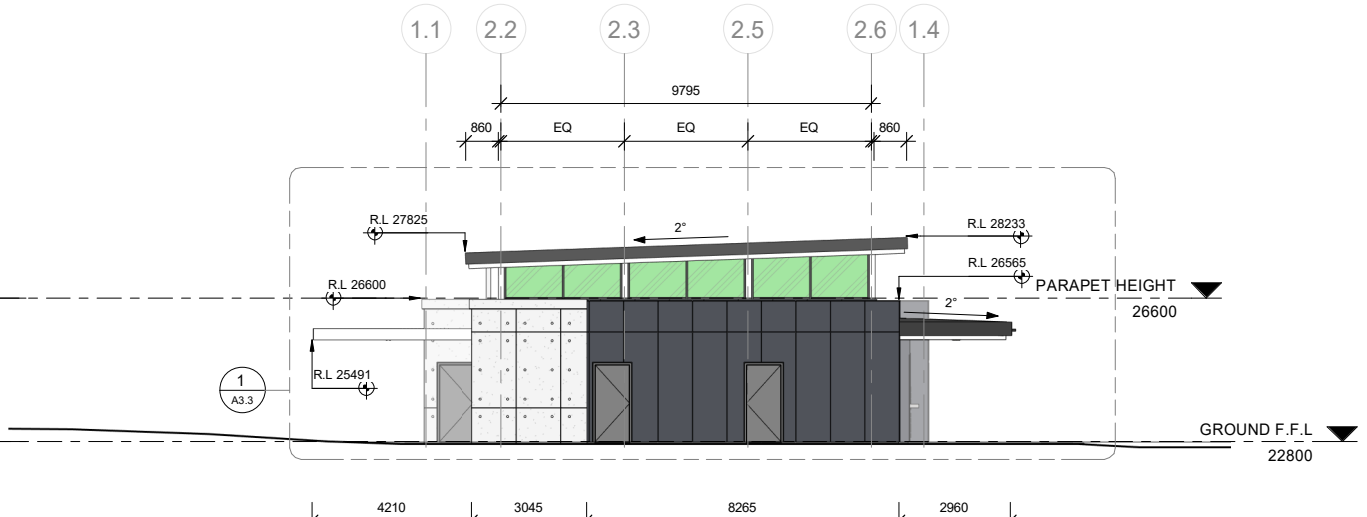
200mm  
400mm  
300mm  
200mm  
100mm



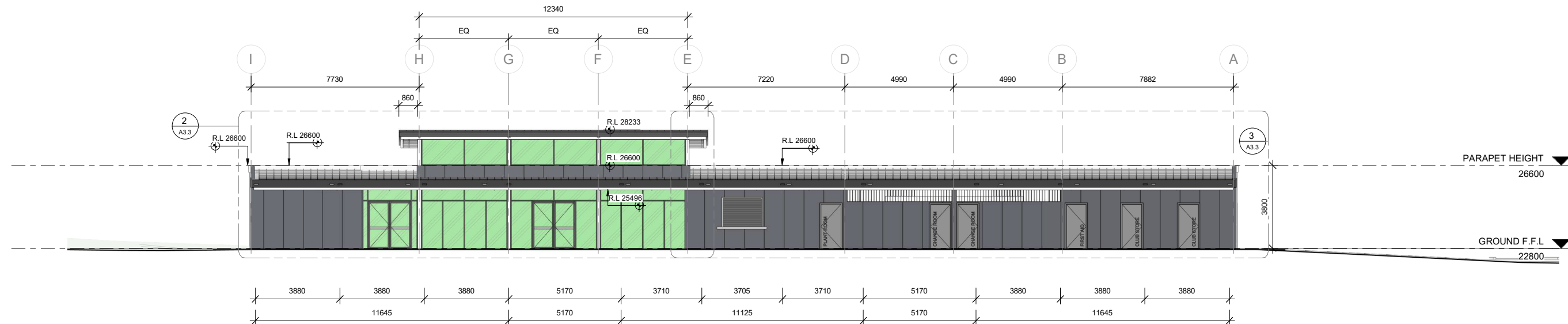
1 SOUTH ELEVATION - OVERALL  
A2.1 1:100



2 WEST ELEVATION - OVERALL  
A2.1 1:100



3 EAST ELEVATION - OVERALL  
A2.1 1:100



4 NORTH ELEVATION - OVERALL  
A2.1 1:100

NOTE:  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER  
DETAILS ON FINISHES, FIXTURES AND FITTINGS IN CONJUNCTION  
WITH ARCHITECTURAL SPECIFICATION.

NO.	DATE	AMENDMENT
a	20 10 12	DEVELOPMENT APPLICATION ISSUE
d	20 08 04	ISSUED TO CONSULTANTS
c	20 07 30	ISSUED FOR REVIEW
b	20 07 01	ISSUED PRELIMINARY COSTING
e	20 06 18	SCHEMATIC DESIGN ISSUE

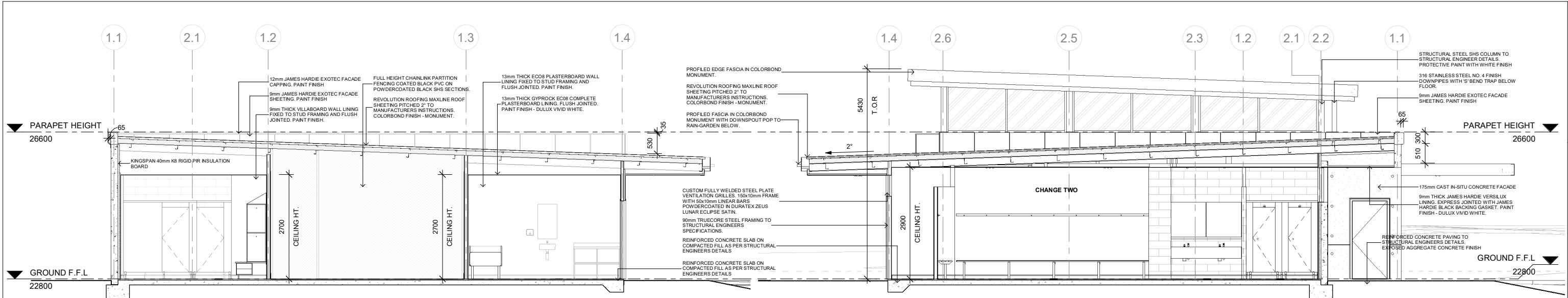


HONEYWOOD PAVILION  
CITY OF KWINANA  
TITLE  
ELEVATIONS OVERALL

BUILDER MUST CHECK ALL DIMENSIONS, HEIGHTS, AND LEVELS  
ON SITE BEFORE COMMENCING ANY WORK.  
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DRAWINGS AND SPEC. FOR FINAL SELECTIONS AND LAYOUT.

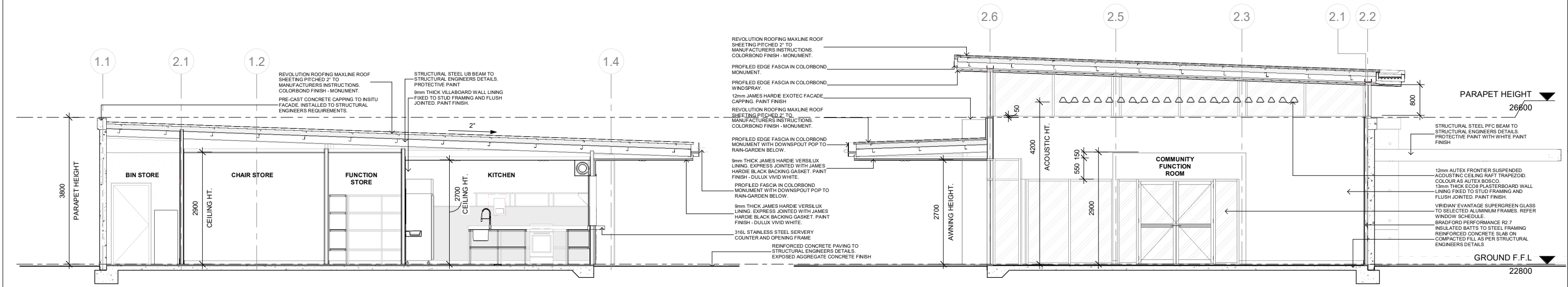
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1:100	A3.1	e
DRAWN BGW		
CHECKED CP		
JOB NO. 2003	PLOT DATE	

CHECK PRINT SCALE



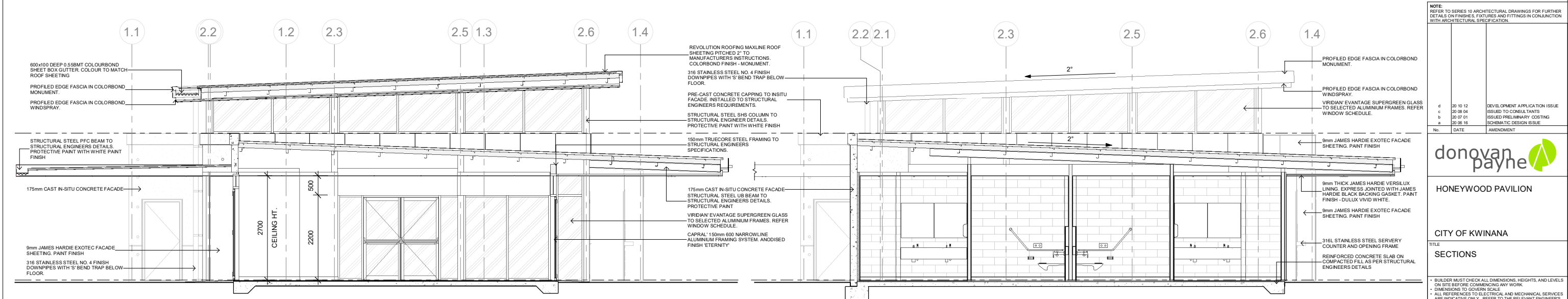
AA SECTION  
A2.1 1:50

BB SECTION  
A2.1 1:50



CC SECTION  
A2.1 1:50

DD SECTION  
A2.1 1:50



EE SECTION  
A2.1 1:50

FF SECTION  
A2.1 1:50

NOTE:  
REFER TO SERIES 10 ARCHITECTURAL DRAWINGS FOR FURTHER  
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a	20 06 18	SCHEMATIC DESIGN ISSUE

donovan payne

HONEYWOOD PAVILION

CITY OF KWINANA

TITLE  
SECTIONS

BUILDER MUST CHECK ALL DIMENSIONS, HEIGHTS, AND LEVELS  
ON SITE BEFORE COMMENCING ANY WORK.  
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DRAWINGS AND SPEC. FOR FINAL SELECTIONS AND LAYOUT.  
© ALL DRAWINGS ARE ARCHITECTS COPYRIGHT

SCALE	DWG NUMBER	REV.
SCALE 1:50		
DRAWN BGW	A4.1	d
CHECKED CP		
JOB NO. 2003	PLOT DATE	

PLEASE PRINT IN  
CHECK PRINT SCALE

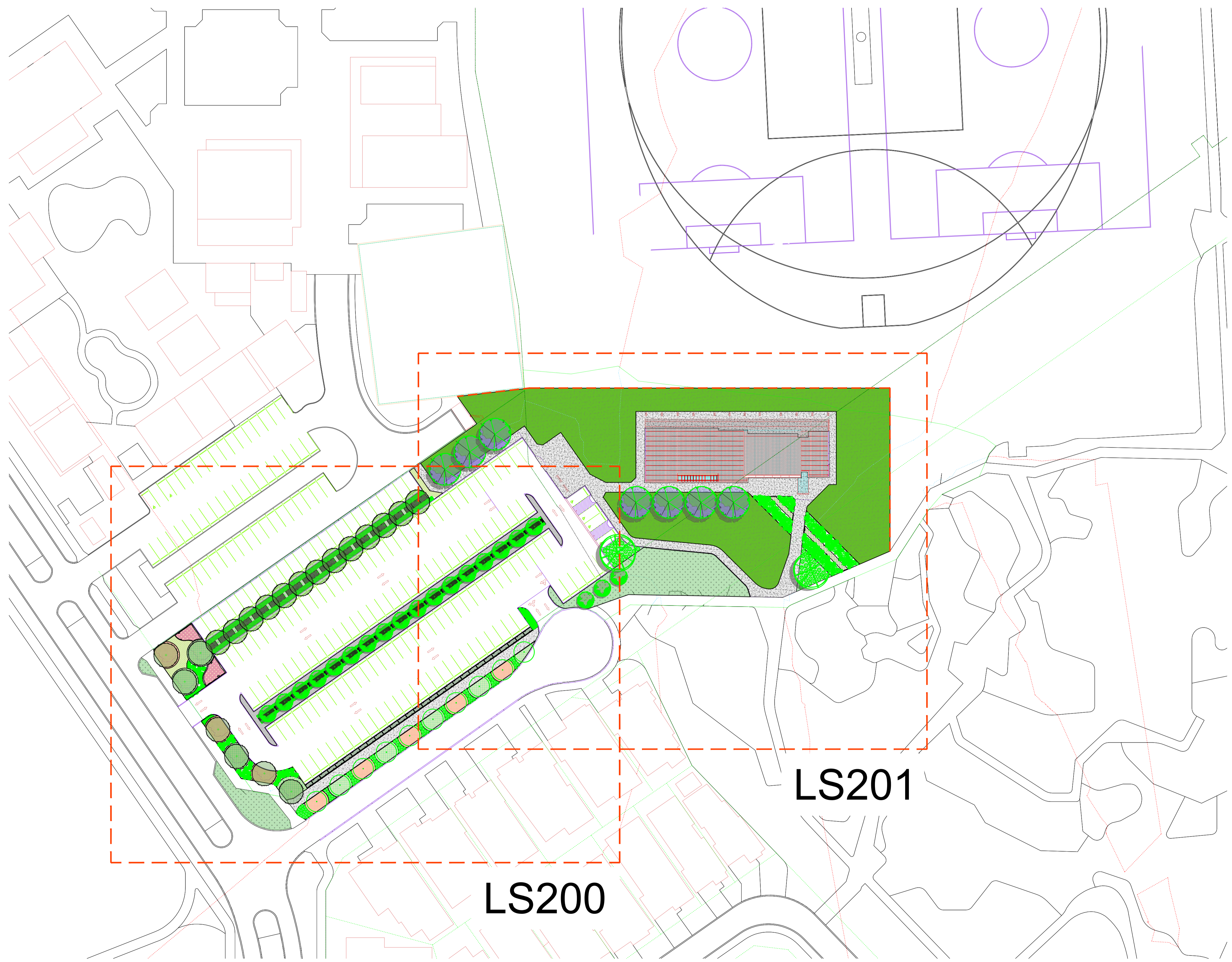


# HORT PLAN

LANDSCAPE DESIGN CONSULTANCY SERVICES

## **PROJECT**

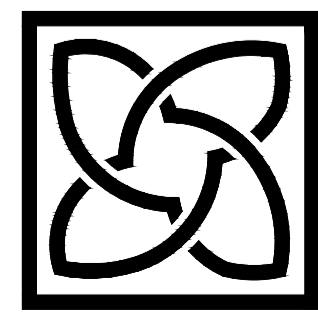
*Honeywood Pavilion  
City Of Kwinana*



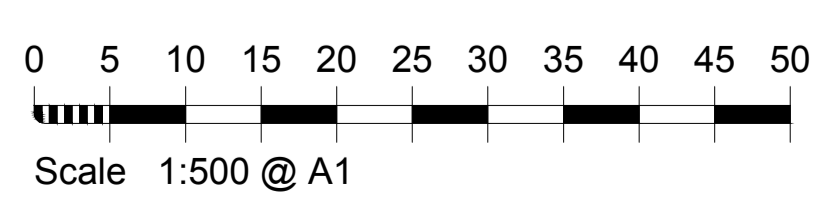
**INDEX**

DWG	DWG NAME	SCALE @ A1	REV
LS -100	SITE PLAN	1:500	B
LS -200	CONCEPT PLAN	1:200	B
LS -201	CONCEPT PLAN	1:200	B
LS -300	PLANTING SCHEDULE	NTS	B
LS -400	TYPICAL DETAILS	AS SHOWN	B

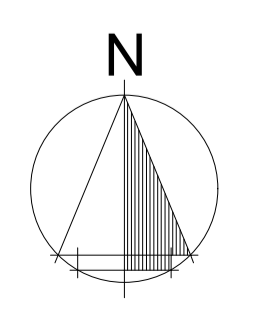
**SITE PLAN**  
SCALE 1:500 @ A1



**HortPlan**  
Consultants, Contractors  
Project Management  
24 Stirling Street, Guildford WA 6055  
Phone - 08 9379 2333  
Email - hortplan@bigpond.com



ISSUE FOR COMMENT



REV	DESCRIPTION	BY	CHKD	DATE
	AMENDMENTS			

SCALE	1:500
DATE	13.07.2020
DRAWN	DAVID STEVENS
DESIGN	DAVID STEVENS
JOB NO	###JB

PROJECT	Honeywood Pavilion City of Kwinana		
TITLE	Proposed Landscape Concept Site Plan LS-100		
SIZE	A1	DWG No	HON-KWI-HP
REV	B		

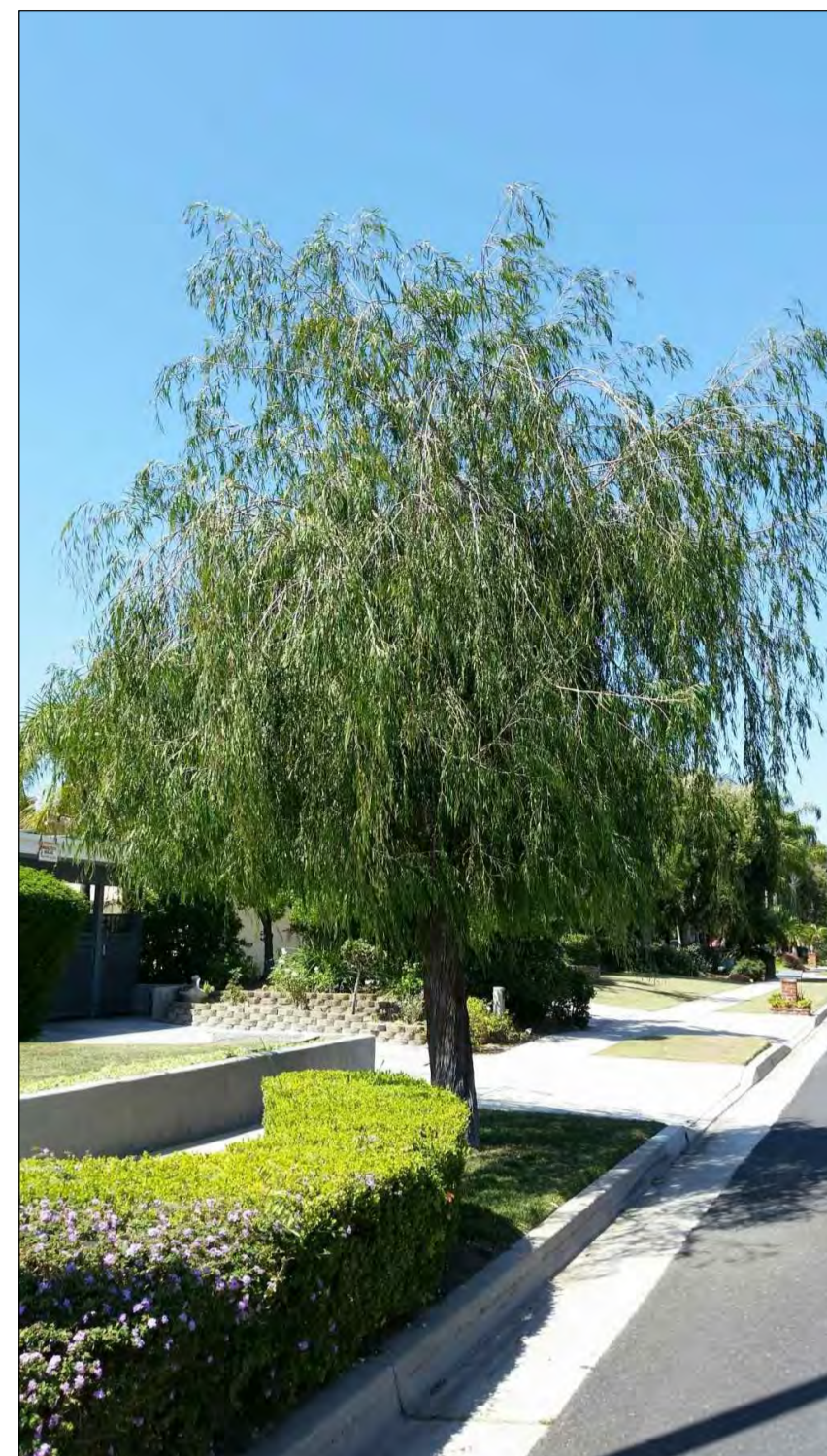






PLANTING IMAGES

TREES



Agonis flexuosa



Eucalyptus victrix



Jacaranda mimosifolia



Tristaniopsis laurina luscious

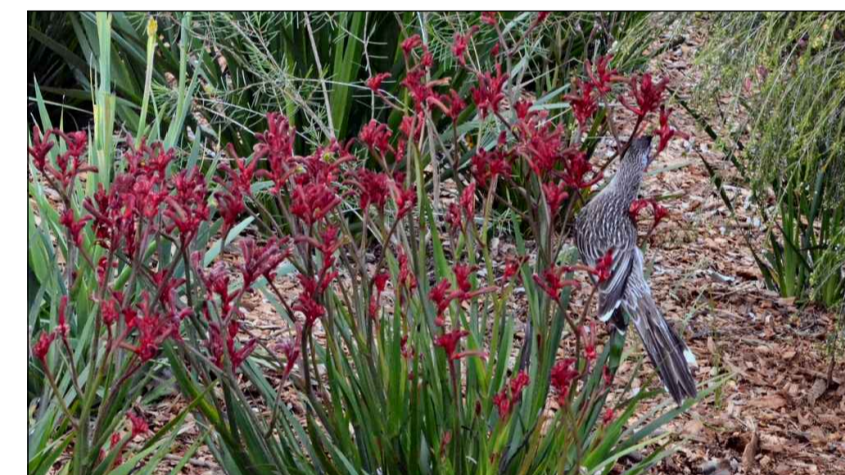
SHRUBS



Anigozanthus Amber Velvet



Anigozanthus Bush Elegance



Anigozanthus Big Red



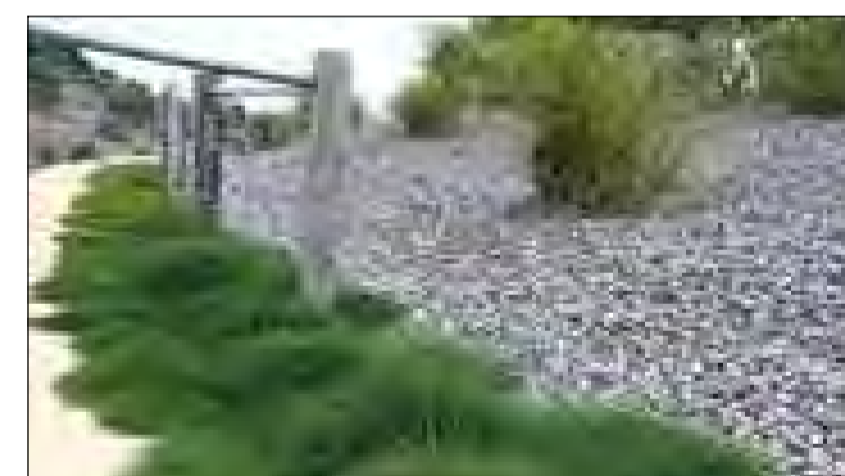
Anigozanthus Humilis



Banksia Nivea



Conostylis aculeata



Casuarina glauca



Eremophila Glabra 'Roseworthy'



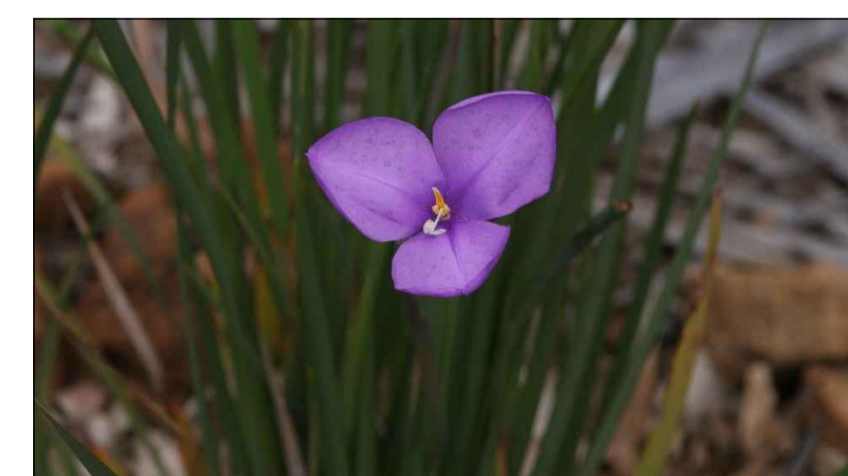
Lomandra Confertifolia 'Wingarra'



Myoporum parvifolium 'Alba'



Olearia axillaris 'Little Smokie'



Patersonia occidentalis

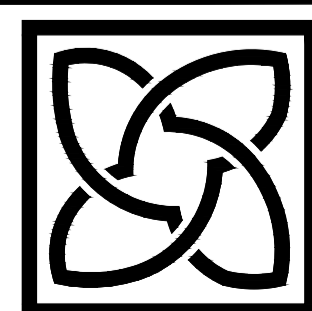


Verticordia chrysanthella

PLANTING SCHEDULE

Trees					
Key	Species	Common Name	Planting Centres	Pot Size	Quantity
AF	Agonis flexuosa	Weeping Peppermint	As Shown	100litre	20
JM	Jacaranda mimosifolia	Jacaranda	As Shown	200litre	7
EV	Eucalyptus victrix	Little Ghost Gum	As Shown	200litre	2
TLL	Tristaniopsis laurina luscious	Kanooka Gum	As Shown	100litre	20

Shrubs					
Key	Species	Common Name	Planting Centres	Pot Size	Quantity
Aa	Anigozanthus Amber Velvet	Kangaroo Paw	3 per sqm	140mm	91
Ab	Anigozanthus Bush Elegance	Kangaroo Paw	3 per sqm	140mm	91
Abr	Anigozanthus Big Red	Kangaroo Paw	3 per sqm	140mm	199
Ah	Anigozanthus Humilis	Catspaw	3 per sqm	140mm	91
Bn	Banksia nivea	Honeypot dryandra	2 per sqm	140mm	77
Ca	Conostylis aculeata	Prickly conostylis	2 per sqm	140mm	243
Cg	Casuarina glauca	Cousin It	2 per sqm	140mm	382
Eg	Eremophila glabra "Roseworthy"	Emu Bush	2 per sqm	140mm	344
Lc	Lomandra confertifolia "Wingarra"	Mat Rush	2 per sqm	140mm	102
Mpa	Myoporum Parvifolium "Alba"	Creeping boobiala	3 per sqm	140mm	930
Oa	Olearia axillaris 'Little Smokie'	Coastal Daisy Bush	2 per sqm	140mm	394
Po	Patersonia occidentalis	Native Iris	3 per sqm	140mm	91
Vc	Verticordia chrysanthella	Feather Flower	3 per sqm	140mm	99



**HortPlan**  
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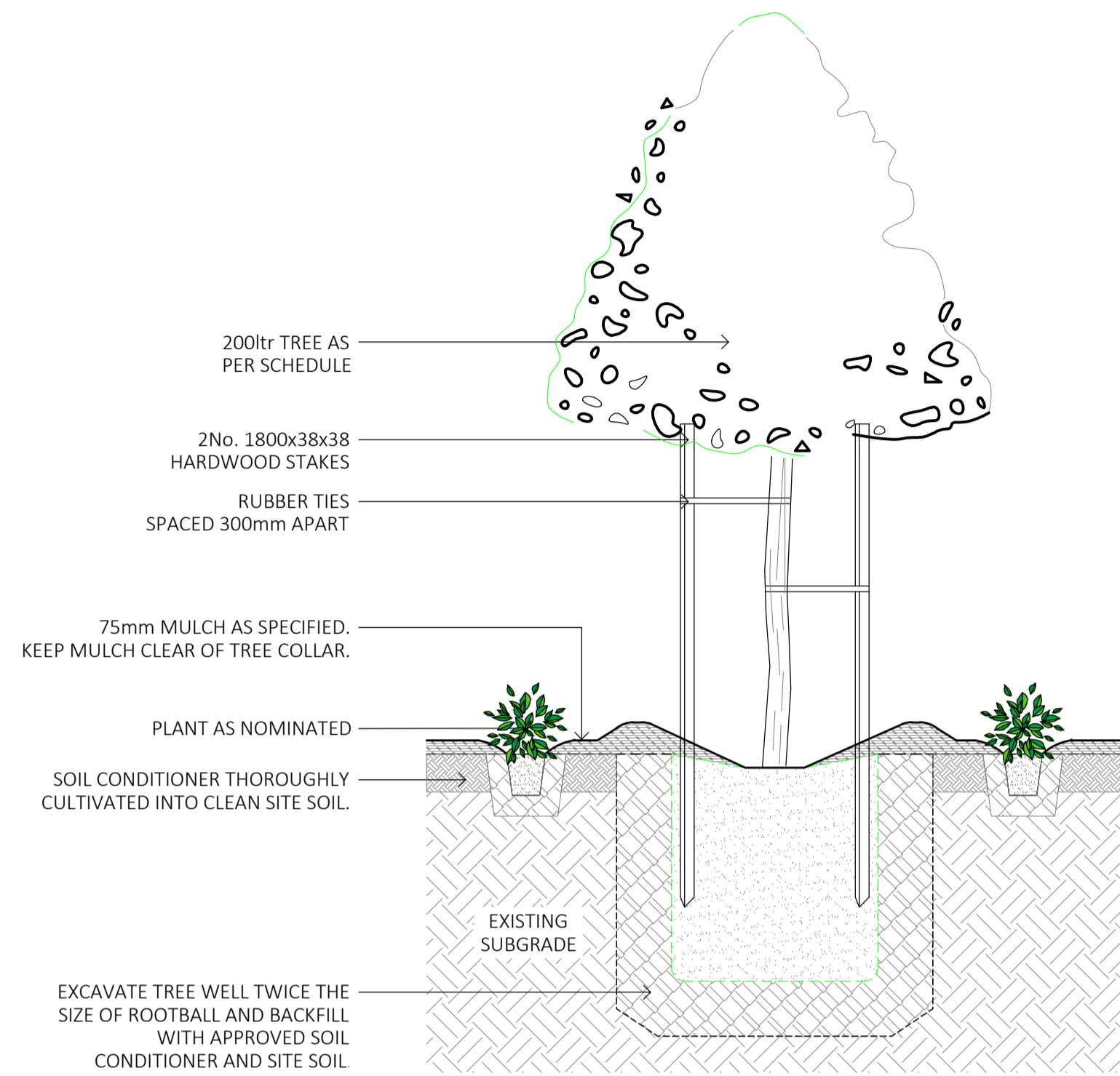
ISSUE FOR COMMENT

REV	DESCRIPTION	BY	CHKD	DATE
	AMENDMENTS			

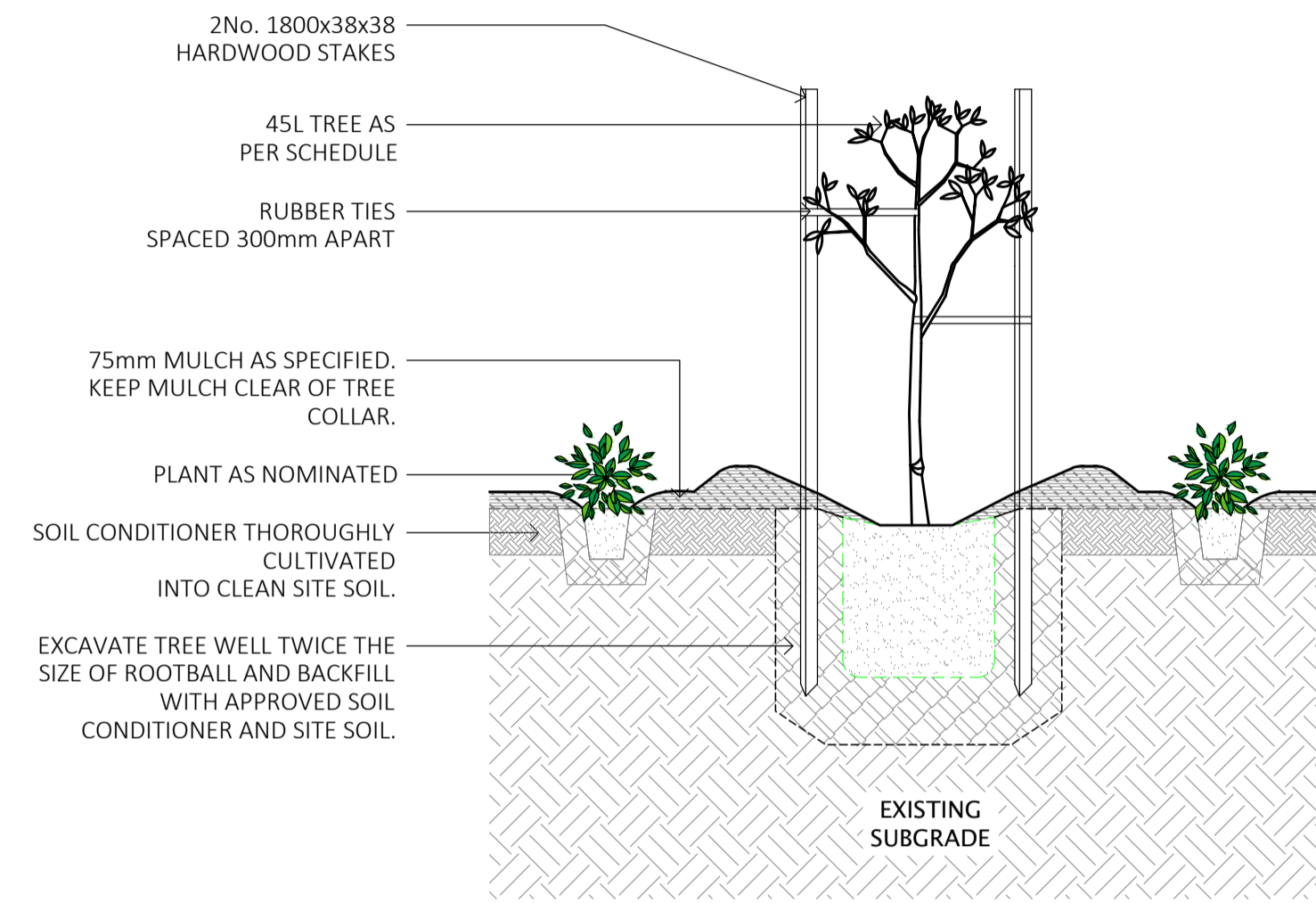
SCALE	AS SHOWN
DATE	13.07.2020
DRAWN	DAVID STEVENS
DESIGN	DAVID STEVENS
JOB NO	###JB

PROJECT	Honeywood Pavilion City of Kwinana		
TITLE	Proposed Landscape Concept Planting Schedule LS-300		
SIZE	A1	DWG No	HON-KWI-HP
REV			B

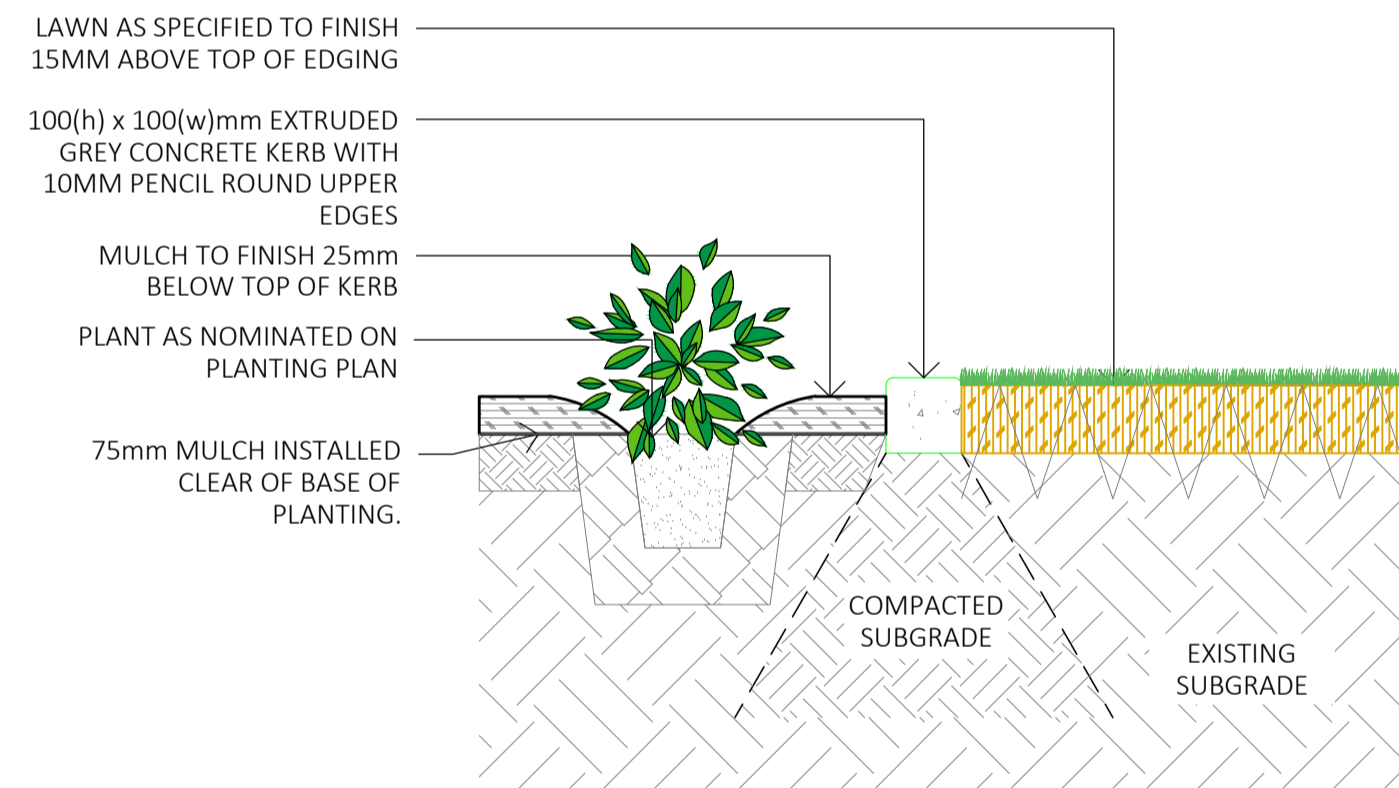
TYPICAL DETAILS



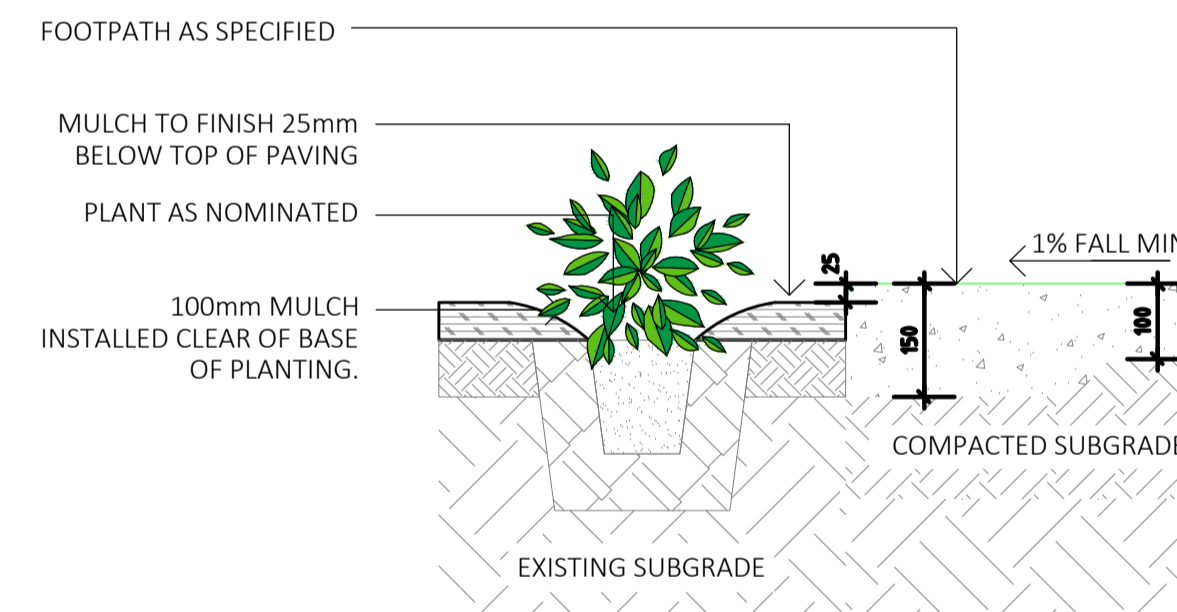
TYPICAL 200ltr TREE PLANTING DETAIL  
SCALE 1:20 @ A1



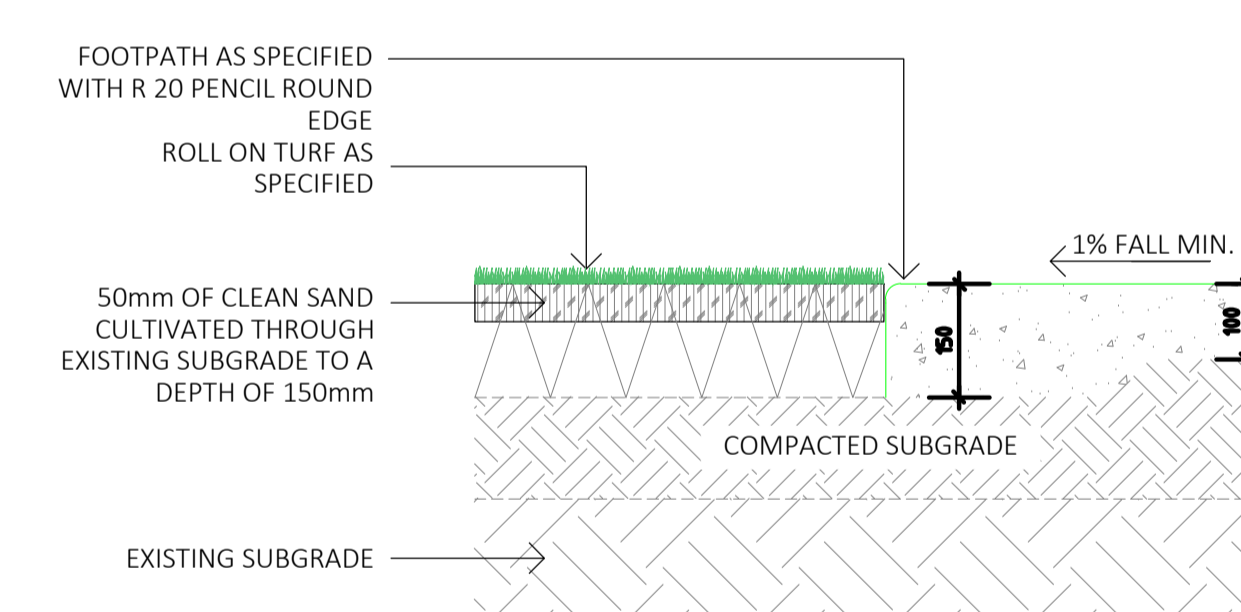
TYPICAL 45ltr TREE PLANTING DETAIL  
SCALE 1:20 @ A1



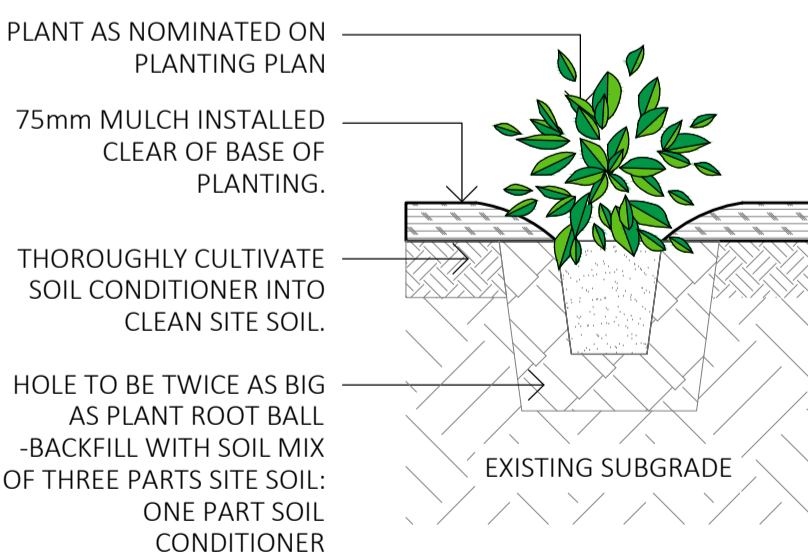
TYPICAL EDGING DETAIL  
SCALE 1:10 @ A1



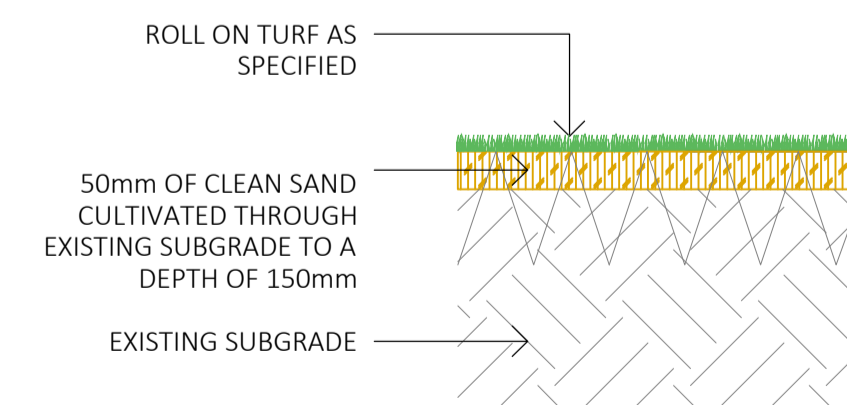
PAVEMENT TO GARDEN BED TYPICAL DETAIL  
SCALE 1:10 @ A1



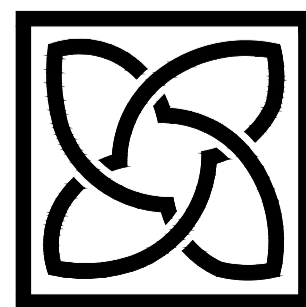
PAVEMENT TO LAWN TYPICAL DETAIL  
SCALE 1:10 @ A1



TYPICAL PLANTING DETAIL  
SCALE 1:10 @ A1



TYPICAL LAWN DETAIL  
SCALE 1:10 @ A1



**HortPlan**  
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Phone - 08 9379 2333  
Email - hortplan@bigpond.com

ISSUE FOR COMMENT

REV	DESCRIPTION	DS	MN	23.07.20
A	Issue for Comment			
		BY	CHKD	DATE
	AMENDMENTS			

SCALE	AS SHOWN
DATE	13.07.2020
DRAWN	DAVID STEVENS
DESIGN	DAVID STEVENS
JOB NO	###JB

PROJECT	Honeywood Pavilion City of Kwinana		
TITLE	Proposed Landscape Concept Typical Details LS-400		
SIZE	A1	DWG No	HON-KWI-HP
REV			B

## 18 Reports – Civic Leadership

### 18.1 Monthly Financial Report February 2021

#### DECLARATION OF INTEREST:

There were no declarations of interest declared.

#### SUMMARY:

The Monthly Financial Report, which includes the Monthly Statement of Financial Activity and explanation of material variances, for the period ended 28 February 2021 has been prepared for Council acceptance.

#### OFFICER RECOMMENDATION:

That Council:

1. Accepts the Monthly Statements of Financial Activity for the period ended 28 February 2021, contained within Attachment A; and
2. Accepts the explanations for material variances for the period ended 28 February 2021, contained within Attachment A.

#### DISCUSSION:

The purpose of this report is to provide a monthly financial report, which includes rating, investment, reserve, debtor, and general financial information to Elected Members in accordance with Section 6.4 of the *Local Government Act 1995*.

The period of review is February 2021. The current closing municipal surplus for this period is \$21,509,793 compared to a budget position of \$18,447,433. This is considered a satisfactory result for the City as it is maintaining a healthy budget surplus position.

The City's cash balances are tracking well with a positive unrestricted cash balance as at 28 February of \$10,831,060. The City no longer expects to draw on the Refuse Reserve contingency or the WATC Short Term Lending Facility.

Income for the February 2021 period year to date is \$57,961,804. This is made up of \$56,694,974 in operating revenues and \$1,266,830 in non-operating grants, contributions and subsidies received and proceeds from other investing activities. The budget estimated \$57,663,063 would be received for the same period. The variance to budget is \$298,741. Details of all significant variances are provided in the notes to the Monthly Financial Report contained within Attachment A.

Expenditure for the February 2021 period year to date is \$50,018,581. This is made up of \$44,803,667 in operating expenditure and \$5,214,914 in capital expenditure. The budget estimated \$52,698,184 would be spent for the same period. The variance to budget is \$2,679,603 due to the timing of works being completed and the supplier invoicing the City. Details of all significant variances are provided in the notes to the Monthly Financial Report contained within Attachment A.

**18.1 MONTHLY FINANCIAL REPORT FEBRUARY 2021****LEGAL/POLICY IMPLICATIONS:**

Section 6.4 of the *Local Government Act 1995* requires a Local Government to prepare an annual financial statement for the preceding year and other financial reports as are prescribed.

Regulation 34 (1) of the *Local Government (Financial Management) Regulations 1996* as amended requires the Local Government to prepare monthly financial statements and report on actual performance against what was set out in the annual budget.

**FINANCIAL/BUDGET IMPLICATIONS:**

Any material variances that have an impact on the outcome of the budgeted closing surplus position are detailed in the Monthly Financial Report contained within Attachment A.

**ASSET MANAGEMENT IMPLICATIONS:**

There are no asset management implications associated with this report.

**ENVIRONMENTAL IMPLICATIONS:**

There are no environmental implications associated with this report.

**STRATEGIC/SOCIAL IMPLICATIONS:**

This proposal will support the achievement of the following outcome and objective detailed in the Corporate Business Plan.

Plan	Outcome	Objective
Corporate Business Plan	Business Performance	5.4 Ensure the financial sustainability of the City of Kwinana into the future

**COMMUNITY ENGAGEMENT:**

There are no community engagement implications as a result of this report.

**PUBLIC HEALTH IMPLICATIONS:**

There are no public health implications as a result of this report.

## 18.1 MONTHLY FINANCIAL REPORT FEBRUARY 2021

**RISK IMPLICATIONS:**

The risk implications in relation to this proposal are as follows:

Risk Event	Inadequate management of the City's provisions, revenues and expenditures.
Risk Theme	Failure to fulfil statutory regulations or compliance Providing inaccurate advice/information
Risk Effect/Impact	Financial Reputation Compliance
Risk Assessment Context	Operational
Consequence	Minor
Likelihood	Unlikely
Rating (before treatment)	Low
Risk Treatment in place	Reduce (mitigate the risk)
Response to risk treatment required/in place	Annual adoption of variance tolerances for reporting purposes.
Rating (after treatment)	Low

**COUNCIL DECISION**

388

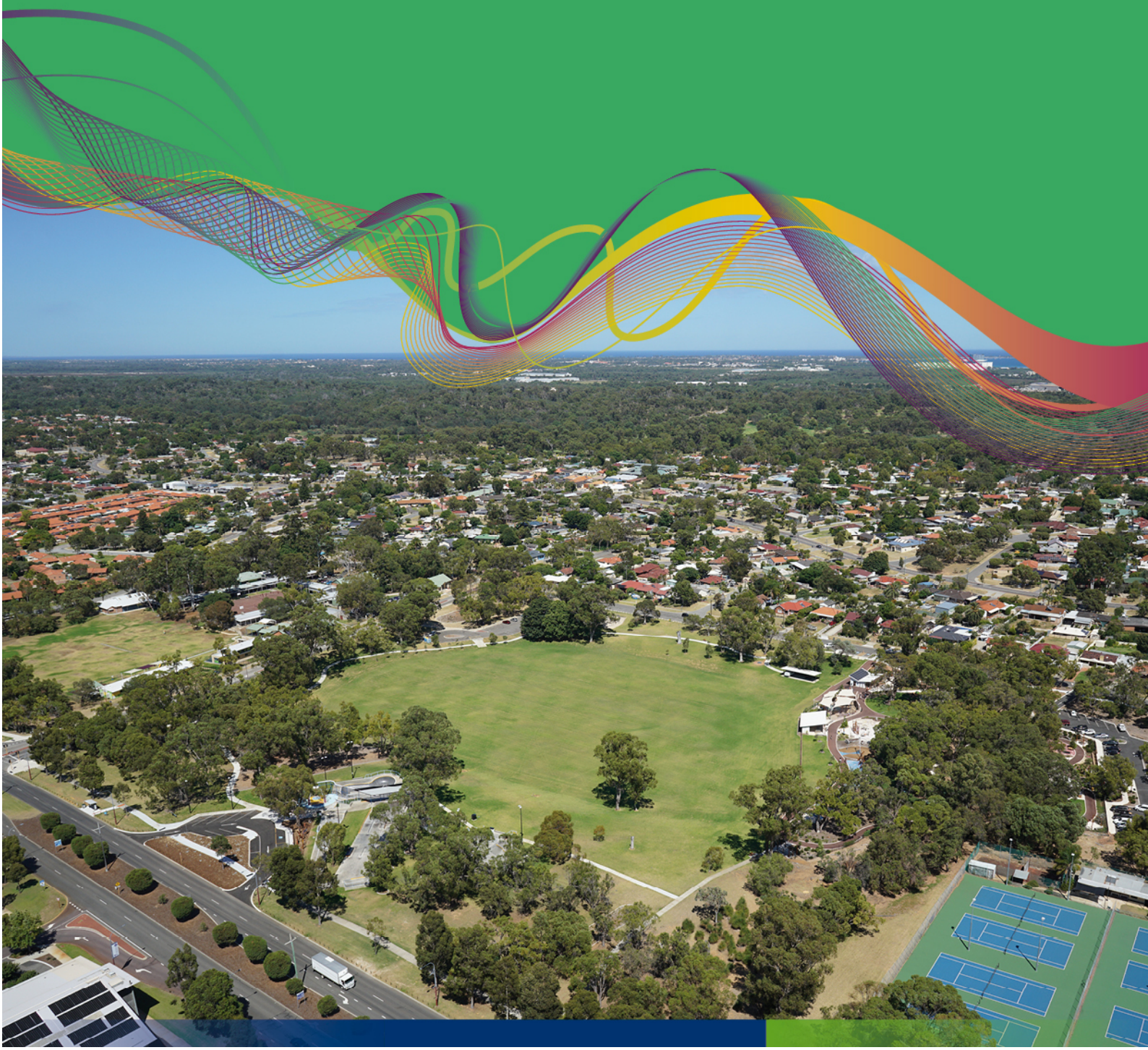
**MOVED CR W COOPER****SECONDED CR S LEE**

**That Council:**

1. **Accepts the Monthly Statements of Financial Activity for the period ended 28 February 2021, contained within Attachment A; and**
2. **Accepts the explanations for material variances for the period ended 28 February 2021, contained within Attachment A.**

**CARRIED  
6/0**

# Monthly Financial Report



# CITY OF KWINANA

## MONTHLY FINANCIAL REPORT (Containing the Statement of Financial Activity) For the period ending 28 February 2021

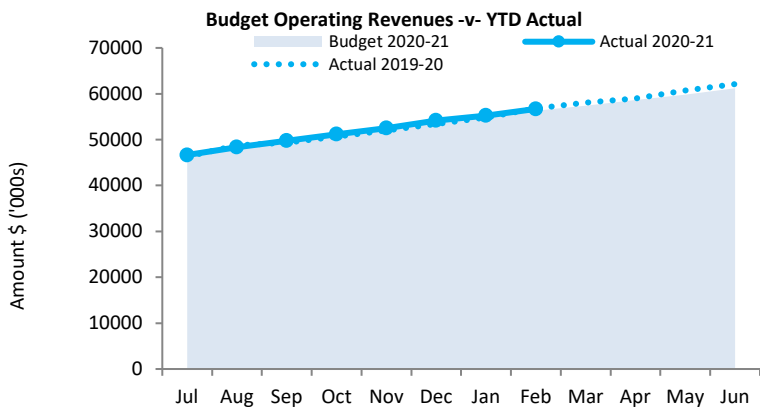
LOCAL GOVERNMENT ACT 1995  
LOCAL GOVERNMENT (FINANCIAL MANAGEMENT) REGULATIONS 1996

### TABLE OF CONTENTS

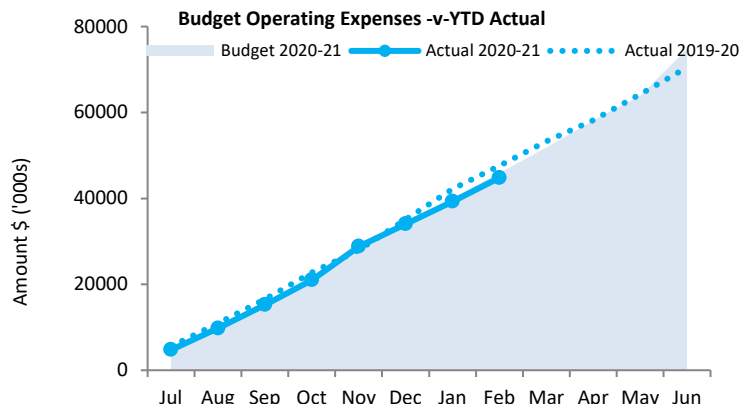
Statement of Financial Activity by Nature or Type	4
Statement of Financial Position	5
Note 1 Explanation of Material Variances	6
Note 2 Net Current Funding Position	7
Note 3 Cash and Financial Assets	8
Note 4 Receivables	9
Note 5 Disposal of Assets	11
Note 6 Capital Acquisitions	12
Note 7 Borrowings	18
Note 8 Cash Reserves	19
Note 9 Operating grants and contributions	20
Note 10 Non operating grants and contributions	21
Note 11 Trust Fund	22
Note 12 Budget Amendments	23

OPERATING ACTIVITIES

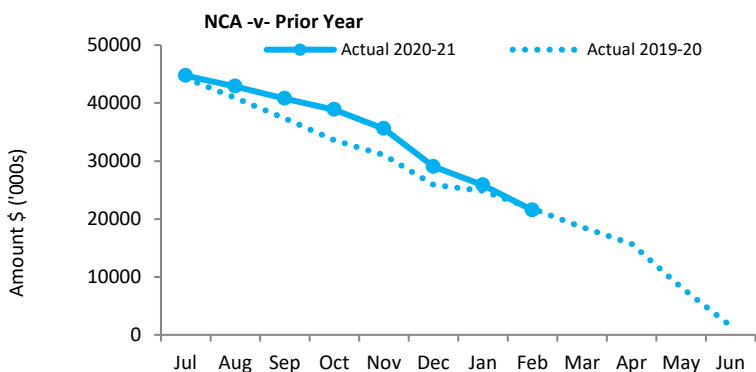
OPERATING REVENUE



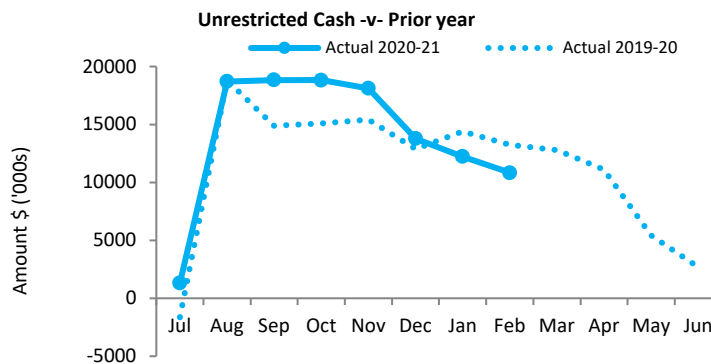
OPERATING EXPENSES



NET CURRENT ASSETS

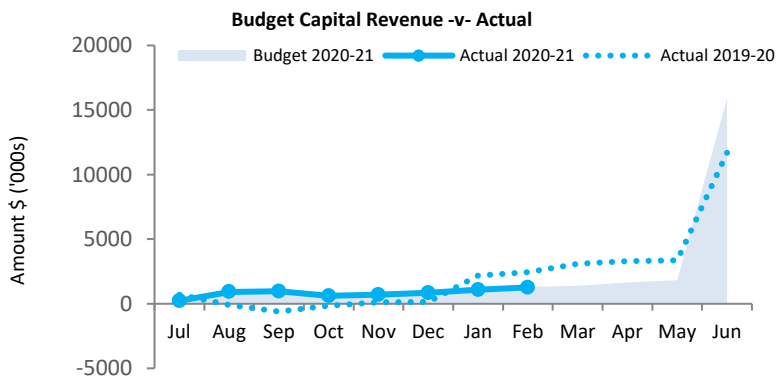


UNRESTRICTED CASH

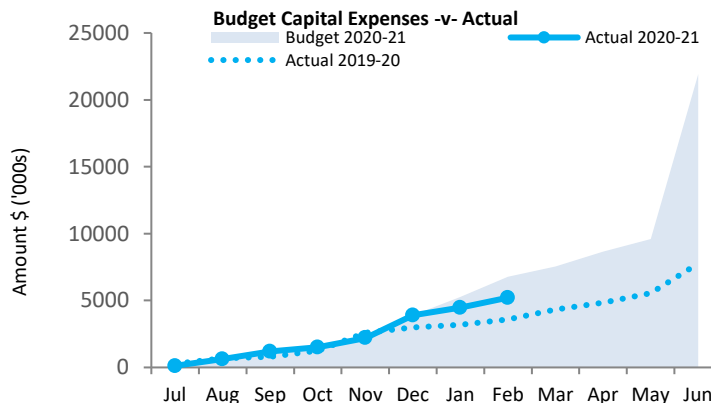


INVESTING ACTIVITIES

CAPITAL REVENUE

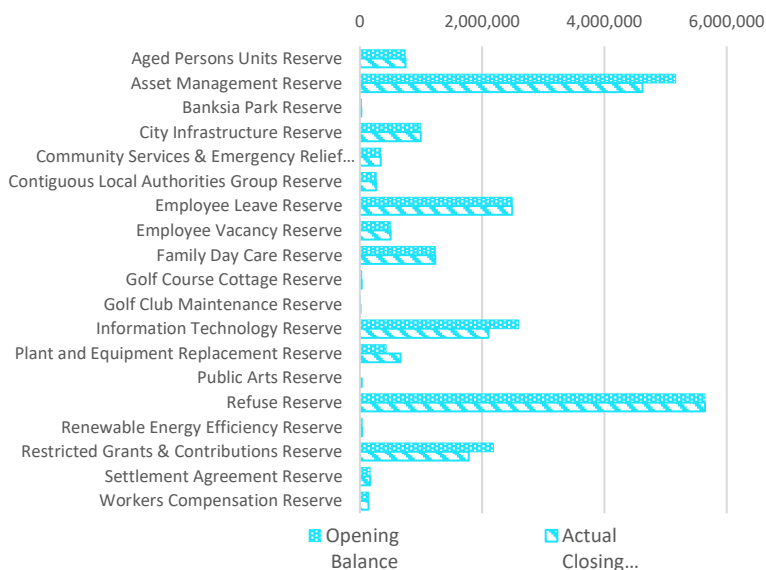


CAPITAL EXPENSES

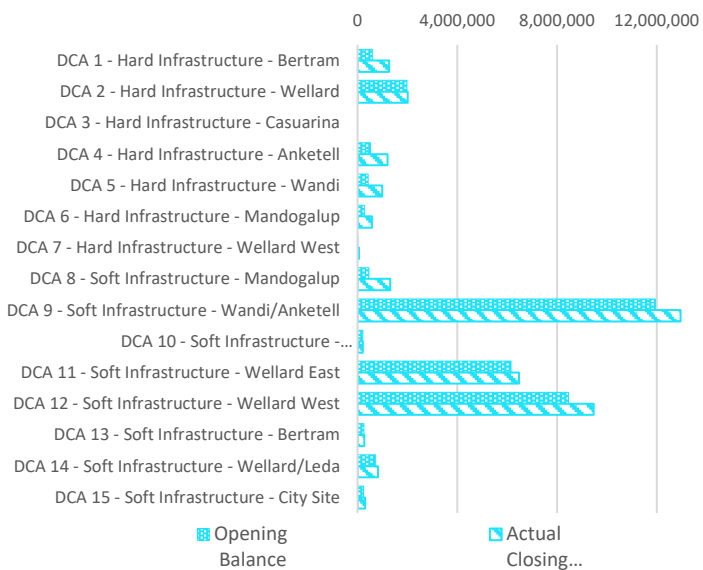


FINANCING ACTIVITIES

RESERVES



DCA RESERVES





Funding surplus / (deficit) Components

Funding surplus / (deficit)				
	Current Budget	YTD Current Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
Opening	\$2.83 M	\$2.83 M	\$2.83 M	(\$0.00 M)
Closing	\$0.00 M	\$18.45 M	\$21.51 M	\$3.06 M

Refer to Statement of Financial Activity

Cash and cash equivalents		
	\$72.57 M	% of total
Unrestricted Cash	\$10.83 M	14.9%
Restricted Cash	\$61.74 M	85.1%

Refer to Note 3 - Cash and Financial Assets

Payables	
	\$3.20 M
Trade Payables	\$0.46 M
Bonds & Deposits Held	\$1.67 M
Other Payables	\$1.08 M

Receivables		
	\$10.67 M	% Outstanding
Rates Receivable	\$9.54 M	17.3%
Trade Receivable	\$1.13 M	

Refer to Note 4 - Receivables

Key Operating Activities

Amount attributable to operating activities			
Current Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
\$1.09 M	\$25.27 M	\$28.09 M	\$2.81 M

Refer to Statement of Financial Activity

Rates Revenue		
	YTD Actual	% Variance
	\$40.87 M	
	YTD Budget	\$40.68 M
		0.5%

Refer to Statement of Financial Activity

Operating Grants and Contributions		
	YTD Actual	% Variance
	\$4.38 M	
	YTD Budget	\$4.37 M
		0.3%

Refer to Note 10 - Operating Grants and Contributions

Fees and Charges		
	YTD Actual	% Variance
	\$10.31 M	
	YTD Budget	\$10.18 M
		1.3%

Refer to Statement of Financial Activity

Key Investing Activities

Amount attributable to investing activities			
Adopted Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
(\$5.21 M)	(\$5.45 M)	(\$3.94 M)	\$1.51 M

Refer to Statement of Financial Activity

Proceeds on sale		
	YTD Actual	% Received
	\$0.21 M	
	YTD Budget	\$0.31 M
		45.9%
	Current Budget	\$0.46 M

Refer to Note 5 - Disposal of Assets

Asset Acquisition		
	YTD Actual	% Spent
	\$5.21 M	
	Current Budget	\$21.92 M
		23.8%
	Adopted Budget	\$19.12 M
		27.3%

Refer to Note 6 - Capital Acquisition

Capital Grants		
	YTD Actual	% Received
	\$1.05 M	
	Current Budget	\$5.15 M
		20.5%
	Adopted Budget	\$3.11 M
		33.9%

Refer to Note 6 - Capital Acquisition

Key Financing Activities

Amount attributable to financing activities			
Adopted Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
\$10.08 M	(\$4.21 M)	(\$5.47 M)	(\$1.26 M)

Refer to Statement of Financial Activity

Borrowings	
	\$0.93 M
Principal repayments	\$0.93 M
Interest expense	\$0.47 M
Principal due	\$19.34 M

Refer to Note 7 - Borrowings

Reserves	
	\$21.83 M
Reserves balance	\$21.83 M
DCA Reserve Balance	\$37.90 M
Interest earned	\$0.13 M

Refer to Note 9 - Cash Reserves

Lease Liability	
	\$0.06 M
Principal repayments	\$0.06 M
Interest expense	\$0.00 M
Principal due	\$0.13 M

reclassified from Lease Expense June 2020 - AASB16

This information is to be read in conjunction with the accompanying Financial Statements and notes.

**STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2021**

**BY NATURE OR TYPE**

	Ref Note	Adopted Budget	Current Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)	Var. % (b)-(a)/(a)	Var.
		\$		\$	\$	\$	%	
<b>Opening funding surplus / (deficit)</b>		1,360,000	2,833,954	2,833,954	<b>2,833,950</b>	(4)	(0.0%)	
<b>Revenue from operating activities</b>								
Rates	5	39,933,543	40,753,225	40,683,130	<b>40,872,835</b>	189,705	0.5%	
Operating grants, subsidies and contributions	10	6,617,653	6,467,901	4,369,038	<b>4,383,806</b>	14,768	0.3%	
Fees and charges		11,913,171	12,388,552	10,184,869	<b>10,313,112</b>	128,243	1.3%	
Interest earnings		1,220,577	844,847	539,489	<b>535,532</b>	(3,957)	(0.7%)	
Other revenue		451,881	696,934	542,305	<b>570,317</b>	28,012	5.2%	
Profit on disposal of assets	5	124,510	57,251	27,051	<b>19,371</b>	(7,680)	(28.4%)	
		<b>60,261,335</b>	<b>61,208,710</b>	<b>56,345,882</b>	<b>56,694,974</b>	349,092		
<b>Expenditure from operating activities</b>								
Employee costs		(28,033,854)	(27,142,999)	(17,353,210)	<b>(17,062,202)</b>	291,008	1.7%	
Materials and contracts		(27,604,630)	(26,718,468)	(15,421,609)	<b>(14,565,239)</b>	856,370	5.6%	▲
Utility charges		(2,557,908)	(2,479,418)	(1,442,145)	<b>(1,472,678)</b>	(30,533)	(2.1%)	
Depreciation on non-current assets		(14,691,676)	(16,428,697)	(10,622,633)	<b>(10,623,968)</b>	(1,335)	(0.0%)	
Interest expenses		(987,579)	(975,909)	(467,409)	<b>(467,391)</b>	18	0.0%	
Insurance expenses		(583,736)	(586,394)	(584,786)	<b>(584,756)</b>	30	0.0%	
Other expenditure		(189,547)	(431,942)	(15,432)	<b>(15,980)</b>	(548)	(3.6%)	
Loss on disposal of assets	5	(83,085)	(20,244)	(17,743)	<b>(11,454)</b>	6,289	35.4%	
		<b>(74,732,015)</b>	<b>(74,784,071)</b>	<b>(45,924,967)</b>	<b>(44,803,667)</b>	1,121,300		
Non-cash amounts excluded from operating activities	2	8,241,957	14,662,650	14,852,718	<b>16,196,123</b>	1,343,406	9.0%	▲
<b>Amount attributable to operating activities</b>		<b>(6,228,723)</b>	<b>1,087,289</b>	<b>25,273,633</b>	<b>28,087,430</b>	2,813,798		
<b>Investing activities</b>								
Grants, Subsidies and Contributions	11	13,434,461	15,436,197	1,007,527	<b>1,053,813</b>	46,286	4.6%	
Proceeds from disposal of assets	5	473,350	463,703	309,654	<b>213,017</b>	(96,637)	(31.2%)	
Self-Supporting Loan Principal Received	7	17,269	17,269	12,891	<b>12,916</b>	25	0.2%	
Self-Supporting Loan Principal Paid		(17,269)	(17,269)	(8,563)	<b>(8,563)</b>	(0)	0.0%	
Payments for property, plant and equipment	6	(19,121,835)	(21,917,448)	(6,773,217)	<b>(5,214,914)</b>	1,558,303	23.0%	▲
<b>Amount attributable to investing activities</b>		<b>(5,214,024)</b>	<b>(6,017,548)</b>	<b>(5,451,708)</b>	<b>(3,943,731)</b>	1,507,977		
<b>Financing Activities</b>								
Proceeds from new debentures	7	5,000,000	0	0	<b>0</b>	0	0.0%	
Repayment of debentures	7	(6,871,990)	(1,871,990)	(924,544)	<b>(924,543)</b>	1	0.0%	
Payments for principal portion of lease liabilities		(229,206)	(226,506)	(85,812)	<b>(64,565)</b>	21,247	24.8%	
Transfer from reserves	9	20,399,567	18,193,336	1,668,145	<b>1,613,920</b>	(54,225)	3.3%	
Transfer to reserves	9	(8,215,624)	(13,998,535)	(4,866,235)	<b>(6,092,669)</b>	(1,226,434)	(25.2%)	▼
<b>Amount attributable to financing activities</b>		<b>10,082,747</b>	<b>2,096,305</b>	<b>(4,208,446)</b>	<b>(5,467,857)</b>	(1,259,411)		
<b>Closing funding surplus / (deficit)</b>		<b>0</b>	<b>0</b>	<b>18,447,433</b>	<b>21,509,793</b>	3,062,360		

**KEY INFORMATION**

▲ ▼ Indicates a variance between Year to Date (YTD) Actual and YTD Budget data as per the adopted materiality threshold.

Refer to Note 1 for an explanation of the reasons for the variance.

This statement is to be read in conjunction with the accompanying Financial Statements and Notes.

**STATEMENT OF FINANCIAL POSITION  
FOR THE PERIOD ENDED 28 FEBRUARY 2021**

		Closing 30 June 2020	Current Month 28 February 2021	This Time Last Year 28 February 2020
	NOTE	\$	\$	\$
<b>CURRENT ASSETS</b>				
Cash and cash equivalents	9	25,471,026	19,567,944	3,431,154
Other financial assets - Term Deposits	9	33,787,559	52,656,800	63,187,945
Other financial assets - Banksia Park Deferred Mgmt Fees		319,490	319,490	297,819
Other financial assets - Self Supporting Loans		17,269	17,269	16,709
Rates receivable	4	3,588,655	9,537,515	9,417,852
Other receivables ( <i>incl. allowance for impairment</i> )	4	866,606	1,323,678	1,546,860
Inventories		37,323	40,992	37,906
Other assets		1,776,055	1,311,758	0
<b>TOTAL CURRENT ASSETS</b>		<b>65,863,982</b>	<b>84,775,445</b>	<b>77,936,245</b>
<b>NON-CURRENT ASSETS</b>				
Trade and other receivables		760,126	746,740	677,069
Other financial assets - Banksia Park Deferred Mgmt Fees		3,184,673	3,184,673	3,095,582
Other financial assets - Local Govt House Trust		124,637	129,162	124,637
Other financial assets - Self Supporting Loans		232,704	219,788	237,475
Investment in associate (SMRC)		504,775	504,775	319,244
Property, plant and equipment		135,058,794	132,288,761	147,775,853
Infrastructure		363,572,820	360,060,545	358,518,303
Intangible assets		616,115	1,360,960	459,422
Right of use assets		190,680	113,990	0
<b>TOTAL NON-CURRENT ASSETS</b>		<b>504,245,324</b>	<b>498,609,394</b>	<b>511,207,586</b>
<b>TOTAL ASSETS</b>		<b>570,109,306</b>	<b>583,384,839</b>	<b>589,143,831</b>
<b>CURRENT LIABILITIES</b>				
Trade and other payables		7,443,587	3,200,458	3,405,298
Banksia Park Unit Contributions		16,975,350	16,975,350	17,133,425
Contract and other liabilities	10 & 11	10,282,025	10,298,906	0
Lease liabilities		114,357	49,792	0
Borrowings	7	2,314,779	2,314,779	1,451,093
Employee related provisions		5,505,931	5,482,014	4,884,656
<b>TOTAL CURRENT LIABILITIES</b>		<b>42,636,029</b>	<b>38,321,300</b>	<b>26,874,472</b>
<b>NON-CURRENT LIABILITIES</b>				
Other liabilities (Developer Contributions)	11	16,953,626	22,531,874	0
Lease liabilities		77,148	77,148	0
Borrowings	7	17,953,926	17,020,819	19,398,709
Employee related provisions		410,209	410,209	469,604
<b>TOTAL NON-CURRENT LIABILITIES</b>		<b>35,394,909</b>	<b>40,040,050</b>	<b>19,868,313</b>
<b>TOTAL LIABILITIES</b>		<b>78,030,938</b>	<b>78,361,350</b>	<b>46,742,785</b>
<b>NET ASSETS</b>		<b>492,078,369</b>	<b>505,023,489</b>	<b>542,401,045</b>
<b>EQUITY</b>				
Retained surplus		216,805,177	225,271,549	257,281,113
Reserves - cash/financial asset backed	9	55,249,686	59,728,435	53,388,097
Revaluation surplus		220,023,505	220,023,505	231,731,835
<b>TOTAL EQUITY</b>		<b>492,078,369</b>	<b>505,023,489</b>	<b>542,401,045</b>

This statement is to be read in conjunction with the accompanying notes.

**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2021**

**NOTE 1  
EXPLANATION OF MATERIAL VARIANCES**

The material variance thresholds are adopted annually by Council as an indicator of whether the actual expenditure or revenue varies from the year to date budget materially.

The material variance adopted by Council for the 2020-21 year is \$50,000 or 5.00% whichever is the greater.

Reporting Program	Var. \$	Var. %	Timing/ Permanent	Explanation of Variance
	\$	%		
<b>Revenue from operating activities</b>				
Rates	189,705	0%	No Material Variance	
Operating Grants, Subsidies and Contributions	14,768	0%	No Material Variance	Refer note 10.
Fees and Charges	128,243	1%	No Material Variance	
Interest Earnings	(3,957)	(1%)	No Material Variance	
Other Revenue	28,012	5%	No Material Variance	
Profit on Disposal of Assets	(7,680)	(28%)	No Material Variance	
<b>Expenditure from operating activities</b>				
Employee Costs	291,008	2%	No Material Variance	
Materials and Contracts	856,370	6%	▲ Timing	Timing of works. Environmental Health \$168k below budget (\$82k Mainstream Waste, \$72k Natural Environment). Community Development \$127k below budget (\$81k Event & Stakeholder Management, \$32k Community Engagement & Place). Facilities Management \$118k below budget, Engineering Services \$109k below budget, Planning & Building Services \$106k below budget and Arts Centre \$103k below budget.
Utility Charges	(30,533)	(2%)	No Material Variance	
Depreciation on Non-Current Assets	(1,335)	(0%)	No Material Variance	
Interest Expenses	18	0%	No Material Variance	
Insurance Expenses	30	0%	No Material Variance	
Other Expenditure	(548)	(4%)	No Material Variance	
Loss on Disposal of Assets	6,289	35%	No Material Variance	
<b>Investing activities</b>				
Non-Operating Grants, Subsidies and Contributions	46,286	5%	No Material Variance	
Proceeds from disposal of assets	(96,637)	(31%)	Timing	Timing of disposals.
Self-Supporting Loan Principal	25	0%	No Material Variance	
Payments for property, plant and equipment	1,558,303	23%	▲ Timing	Refer note 6. Timing of capital project expenditure.
<b>Financing activities</b>				
Proceeds from new debentures	0	0%	No Material Variance	
Repayment of debentures	1	0%	No Material Variance	
Payments for principal portion of lease liabilities	21,247	25%	No Material Variance	
Transfer from reserves	(54,225)	3%	No Material Variance	
Transfer to reserves	(1,226,434)	(25%)	▼ Timing	Timing of Developer Contributions received.

**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2021**

**NOTE 2  
NET CURRENT FUNDING POSITION**

	Notes	Last Years Closing 30 June 2020	This Time Last Year 28 February 2020	YTD Actual 28 February 2021
		\$	\$	\$
<b>(a) Non-cash items excluded from operating activities</b>				
The following non-cash revenue and expenditure has been excluded from operating activities within the Statement of Financial Activity in accordance with Financial Management Regulation 32.				
<b>Non-cash items excluded from operating activities</b>				
<b>Adjustments to operating activities</b>				
Less: Profit on asset disposals	5	(124,076)	(76,107)	(19,371)
Less: Movement in pensioner deferred rates (non-current)		(48,371)	34,686	13,386
Movement in employee benefit provisions		544,157	(17,723)	(23,917)
Add: Loss on asset disposals		1,247,308	90,250	11,454
Mvmt in Local Govt House Trust		(2,017)	(2,017)	(4,525)
Change in accounting policies		(1,040,123)	-	-
Add: Depreciation on assets		15,630,143	9,559,832	10,623,968
Mvmt in operating contract liabilities associated with restricted cash		194,030	-	(47,043)
Mvmt in non-operating liabilities associated with restricted cash		82,701	-	5,642,172
Mvmt in Banksia Park deferred management fees receivable		(110,762)	-	-
Mvmt in Banksia Park valuation of unit contribution		(158,075)	-	-
<b>Total non-cash items excluded from operating activities</b>		<b>16,214,915</b>	<b>9,588,922</b>	<b>16,196,123</b>
<b>(b) Adjustments to net current assets in the Statement of Financial Activity</b>				
The following current assets and liabilities have been excluded from the net current assets used in the Statement of Financial Activity in accordance with <i>Financial Management Regulation</i> 32 to agree to the surplus/(deficit) after imposition of general rates.				
<b>Adjustments to net current assets</b>				
Less: Reserves - restricted cash		(55,249,686)	(53,388,097)	(59,728,435)
Less: Financial assets at amortised cost - self supporting loans		(17,269)	(16,709)	(17,269)
Less: Banksia Park DMF Recievable		(319,490)	(297,819)	(319,490)
Add: Borrowings		2,314,779	1,451,093	2,314,779
Add: Provisions - employee		5,505,931	4,884,656	5,482,014
Add: Current portion of contract and other liability held in reserve		10,282,025	-	10,298,906
Add: Lease liabilities		114,357	-	49,792
Add: Banksia Park Unit Contributions		16,975,350	17,133,425	16,975,350
<b>Total adjustments to net current assets</b>		<b>(20,394,003)</b>	<b>(30,233,451)</b>	<b>(24,944,352)</b>
<b>(c) Net current assets used in the Statement of Financial Activity</b>				
<b>Current assets</b>		65,863,982	77,936,245	84,775,445
<b>Less: Current liabilities</b>		(42,636,029)	(26,874,472)	(38,321,300)
<b>Less: Total adjustments to net current assets</b>		<b>(20,394,003)</b>	<b>(30,233,451)</b>	<b>(24,944,352)</b>
<b>Closing funding surplus / (deficit)</b>		<b>2,833,950</b>	<b>20,828,321</b>	<b>21,509,793</b>

**CURRENT AND NON-CURRENT CLASSIFICATION**

In the determination of whether an asset or liability is current or non-current, consideration is given to the time when each asset or liability is expected to be settled. Unless otherwise stated assets or liabilities are classified as current if expected to be settled within the next 12 months, being the Council's operational cycle.

Description	Unrestricted	Restricted	Trust	Total Cash	Interest Rate	Institution	S&P Rating	Maturity Date	Days
	\$	\$	\$	\$					
<b>Cash on hand</b>									
CBA Municipal Bank Account	3,547,303			3,547,303	Variable	CBA	AA		
CBA Online Saver Bank Account	4,279,987	8,736,884		13,016,871	0.20%	CBA	AA		
CBA Trust Bank Account			346,524	346,524	N/A	CBA	AA		
Cash On Hand - Petty Cash	3,770			3,770	N/A	PC	N/A		
<b>Term Deposits - Investments</b>									
NAB	3,000,000			3,000,000	0.30%	NAB	AA	Apr 2021	91
<b>Reserve Funds Investments (Cash Backed Reserves)</b>									
CLAG Reserve		266,451		266,451	0.60%	NAB	AA	Mar 2021	122
Workers Compensation Reserve		144,235		144,235	0.60%	NAB	AA	Mar 2021	122
Settlement Agreement Reserve		168,130		168,130	0.60%	NAB	AA	Mar 2021	122
Golf Club Maintenance Reserve		8,860		8,860	0.65%	NAB	AA	Mar 2021	181
Golf Course Cottage Reserve		29,430		29,430	0.65%	NAB	AA	Mar 2021	181
Asset Management Reserve		4,798,260		4,798,260	0.40%	NAB	AA	Mar 2021	90
Refuse Reserve		5,644,890		5,644,890	0.40%	NAB	AA	Mar 2021	90
Plant and Equipment Replacement Reserve		438,640		438,640	0.40%	NAB	AA	Mar 2021	90
Community Services & Emergency Relief Reserve		343,401		343,401	0.33%	NAB	AA	Apr 2021	90
Family Day Care Reserve		1,234,100		1,234,100	0.40%	BEN	AAA	Jun 2021	182
City Infrastructure Reserve		998,535		998,535	0.40%	BEN	AAA	Jun 2021	182
APU Reserve		743,513		743,513	0.40%	BEN	AAA	Jun 2021	182
BP Reserve		25,059		25,059	0.40%	BEN	AAA	Jun 2021	182
Information Technology Reserve		2,106,378		2,106,378	0.40%	BEN	AAA	Jun 2021	182
Renewable Energy Efficiency Reserve		34,286		34,286	0.40%	BEN	AAA	Jun 2021	182
Employee Vacancy Reserve		501,658		501,658	0.40%	NAB	AA	Jun 2021	180
Employee Leave Reserve		2,495,342		2,495,342	0.30%	NAB	AA	Jun 2021	122
<b>Reserve Funds Investments (Developer Contributions)</b>									
DCA - 1 Hard Infrastructure - Bertram		600,664		600,664	0.55%	BEN	AAA	Mar 2021	180
DCA - 2 Hard Infrastructure - Wellard		1,991,997		1,991,997	0.55%	BEN	AAA	Mar 2021	180
DCA 6 - Hard Infrastructure - Mandogalup		274,651		274,651	0.55%	BEN	AAA	Mar 2021	180
DCA - 11 Soft Infrastructure - Wellard East		6,162,219		6,162,219	0.55%	BEN	AAA	Mar 2021	180
DCA - 12 Soft Infrastructure - Wellard West		8,480,802		8,480,802	0.55%	BEN	AAA	Mar 2021	180
DCA - 4 Hard Infrastructure - Anketell		575,926		575,926	0.55%	NAB	AA	Apr 2021	180
DCA 5 - Hard Infrastructure - Wandi		456,267		456,267	0.55%	NAB	AA	Apr 2021	180
DCA 7 - Hard Infra Mandogalup		31,613		31,613	0.55%	NAB	AA	Apr 2021	180
DCA - 8 Soft Infrastructure - Mandogalup		684,193		684,193	0.55%	NAB	AA	Apr 2021	180
DCA - 9 Soft Infrastructure - Wandi/Anketell		11,972,544		11,972,544	0.55%	NAB	AA	Apr 2021	180
DCA - 10 Soft Infrastructure - Casuarina/Anketell		204,782		204,782	0.55%	NAB	AA	Apr 2021	180
DCA - 13 Soft Infrastructure - Bertram		264,079		264,079	0.55%	NAB	AA	Apr 2021	180
DCA - 14 Soft Infrastructure - Wellard/Leda		717,656		717,656	0.55%	NAB	AA	Apr 2021	180
DCA - 15 Soft Infrastructure - Townsite		258,241		258,241	0.55%	NAB	AA	Apr 2021	180
<b>Total</b>	<b>10,831,060</b>	<b>61,393,684</b>	<b>346,524</b>	<b>72,571,268</b>					
	15%	85%	0.5%						
<b>Comprising</b>									
Cash and cash equivalents (Exclude Trust)	7,831,060	19,962,075	0	27,793,135					
Financial assets at amortised cost	3,000,000	41,431,609	0	44,431,609					
Trust	0	0	346,524	346,524					
	<b>10,831,060</b>	<b>61,393,684</b>	<b>346,524</b>	<b>72,571,268</b>					

**KEY INFORMATION**

Cash and cash equivalents include cash on hand, cash at bank, deposits available on demand with banks and other short term highly liquid investments highly liquid investments with original maturities of three months or less that are readily convertible to known amounts of cash and which are subject to an insignificant risk of changes in value.

The local government classifies financial assets at amortised cost if both of the following criteria are met:

- the asset is held within a business model whose objective is to collect the contractual cashflows, and
- the contractual terms give rise to cash flows that are solely payments of principal and interest.

**Note 3(b): Cash and Investments - Compliance with Investment Policy**

Portfolio Credit Risk	Funds Held	Actual at Period End	Limit per Policy	
AAA & Bendigo Bank Kwinana Community Branch	22,652,202	31%	100%	✓
AA	49,915,296	69%	100%	✓
A	-	0%	60%	✓
BBB	-	0%	20%	✓
Unrated	3,770	0%	20%	✓

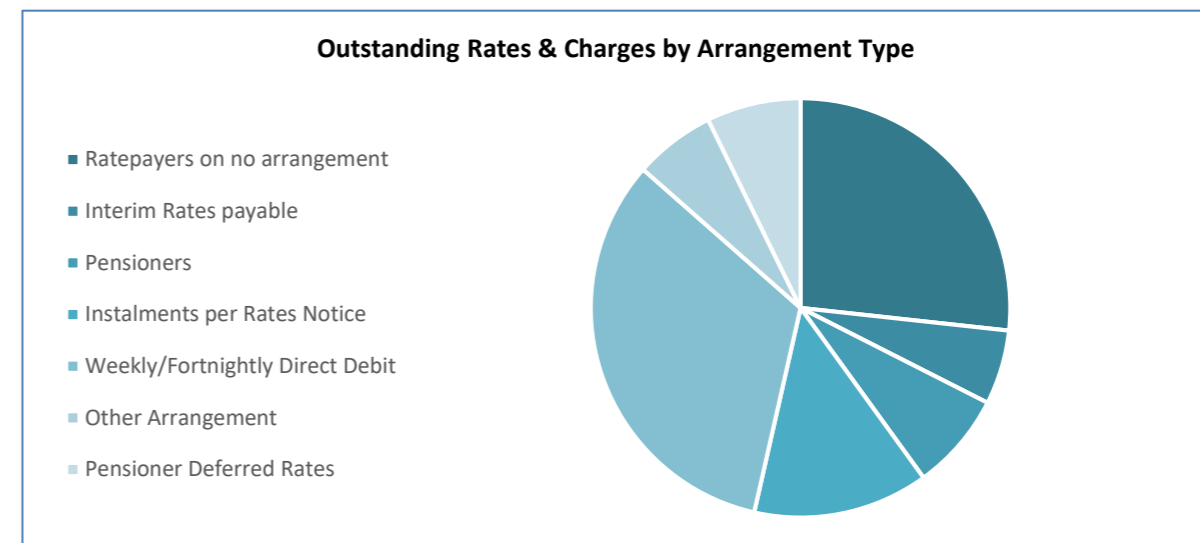
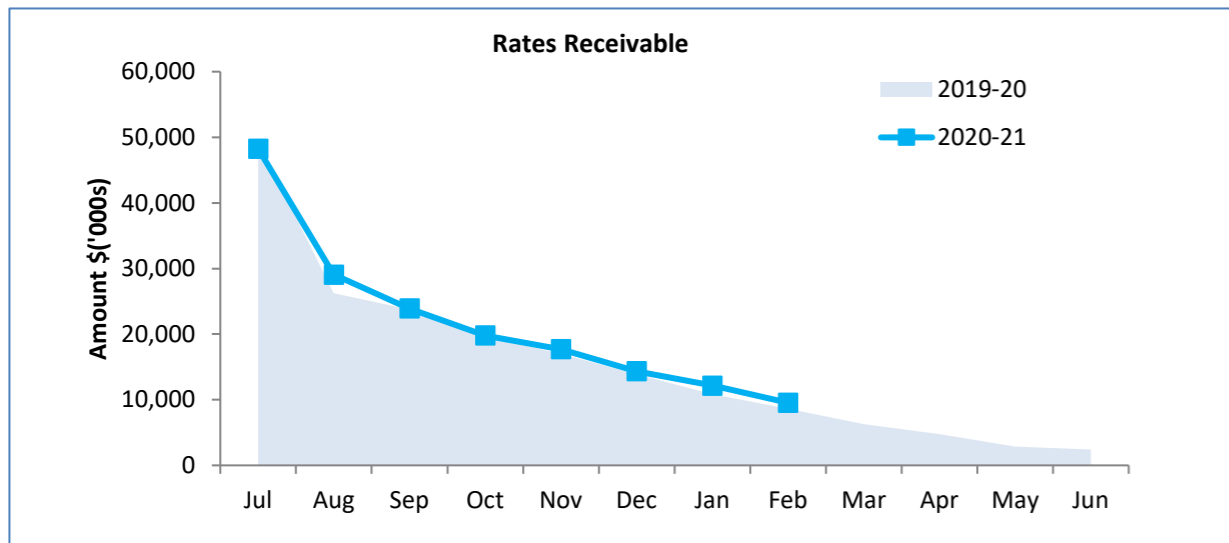
Counterparty Credit Risk	Funds Held	Actual at Period End	Limit per Policy	
BEN (AAA)	22,652,202	31%	45%	✓
NAB (AA)	33,004,598	45%	45%	✓
CBA (AA)	16,910,698	23%	45%	✓
WBC (AA)	-	0%	45%	✓

NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2021

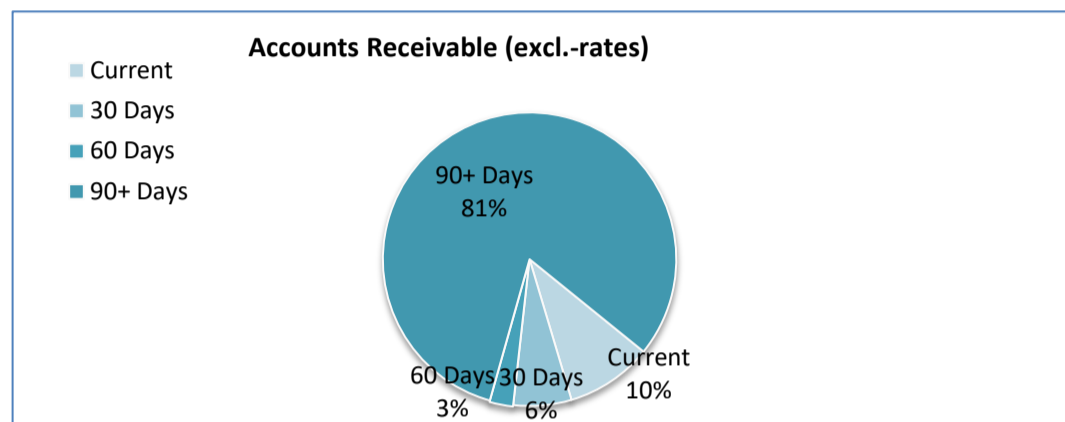
OPERATING ACTIVITIES  
NOTE 4  
RECEIVABLES

Rates receivable	30 June 2020	28 Feb 2020	28 Feb 2021
	\$		\$
Opening arrears previous years	4,576,831	4,576,831	3,148,533
Levied this year	51,941,047	51,798,599	52,136,744
<b>Rates &amp; Charges to be collected</b>	<b>56,517,878</b>	<b>56,375,430</b>	<b>55,285,277</b>
<u>Less</u> Collections to date	(52,169,098)	(46,280,510)	(45,001,023)
<u>Less</u> Pensioner Deferred Rates	(760,126)	(677,069)	(746,740)
<b>Net Rates &amp; Charges Collectable</b>	<b>3,588,655</b>	<b>9,417,852</b>	<b>9,537,515</b>
% Outstanding	6.35%	16.71%	17.25%
<u>Prepaid</u> Rates received (not included above)	(1,198,500)	(781,528)	(709,074)
	4.23%	15.32%	15.97%

Outstanding Rates & Charges by Payment Arrangement Type	28 Feb 2021		
	No. of Assessments	\$	%
Ratepayers on no arrangement	1,165	2,748,958	27%
Interim Rates payable	263	590,945	6%
Pensioners	1,094	776,283	8%
Instalments per Rates Notice	1,765	1,391,110	14%
Weekly/Fortnightly Direct Debit	4,597	3,387,016	33%
Other Arrangement	143	643,203	6%
	<b>9,027</b>	<b>9,537,515</b>	<b>93%</b>
Pensioner Deferred Rates	210	746,740	7%
	<b>9,237</b>	<b>10,284,255</b>	<b>100%</b>



Receivables - general	Current	30 Days	60 Days	90+ Days	Total
<i>Amounts shown below include GST (where applicable)</i>	\$	\$	\$	\$	\$
Sundry receivable	44,889	24,618	7,116	194,827	271,451
Infringements Register	2,570	7,156	5,778	210,060	225,564
<b>Total sundry receivables outstanding</b>	<b>47,459</b>	<b>31,774</b>	<b>12,895</b>	<b>404,887</b>	<b>497,015</b>
<i>Exclude rebates receivable</i>	10%	6%	3%	81%	
Rebates receivable - Rates	620,162	0	0	0	620,162
Rebates receivable - ESL	14,513	0	0	0	14,513
<b>Total general receivables outstanding</b>	<b>682,134</b>	<b>31,774</b>	<b>12,895</b>	<b>404,887</b>	<b>1,131,690</b>



**Sundry Debtors Outstanding Over 90 Days Exceeding \$1,000**

Description	Debtor #	Status	\$
<b>Debts with Fines Enforcement Registry (FER)</b>			
Prosecution Dog Act 1976	2442.07	Registered with FER. Commencing collection of debt.	4,928
Prosecution Planning & Development Act	3859.07	Registered with FER.	16,675
Prosecution Local Government Act 1995	3909.07	Registered with FER.	3,652
Prosecution Dog Act 1976 Dangerous Dog	4131.07	Registered with FER. Debtor making payments to FER. City yet to receive payments due to quantum of fines outstanding.	4,654
Prosecution Local Law Fencing	4233.07	Registered with FER.	2,500
Prosecution Dog Act 1976	4274.07	Registered with FER. Regular payments being received.	4,837
Prosecution Local Law Urban Environment Nuisance - Disrepair Vehicle	4275.07	Registered with FER. Commencing collection of debt.	13,734
Prosecution Dog Act 1976	4387.07	Registered with FER.	10,200
Prosecution Dog Act 1976	4465.07	Registered with FER. Regular payments being received.	5,670
Prosecution Planning & Development Act	4467.07	Registered with FER. Regular payments being received.	13,800
Prosecution Dog Act 1976	4610.07	Registered with FER.	25,598
Prosecution Dog Act 1976	4741.07	Registered with FER.	4,675
Prosecution Planning & Development Act	4885.07	Waiting confirmation registered with FER.	18,485
			<b>129,409</b>
<b>Other Sundry Debtors</b>			
Lease & Outgoings	854.16	Communications in progress between Lease Officer & tenant.	2,953
Local Government Act 1995 abandoned vehicle	3884.03	Defaulted Payment arrangement.	1,155
Shortfall outgoings - as per lease agreement	4822.16	Communications in progress between Lease Officer & tenant.	20,477
<b>Total Debtors 90+ days &gt; \$1,000</b>			<b>153,994</b>

**KEY INFORMATION**

Trade and other receivables include amounts due from ratepayers for unpaid rates and service charges and other amounts due from third parties for goods sold and services performed in the ordinary course of business. Receivables expected to be collected within 12 months of the end of the reporting period are classified as current assets. All other receivables are classified as non-current assets. Collectability of trade and other receivables is reviewed on an ongoing basis. Debts that are known to be uncollectible are written off when identified. An allowance for impairment of receivables is raised when there is objective evidence that they will not be collectible.



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2021

OPERATING ACTIVITIES  
NOTE 5  
DISPOSAL OF ASSETS

Asset description	Current Budget				YTD Actual			
	Net Book Value	Proceeds	Profit	(Loss)	Net Book Value	Proceeds	Profit	(Loss)
	\$	\$	\$	\$	\$	\$	\$	\$
<b>Motor Vehicles</b>								
Plant Replacement - P432	18,000	22,272	4,272	0	0	0	0	0
Plant Replacement - P448	17,350	17,100	0	(250)	0	0	0	0
Plant Replacement - P479	15,500	15,500	0	0	0	0	0	0
Plant Replacement - P483 - Insurance Claim	17,256	13,500	0	(3,756)	17,256	13,500	0	(3,756)
Plant Replacement - P484	15,000	18,900	3,900	0	0	0	0	0
Plant Replacement - P485	19,828	22,273	2,445	0	19,828	22,273	2,445	0
Plant Replacement - P487	19,800	22,273	2,473	0	19,800	22,273	2,473	0
Plant Replacement - P492	22,050	22,050	0	0	0	0	0	0
Plant Replacement - P493	18,000	22,273	4,273	0	18,000	22,273	4,273	0
Plant Replacement - P494	18,000	24,090	6,090	0	18,000	24,091	6,091	0
Plant Replacement - P511	34,137	32,632	0	(1,505)	34,137	32,632	0	(1,505)
Plant Replacement - P515	22,825	18,000	0	(4,825)	0	0	0	0
Plant Replacement - P551	34,289	29,677	0	(4,612)	34,289	29,678	0	(4,611)
Plant Replacement P574 - Insurance Claim	35,668	39,197	3,529	0	35,606	39,696	4,091	0
<b>Plant &amp; Equipment</b>								
Plant Replacement P275 - Trailer Roller	3,931	4,000	69	0	0	0	0	0
Plant Replacement P499 - DECK - Toro Ride on mower	0	0	0	0	0	0	0	0
Plant Replacement P500 - DECK - Toro Ride on mower	0	0	0	0	0	0	0	0
Plant Replacement P531 - DECK - Toro Ride on mower	0	0	0	0	0	0	0	0
Plant Replacement P114 - Tandem Axle Tilt Trailer	2,500	2,000	0	(500)	0	0	0	0
Plant Replacement P120 - Trailer Box top - Coast care	0	0	0	0	0	0	0	0
Plant Replacement P148 Trailer box 7x4 with water tank and Pump	0	0	0	0	0	0	0	0
Plant Replacement P199 Trailer box 7x4 with water tank and Pump	0	0	0	0	0	0	0	0
Plant Replacement P351 Mitsubishi Canter Tip Truck	24,800	25,000	200	0	0	0	0	0
Plant Replacement P392 Mitsubishi Canter Fuso	27,000	25,000	0	(2,000)	0	0	0	0
Plant Replacement P443 Isuzu Tip Truck 6x4	0	0	0	0	0	0	0	0
Plant Replacement P223 Volvo Loader L70	50,000	80,000	30,000	0	0	0	0	0
Plant Replacement P227 Molnar 4 post Hoist	2,580	1,364	0	(1,215)	0	0	0	0
Disposal P350 - KWN5307 Tandem Axle Trailer	5,412	4,403	0	(1,009)	5,412	4,403	0	(1,009)
Disposal P348 - 1TMI191 trailer	2,771	2,199	0	(572)	2,771	2,199	0	(572)
	<b>426,697</b>	<b>463,703</b>	<b>57,251</b>	<b>(20,244)</b>	<b>205,099</b>	<b>213,017</b>	<b>19,371</b>	<b>(11,454)</b>
			<b>Net Profit/(Loss)</b>	<b>37,007</b>			<b>Net Profit/(Loss)</b>	<b>7,918</b>

Capital acquisitions	Adopted Budget	Current Budget	YTD Budget	YTD Actual	YTD Variance
	\$	\$	\$	\$	\$
Land and Buildings	5,231,570	6,004,355	1,018,054	524,827	(493,227)
Plant, Furniture and Equipment	2,344,265	3,015,799	757,828	664,533	(93,295)
Intangible Assets	622,809	622,809	931,301	744,845	(186,456)
Infrastructure - Roads	3,642,090	4,632,810	2,620,160	1,919,212	(700,948)
Infrastructure - Parks & Reserves	994,154	845,835	411,945	399,553	(12,392)
Infrastructure - Footpaths	191,075	699,968	220,638	194,881	(25,757)
Infrastructure - Car Parks	0	0	0	0	0
Infrastructure - Drainage	5,822,737	5,822,737	640,999	596,872	(44,127)
Infrastructure - Bus Shelters	36,630	36,630	6,221	34,995	28,774
Infrastructure - Street Lights	236,505	236,505	166,071	135,196	(30,875)
Infrastructure - Other	0	0	0	0	0
<b>Payments for Capital Acquisitions</b>	<b>19,121,835</b>	<b>21,917,448</b>	<b>6,773,217</b>	<b>5,214,914</b>	<b>(1,558,303)</b>
Right of use assets	553,030	518,969	34,061	0	(34,061)
<b>Total Capital Acquisitions</b>	<b>19,674,865</b>	<b>22,436,417</b>	<b>6,807,278</b>	<b>5,214,914</b>	<b>(1,592,364)</b>
<b>Capital Acquisitions Funded By:</b>					
	\$	\$	\$	\$	\$
Capital grants and contributions	3,112,697	5,152,074	1,007,526	1,053,813	46,287
Borrowings	0	0	0	0	0
Lease liabilities	553,030	518,969	34,061	0	(34,061)
Disposal of Assets	473,350	457,101	309,654	206,415	(103,239)
Cash Backed Reserves	8,684,361	14,781,709	1,086,270	1,043,011	(43,259)
Municipal Funds	6,851,427	1,526,564	4,369,767	2,911,675	(1,458,092)
<b>Capital funding total</b>	<b>19,674,865</b>	<b>22,436,417</b>	<b>6,807,278</b>	<b>5,214,914</b>	<b>(1,592,364)</b>

#### SIGNIFICANT ACCOUNTING POLICIES

All assets are initially recognised at cost. Cost is determined as the fair value of the assets given as consideration plus costs incidental to the acquisition. For assets acquired at no cost or for nominal consideration, cost is determined as fair value at the date of acquisition. The cost of non-current assets constructed by the local government includes the cost of all materials used in the construction, direct labour on the project and an appropriate proportion of variable and fixed overhead. Certain asset classes may be revalued on a regular basis such that the carrying values are not materially different from fair value. Assets carried at fair value are to be revalued with sufficient regularity to ensure the carrying amount does not differ materially from that determined using fair value at reporting date.

Capital Expenditure			Adopted Budget	Current Budget	YTD Budget	YTD Actual	YTD Variance	Comments
			\$	\$	\$	\$	\$	
<b>Buildings</b>								
	13617	Building & Parks Contingency	101,750	101,750	58,920	52,826	(6,095)	
	13634	Administration Building Refurbishment	40,000	190,000	15,887	15,887	0	In progress.
	13636	Animal Care Facility Refurbishment	85,470	85,470	40,000	0	(40,000)	Quotes received January.
	13619	Kwinana South VBFB Station Extensions	376,000	699,600	72,183	7,708	(64,475)	Practical completion estimated Oct-21.
	13620	Kwinana South VBFB Station Upgrade	30,000	62,700	62,700	0	(62,700)	Practical completion estimated Oct-21.
	13621	Mandogalup VBFB Station Extensions	328,159	134,246	134,246	134,246	0	DFES Grant funded. Construction completed August.
	13622	Mandogalup VBFB Station Upgrade	25,000	26,302	1,600	1,600	0	Construction completed August.
	13849	Mandogalup VBFB - Repair storage room cracked wall	0	5,000	5,000	0	(5,000)	LRCI Projects.
	13940	Koorliny Arts Centre - air conditioner replacement	0	8,500	0	0	0	LRCI Project Funding Round 2.
	13616	Recquatic Front Counter - Automated Gates	90,000	90,000	0	0	0	
	13873	Recquatic - Swim School Office Relocation	0	10,400	10,400	9,820	(580)	Project complete.
	13640	Recquatic Resurface Floor	7,875	7,855	7,855	375	(7,480)	
	13641	Recquatic Spa balance tank lid and ladder	8,400	8,400	400	0	(400)	
	13642	Recquatic 25m backwash tank lid Replacement	7,350	0	350	0	(350)	
	13643	Recquatic Centre - Internal paint public areas	0	8,000	0	0	0	LRCI Projects.
	13834	Recquatic Centre - Entrance area external walls repaint	0	25,000	25,000	0	(25,000)	LRCI Projects.
	13835	Recquatic Centre - Grout renewal - change rooms replace damaged grout	0	25,000	0	0	0	LRCI Projects.
	13944	Recquatic Centre - front admin east and reception area airconditioner replacement	0	67,000	0	0	0	LRCI Project Funding Round 2.
	13627	Darius Wells Stormwater System	27,300	27,300	1,300	1,300	0	Specification complete, quoting in progress.
	13629	Darius Wells Roof Fall Arrest System	21,000	21,000	21,000	1,000	(20,000)	Works planned for February.
	13836	Darius Wells Knowledge Centre - Roof leak repair	0	3,500	3,500	0	(3,500)	LRCI Projects. In progress.
	13630	Darius Wells Automate Disabled Toilet Doors	21,000	26,000	26,000	25,988	(12)	Project complete.
	13632	Darius Wells Room Conversion	9,666	9,666	0	0	0	To be completed by end of May 2021.
	13937	Darius Wells electrical works	0	80,000	0	0	0	LRCI Project Funding Round 2.
	13938	Darius Wells - Ken Jackman Hall flooring	0	45,000	0	0	0	LRCI Project Funding Round 2.
	13623	John Wellard Community Centre External Paint	6,300	6,300	6,300	1,550	(4,750)	LRCI Projects. In progress.
	13628	John Wellard Community Centre Stormwater System	22,050	12,382	12,382	12,382	0	Project complete.
	13838	John Wellard Community Centre - Paving stain cleaning/repaint due to vandalism	0	10,000	10,000	9,890	(110)	LRCI Projects. Project complete.
	13839	John Wellard Community Centre - Wall Finishes - meeting rooms repaint	0	7,000	7,000	5,020	(1,980)	LRCI Projects. Project complete.
	13840	John Wellard Community Centre - Wisteria room carpet replacement	0	15,000	15,000	0	(15,000)	LRCI Projects.
	13939	John Wellard Community Centre - Repair solar panel connections	0	10,000	0	0	0	LRCI Project Funding Round 2.
	13846	William Bertram Community Centre - External cracked walls repairs/metal fencing repaint	0	7,000	7,000	0	(7,000)	LRCI Projects. In progress.
	13847	William Bertram Community Centre - External walls repaint in the playground area	0	7,000	7,000	1,380	(5,620)	LRCI Projects. In progress.
	13848	William Bertram Community Centre - Rusted gutters restoration close to playground area	0	7,000	0	0	0	LRCI Projects. In progress.
	13850	Zone Youth Centre - Roof leak repair	0	10,000	0	0	0	LRCI Projects.
	13943	Zone Youth Centre - repaint basketball court	0	30,000	0	0	0	LRCI Project Funding Round 2.
	13633	Sloans Cottage Accessibility Works	26,709	12,709	12,709	11,000	(1,709)	Engaging heritage architect.
	13644	Sloans Heritage (Caretaker) Cottage Accessibility Works	20,700	64,700	25,350	350	(25,000)	Engaging heritage architect.
	13624	Parmelia House Roof Renovation	47,775	15,002	15,002	15,002	0	Project complete. Budget savings transferred to Kwinana South VBFB Station upgrade.
	13842	Medina Oval Changerooms - Terracing/seating/accessibility	0	15,000	15,000	0	(15,000)	LRCI Projects. Works planned for January.

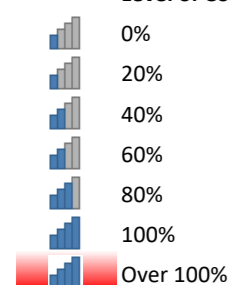
Capital Expenditure	Adopted Budget	Current Budget	YTD Budget	YTD Actual	YTD Variance	Comments
	\$	\$	\$	\$	\$	
13626 Medina Oval Toilet Refurbishment	26,250	26,250	26,250	1,250	(25,000)	
13941 Medina Oval Spectators stand and shelter	0	20,000	0	0	0	LRCI Project Funding Round 2.
13841 Medina Hall - Isabela Corker wing - lift, dispose, install and underlay carpet	0	4,000	4,000	0	(4,000)	LRCI Projects.
13814 Medina Centre - CCTV, lighting and WIFI upgrades	0	10,007	8,757	10,007	1,250	\$10k cfwd from 2019/20. Grant funded.
13631 Calista Oval Tennis Clubrooms	137,363	137,363	137,363	122,764	(14,599)	Project complete.
13635 Rhodes Park Accessible Toilet Design	15,750	0	0	0	0	Brief in progress.
13637 Challenger Beach Ablutions Structural Renewal	10,500	0	500	0	(500)	Assessment in progress.
13618 DCA 9 Local Sportsground Clubroom Honeywood Primary School Oval	3,718,203	3,718,203	164,350	82,483	(81,867)	Project on hold during community engagement process.
13639 Wandi Resource Centre Asbestos Removal Program	15,750	15,750	15,750	750	(15,000)	Quotes received.
13837 Goldney Velodrome BMX Track Kiosk & Ablution - External wall repaint/paving restoration	0	12,000	12,000	0	(12,000)	LRCI Projects.
13843 Thomas Oval Electrical Compound - External walls cleaning/renewal and painting	0	4,500	4,500	0	(4,500)	LRCI Projects.
13942 Thomas Oval Pavilion (old) roof restoration	0	20,000	0	0	0	LRCI Project Funding Round 2.
13844 Wellard Pavilion - External wall finishes.	0	10,000	10,000	0	(10,000)	LRCI Projects.
13845 Wells Park public toilet - External brick wall and paving cleaning/repaint	0	10,000	10,000	0	(10,000)	LRCI Project Funding Round 2.
13638 18 Maydwell Way Asbestos Removal Program	5,250	5,500	5,500	250	(5,250)	In progress.
13856 18 Maydwell Way - Window screen renewal	0	7,000	0	0	0	In progress.
13855 Magenup Equestrian Centre - Stable renovation	0	7,000	0	0	0	Works planned for March.
<b>Buildings Total</b>	<b>5,231,570</b>	<b>6,004,355</b>	<b>1,018,054</b>	<b>524,827</b>	<b>(493,227)</b>	

Capital Expenditure	Adopted Budget	Current Budget	YTD Budget	YTD Actual	YTD Variance	Comments
	\$	\$	\$	\$	\$	
<b>Plant, Furniture and Equipment</b>						
<b>Furniture and Equipment</b>						
13660 Library - self returns shelves	12,355	12,355	12,305	12,305	0	Purchase complete.
13661 Library - self check out touchscreen computer & workstation	7,000	7,000	7,000	7,046	46	Purchase complete.
13662 Library - self check out stations for Community Centres	25,438	25,438	0	0	0	
13825 Replacement of Mayoral Chains	0	10,540	0	0	0	Carried forward from 2019/20.
13900 Folding/insertion machine with software for Rates	0	17,505	17,505	17,505	0	Purchase complete.
<b>Computing Equipment</b>						
13646 City Website Redevelopment Stage 3 of 3	122,860	122,860	60,759	53,375	(7,384)	In progress.
13647 Computing Equipment as required	25,438	25,438	8,480	0	(8,480)	
13648 Corporate Business System Implementation	1,519,953	2,139,748	870,542	691,470	(179,072)	In progress.
<b>Plant and Equipment</b>						
13606 Plant Replacement P275 Trailer Roller	23,500	21,000	21,000	18,516	(2,484)	Purchase complete.
13663 Plant Replacement P499 DECK - Toro Ride on mower	5,500	5,440	5,440	5,440	0	Purchase complete.
13664 Plant Replacement P500 DECK - Toro Ride on mower	5,500	5,440	5,440	5,440	0	Purchase complete.
13665 Plant Replacement P531 DECK - Toro Ride on mower	5,500	5,440	5,440	0	(5,440)	
13666 Plant Replacement P114 Tandem Axle Tilt Trailer	15,000	15,000	0	0	0	
13667 Plant Replacement P120 Trailer Box top - Coastcare	3,500	3,500	0	0	0	
13668 Plant Replacement P148 Trailer box 7x4 with water tank and Pump	10,500	9,200	9,200	9,200	0	Purchase complete.
13669 Plant Replacement P199 Trailer box 7x4 with water tank and pump	10,500	10,150	10,150	9,950	(200)	Purchase complete.
13670 Plant Replacement P351 Mitsubishi Canter Tip Truck	135,000	140,000	0	0	0	
13671 Plant Replacement P392 Mitsubishi Canter Fuso	95,000	100,000	0	0	0	
13672 Plant Replacement P443 Isuzu Tip Truck 6x4	150,000	0	0	0	0	
13673 Plant Replacement P223 Volvo Loader L70	260,000	260,000	0	0	0	
13905 Plant Replacement P227 Molnar 4 post Hoist	0	18,541	18,541	0	(18,541)	
13674 Traffic Management - Traffic Equipment	26,530	26,530	22,491	22,491	0	Purchase complete.
13833 Recquatic Pool Upgrades	0	30,500	30,500	30,500	0	Purchase complete.
13903 Outdoor toxic substance unit PR850-6	0	6,081	6,081	6,080	(1)	Purchase complete.
<b>Motor Vehicles</b>						
13886 Plant Replacement P551	43,000	32,117	32,117	32,117	0	Purchase complete.
13686 Plant Replacement P483	27,500	25,978	25,978	25,978	0	Purchase complete.
13693 Plant Replacement P511	43,000	42,629	42,629	42,629	0	Purchase complete.
13696 Plant Replacement P515	35,000	34,826	34,826	34,826	0	Purchase complete.
13697 Plant Replacement P492	27,500	27,069	27,069	27,069	0	Purchase complete.
13694 Plant Replacement P448	42,000	46,357	46,408	45,635	(773)	Purchase complete.
13687 Plant Replacement P479	40,000	45,066	45,066	44,759	(307)	Purchase complete.
13688 Plant Replacement P484	45,000	48,192	48,192	47,699	(493)	Purchase complete.
13695 Plant Replacement P485	40,000	39,971	39,971	39,971	(0)	Purchase complete.
13685 Plant Replacement P487	40,000	42,641	42,641	42,641	(0)	Purchase complete.
13689 Plant Replacement P493	40,000	41,234	41,234	41,234	(0)	Purchase complete.
13690 Plant Replacement P494	40,000	45,855	45,855	45,182	(673)	Purchase complete.
13691 Plant Replacement P432	45,000	42,180	42,180	41,571	(609)	Purchase complete.
13895 Additions to P587 - Bar lights and recovery points	0	9,090	9,090	8,750	(340)	Purchase complete.
13907 Plant Replacement P574 (Insurance replacement)	0	39,697	0	0	0	
13908 Plant replacement P478	0	55,000	55,000	0	(55,000)	Replacing previously leased FDC vehicle.
13964 Deposits paid to secure 2021/2022 vehicles	0	3,000	0	0	0	
<b>Plant, Furniture and Equipment Total</b>	<b>2,967,074</b>	<b>3,638,608</b>	<b>1,689,129</b>	<b>1,409,378</b>	<b>(279,751)</b>	

Capital Expenditure	Adopted Budget	Current Budget	YTD Budget	YTD Actual	YTD Variance	Comments
	\$	\$	\$	\$	\$	
<b>Park and Reserves</b>						
13675 KIA Street Tree Planting Program	76,313	76,313	3,634	3,634	0	Design completed.
13676 Kwinana Loop Trail	10,000	10,608	10,608	10,608	(0)	Carried forward from 2019/20. Project complete.
13677 Thomas Oval Lighting	413,361	278,754	278,754	260,298	(18,456)	Project complete, awaiting invoices.
13678 Bertram Street Tree Planting Program	111,925	111,925	5,330	5,330	0	Design complete.
13679 Parks for People Strategy Ascot Park	145,000	1,320	1,320	1,320	0	Design endorsed by community.
13680 Parks for People Strategy Peace Park	10,000	10,000	10,000	11,528	1,528	Project complete.
13888 Parks for People Strategy Gemstone Park	0	129,360	1,800	1,800	0	Tree assessment complete. Quoting in progress.
13682 POS / Parks & Reserves Renewals Bores, Fencing, Irrigation	227,555	227,555	100,499	105,035	4,536	
<b>Parks and Reserves Total</b>	<b>994,154</b>	<b>845,835</b>	<b>411,945</b>	<b>399,553</b>	<b>(12,392)</b>	
<b>Roads</b>						
<b>Urban Road Grant Construction</b>						
13704 Wellard Road Duplication Project MRRG / DCA Funded	282,905	139,908	15,502	15,502	(0)	Design and drafting in progress. Landscaping tender completed.
13705 Road Reseal Renewals MRRG Ocean Street	345,200	345,200	345,200	319,293	(25,907)	Works completed.
13706 Road Reseal Renewals MRRG Mandurah Road SB1	188,300	187,800	187,800	138,909	(48,891)	Works completed.
13710 Road Reseal Renewals MRRG Mandurah Road NB 2	64,800	64,800	64,800	62,014	(2,786)	Works completed.
13707 Road Reseal Renewals MRRG Cockburn Road NB	46,400	46,400	46,400	46,224	(176)	Works completed.
13709 Road Reseal Renewals MRRG Leath Road 1	99,300	99,300	99,300	95,366	(3,934)	Works completed.
13711 Road Reseal Renewals MRRG Leath Road 2	49,700	49,700	49,700	71,343	21,643	Works completed.
13708 Road Reseal Renewals MRRG Leath Road 3	395,300	395,300	395,300	368,861	(26,439)	Works completed.
13712 Road Reseal Renewals MRRG Mason Rd	211,701	211,701	211,701	173,386	(38,315)	Works completed.
<b>Black Spot Grant Construction</b>						
13703 Gilmore Avenue Pedestrian Crossing	262,073	258,273	258,273	245,965	(12,308)	Works complete, awaiting invoices.
13887 Meares Road Challenger Avenue roundabout pre-deflection treatment	0	110,000	0	2,580	2,580	In progress.
<b>Roads to Recovery Grant Construction</b>						
13700 Road Reseal Renewals RTR Pace Road	275,000	275,000	275,000	222,695	(52,305)	In progress.
13701 Road Reseal Renewals RTR Orton Road	174,000	174,000	15,818	15,818	0	Works planned for April.
13702 Road Reseal Renewals RTR Milton Place	77,000	77,000	7,000	7,000	0	Works planned for March.
<b>DCA Funded Construction</b>						
13713 DCA 5 - Lyon Road - Cassowary to Kenby	437,250	437,250	437,250	0	(437,250)	
13714 DCA 2 Millar Road Chg 470 to 577. Developer to complete	293,983	293,983	0	0	0	
13715 DCA 2 Sunrise Boulevard Internal Collector Road (b) Lot 59 - (Developer to complete)	351,079	351,079	0	0	0	
<b>LRCI Projects</b>						
13853 Stefanelli Close - Road Resurfacing	0	75,000	75,000	28,578	(46,422)	LRCI Projects. In progress.
13854 Wandri Drive - Road Resurfacing	0	85,000	85,000	79,870	(5,130)	LRCI Projects. Works complete, awaiting invoices.
13958 Sicklemore Road, Parmelia reconstruction. Strip spray seal, remove roots, backfill with roadbase compact, asphalt. Reconstruct kerbs. Construct 2.5m red asphalt shared path.	0	860,000	0	0	0	LRCI Project Funding Round 2. Construction to begin June.
13959 Summerton Road, Calista Traffic signals - Pedestrian crossing upgrade.	0	45,000	0	0	0	LRCI Project Funding Round 2. Design issued for review.
<b>Municipal Road Construction</b>						
13698 Traffic Management - Breccia Parade speed calming	6,699	0	0	0	0	Project cancelled.
13699 General Traffic Management - Traffic calming	81,400	51,116	51,116	25,807	(25,309)	
<b>Roads Total</b>	<b>3,642,090</b>	<b>4,632,810</b>	<b>2,620,160</b>	<b>1,919,212</b>	<b>(700,948)</b>	

Capital Expenditure	Adopted Budget	Current Budget	YTD Budget	YTD Actual	YTD Variance	Comments
	\$	\$	\$	\$	\$	
<b>Street Lighting</b>						
13683 Street Lighting - New - Leath Road	208,525	208,525	164,713	129,425	(35,288)	Project complete.
13684 Street Lighting Upgrade reactive work street light requests	27,980	27,980	1,358	5,771	4,413	
<b>Street Lighting Total</b>	<b>236,505</b>	<b>236,505</b>	<b>166,071</b>	<b>135,196</b>	<b>(30,875)</b>	
<b>Bus Shelter Construction</b>						
13645 Bus Shelter Upgrades	36,630	36,630	6,221	34,995	28,774	Civil works completed.
<b>Bus Shelter Construction Total</b>	<b>36,630</b>	<b>36,630</b>	<b>6,221</b>	<b>34,995</b>	<b>28,774</b>	
<b>Footpath Construction</b>						
13657 Footpath Renewals	50,875	50,875	35,351	39,305	3,954	
13658 Gilmore Avenue Shared Path Stage 1 Design (Perth Bicycle Network LG Grants Program)	81,400	81,400	16,900	16,900	0	Design in progress.
13659 Parmelia Avenue Shared Path Stage 1 Design (Perth Bicycle Network LG Grants Program)	58,800	58,800	15,800	15,800	0	Design in progress.
13821 Breccia Parade footpath	0	3,590	3,590	3,590	0	Carried forward from 2019/20. Project complete.
13851 Barwell Road - Construction of 1.5m wide concrete footpath	0	33,997	33,997	33,997	0	LRCI Funding. Project complete.
13852 Bilya Gardens - Construction of 2m wide concrete footpath	0	115,000	115,000	85,288	(29,712)	LRCI Funding. Project complete.
13945 Gamble Place, Orelia - Install new footpath and widen the cul-de sac.	0	30,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13946 Joiner Place, Parmelia - Install new footpath and widen the cul-de sac.	0	30,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13947 Hunt Place (with Cowling Way), Parmelia - New 1.5m footpath construction.	0	20,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13948 Cowling Way (with Hunt Place ), Parmelia - New 1.5m footpath construction.	0	50,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13949 Porter Garden, Leda - New 1.5m footpath construction.	0	60,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13950 Pudney Place, Orelia - New 1.5m footpath construction.	0	20,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13951 Napoleon Way, Bertram - New 1.5m footpath construction.	0	16,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13952 Warrior Pass, Bertram - New 1.5m footpath construction.	0	18,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13953 Tranby Way, Bertram - New 1.5m footpath construction.	0	25,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13954 Ameer Cres, Bertram - New 1.5m footpath construction	0	23,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13955 Trust Way West, Bertram - New 1.5m footpath construction.	0	14,306	0	0	0	LRCI Project Funding Round 2. Design in progress.
13956 Roach Place, Orelia - New 1.5m footpath construction.	0	20,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
13957 Sub N drain path - Sunrise/Living Edge - New 2m footpath construction.	0	30,000	0	0	0	LRCI Project Funding Round 2. Design in progress.
<b>Footpath Construction Total</b>	<b>191,075</b>	<b>699,968</b>	<b>220,638</b>	<b>194,881</b>	<b>(25,757)</b>	
<b>Drainage Construction</b>						
13651 Drainage - DCA 2 Peel Sub N Drain - Lot 64 Woolcoat Road & Lot 379	1,093,205	1,093,205	251,407	271,166	19,759	Carried forward from 2019/20. Project complete.
13652 Drainage - DCA 2 - Peel Sub N1 Drain - Lot 378 & 90	334,000	334,000	0	0	0	Completed prior year.
13653 DCA1 Stormwater Management Infrastructure	572,473	572,473	16,170	23,862	7,692	Draft design issued, under tender.
13654 DCA 3 Peel Sub P1 Drain Casuarina	1,164,627	1,164,627	0	0	0	
13655 DCA 3 Peel Sub P1A Drain Casuarina	1,098,363	1,098,363	0	0	0	
13656 DCA 3 Peel Sub P Drain Anketell South & Casuarina	1,186,647	1,186,647	0	0	0	
13649 Drainage New Ocean Street	212,850	212,850	212,850	161,968	(50,882)	Works complete, awaiting invoices.
13650 Drainage New Pace Road	160,572	160,572	160,572	139,876	(20,696)	In progress.
<b>Drainage Construction Total</b>	<b>5,822,737</b>	<b>5,822,737</b>	<b>640,999</b>	<b>596,872</b>	<b>(44,127)</b>	
<b>Capital Expenditure Total</b>	<b>19,121,835</b>	<b>21,917,448</b>	<b>6,773,217</b>	<b>5,214,914</b>	<b>(1,558,303)</b>	

Level of Completion Indicators (Percentage YTD Actual to Annual Budget)



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2021

FINANCING ACTIVITIES  
NOTE 7  
BORROWINGS

Repayments - borrowings

Information on borrowings

Particulars	Finalisation of Loan	1 July 2020	New Loans		Principal Repayments		Principal Outstanding		Interest Repayments	
			Actual	Current Budget	Actual	Current Budget	Actual	Current Budget	Actual	Current Budget
		\$	\$	\$	\$	\$	\$	\$	\$	
<b>Governance</b>										
Loan 99 - Administration Building Renovations	2024/25	576,328	0	0	49,985	101,532	526,343	474,796	19,522	38,476
<b>Education and welfare</b>										
Loan 96 - Youth Specific Space	2022/23	95,162	0	0	14,432	29,407	80,730.36	65,755	3,687	7,290
Loan 100 - Youth Specific Space	2027/28	1,270,494	0	0	66,407	134,365	1,204,086	1,136,129	33,076	66,272
<b>Recreation and culture</b>										
Loan 94 - Wellard Sports Pavilion	2021/22	108,836	0	0	25,941	52,709	82,895	56,127	2,807	6,728
Loan 95 - Orelia Oval Pavilion	2022/23	228,389	0	0	34,636	70,576	193,753	157,813	8,848	17,497
Loan 97 - Orelia Oval Pavilion Extension	2024/25	1,275,247	0	0	110,602	224,660	1,164,645	1,050,587	43,196	85,135
Loan 102 - Library & Resource Centre	2028/29	6,820,176	0	0	310,973	629,005	6,509,203	6,191,171	173,003	347,778
Loan 104 - Recquatic Refurbishment	2029/30	3,350,000	0	0	137,532	277,849	3,212,468	3,072,151	77,797	155,846
Loan 105 - Bertram Community Centre	2029/30	1,269,272	0	0	55,809	112,530	1,213,463	1,156,742	24,199	48,147
Loan 106 - Destination Park - Calista	2030/31	1,321,267	0	0	50,747	102,291	1,270,519	1,218,976	24,605	49,749
<b>Transport</b>										
Loan 98 - Streetscape Beautification	2024/25	778,043	0	0	67,479	137,066	710,563	640,977	26,354	51,943
Loan 101 - City Centre Redevelopment	2021/22*	2,500,000	0	0	0	0	2,500,000	2,500,000	23,588	79,250
<b>B/Fwd Balance</b>		19,593,213	0	0	924,543	1,871,990	18,668,669	17,721,223	460,682	954,111
<b>Self supporting loans</b>										
<b>Recreation and culture</b>										
Loan 103B - Golf Club Refurbishment	2031/32	249,972	0	0	8,563	17,269	241,409	232,703	4,483	9,869
		249,972	0	0	8,563	17,269	241,409	232,703	4,483	9,869
**Share of SMRC Loan		425,520	0	0	0	0	425,520	425,520	0	0
<b>Total</b>		20,268,705	0	0	933,107	1,889,259	19,335,598	18,379,446	465,165	963,980
Current borrowings		1,889,259					2,314,779			
Non-current borrowings		18,379,446					17,020,819			
		20,268,705					19,335,598			

\*City Centre Redevelopment loan expected to be refinanced upon maturity in 2021/22, as the City awaits reimbursement from the State Government.

\*\* Current portion of borrowings includes \$425,520, which represents the City's share of the Southern Metropolitan Regional Council (SMRC) Administration Building loan with the WATC.



NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2021

OPERATING ACTIVITIES  
NOTE 9  
CASH RESERVES

Cash backed reserve

Reserve name	Opening Balance	Budget Interest Earned	Actual Interest Earned	Budget Transfers In (+)	Actual Transfers In (+)	Budget Transfers Out (-)	Actual Transfers Out (-)	Budget Closing Balance	Actual Closing Balance
<b>Municipal Reserves</b>	\$	\$	\$	\$	\$	\$	\$	\$	\$
Aged Persons Units Reserve	741,044	4,143	2,467	195,761	0	(190,000)	0	750,948	743,511
Asset Management Reserve	5,163,247	46,994	41,165	1,725,401	0	(2,014,379)	(579,114)	4,921,263	4,625,299
Banksia Park Reserve	23,997	151	93	221,902	0	(136,050)	0	110,000	24,090
City Infrastructure Reserve	995,255	5,530	3,279	360,000	0	0	0	1,360,785	998,535
Community Services & Emergency Relief Reserve	340,995	3,535	2,888	0	0	(255,000)	(5,000)	89,530	338,884
Contiguous Local Authorities Group Reserve	265,918	1,593	533	32,941	0	(28,420)	0	272,032	266,451
Employee Leave Reserve	2,488,383	11,387	0	458,300	0	0	0	2,958,070	2,488,383
Employee Vacancy Reserve	500,233	1,872	1,426	0	0	0	0	502,105	501,659
Family Day Care Reserve	1,229,903	7,075	4,196	0	0	(71,110)	0	1,165,868	1,234,099
Golf Course Cottage Reserve	29,396	174	44	0	0	0	0	29,570	29,440
Golf Club Maintenance Reserve	8,846	52	13	5,004	0	(5,004)	0	8,898	8,859
Information Technology Reserve	2,597,169	11,967	8,040	400,000	0	(2,153,806)	(498,831)	855,330	2,106,378
Plant and Equipment Replacement Reserve	430,899	2,164	1,235	424,506	355,350	(671,657)	(121,726)	185,912	665,759
Public Arts Reserve	0	0	0	29,900	29,900	0	0	29,900	29,900
Refuse Reserve	5,644,888	19,713	0	0	0	(298,954)	0	5,365,647	5,644,888
Renewable Energy Efficiency Reserve	34,172	193	114	2,035	0	0	0	36,400	34,286
Restricted Grants & Contributions Reserve	2,187,985	0	0	400,000	0	(2,150,481)	(409,250)	437,504	1,778,735
Settlement Agreement Reserve	167,771	1,027	358	0	0	0	0	168,798	168,129
Workers Compensation Reserve	143,928	881	307	180,000	0	0	0	324,809	144,235
<b>Sub-Total Municipal Reserves</b>	<b>22,994,028</b>	<b>118,451</b>	<b>66,159</b>	<b>4,435,750</b>	<b>385,250</b>	<b>(7,974,861)</b>	<b>(1,613,920)</b>	<b>19,573,368</b>	<b>21,831,518</b>
<b>Developer Contribution Reserves</b>									
DCA 1 - Hard Infrastructure - Bertram	599,722	3,127	828	660,537	660,537	(609,455)	0	653,931	1,261,087
DCA 2 - Hard Infrastructure - Wellard	1,988,294	10,875	3,255	16,847	16,847	(2,095,102)	0	(79,086)	2,008,396
DCA 3 - Hard Infrastructure - Casuarina	0	0	0	3,472,228	0	(3,472,228)	0	0	0
DCA 4 - Hard Infrastructure - Anketell	521,489	3,308	1,412	616,222	686,769	(22,591)	0	1,118,428	1,209,670
DCA 5 - Hard Infrastructure - Wandi	427,218	2,669	1,169	561,924	561,924	(460,231)	0	531,580	990,310
DCA 6 - Hard Infrastructure - Mandogalup	274,056	1,575	524	297,311	297,311	(22,591)	0	550,351	571,890
DCA 7 - Hard Infrastructure - Wellard West	18,172	153	47	32,989	31,989	(22,954)	0	28,360	50,208
DCA 8 - Soft Infrastructure - Mandogalup	450,501	3,499	1,248	858,335	859,335	(23,902)	0	1,288,433	1,311,085
DCA 9 - Soft Infrastructure - Wandi/Anketell	11,941,274	70,651	31,270	973,352	980,353	(3,342,105)	0	9,643,172	12,952,897
DCA 10 - Soft Infrastructure - Casuarina/Anketell	204,182	1,275	599	0	0	(24,774)	0	180,683	204,782
DCA 11 - Soft Infrastructure - Wellard East	6,152,977	31,695	8,125	307,660	307,660	(25,662)	0	6,466,670	6,468,761
DCA 12 - Soft Infrastructure - Wellard West	8,468,096	43,608	11,170	988,378	988,378	(24,362)	0	9,475,720	9,467,644
DCA 13 - Soft Infrastructure - Bertram	263,326	1,623	754	0	0	(24,216)	0	240,733	264,079
DCA 14 - Soft Infrastructure - Wellard/Leda	706,921	4,283	1,922	419,728	118,145	(24,151)	0	1,106,781	826,988
DCA 15 - Soft Infrastructure - City Site	239,431	1,537	688	58,945	69,001	(24,151)	0	275,762	309,121
<b>Sub-Total Developer Contribution Reserves</b>	<b>32,255,658</b>	<b>179,878</b>	<b>63,011</b>	<b>9,264,456</b>	<b>5,578,248</b>	<b>(10,218,475)</b>	<b>0</b>	<b>31,481,517</b>	<b>37,896,917</b>
<b>Total Reserves</b>	<b>55,249,686</b>	<b>298,329</b>	<b>129,171</b>	<b>13,700,206</b>	<b>5,963,498</b>	<b>(18,193,336)</b>	<b>(1,613,920)</b>	<b>51,054,885</b>	<b>59,728,435</b>

Operating grants, subsidies and contributions Provider	Unspent Grant Liability	Current Budget Revenue	Budget YTD Revenue	Actual YTD Revenue	Comments
	\$	\$		\$	
<b>Governance</b>					
Local Government General Purpose Grant		503,192	377,394	377,394	Qtr 1 instalment received, higher than budgeted.
Local Government General Purpose Grant - Roads		392,264	294,198	294,197	Qtr 1 instalment received, higher than budgeted.
Non Rateable Property - Dampier to Bunbury Natural Gas Pipeline Corridor		177,000	0	0	
<b>Law, order, public safety</b>					
Department Fire and Emergency Services - ESL	0	141,129	94,086	141,129	First instalment of 2021 grant received early in June 20.
Department Fire and Emergency Services - RCTI ESG 1819 & 1920	0	28,646	28,646	38,110	DFES Operating Grant from FY19 received July.
Western Australian Local Government (WALGA)	0	0	2,000	2,000	Review & development of plan for animal welfare in emergencies.
Mitigation Activity Fund Grant 20/21 Round 2	0	15,251	7,626	7,626	
<b>Health</b>					
Mosquito Management Contributions (CLAG)	0	32,941	30,477	33,907	
Department of Health - Larvicide	0	3,339	1,839	0	
<b>Education and welfare</b>					
Banksia Park Operating Cost Contribution	0	346,320	230,880	230,880	
Family Daycare - Mainstream Childcare Benefit Subsidy		2,677,419	1,767,807	1,683,504	
Family Daycare - Subsidy Other		200,284	200,284	260,678	
Family Daycare - Operational Subsidy - Dept of Communities NW04		0	0	533	
Family Daycare - Inclusion Subsidy Scheme		5,160	1,720	0	
In-Home Care - CCB Subsidy		848,620	593,733	601,438	
In-Home Care - Subsidy - Department of Communities		34,277	34,277	49,968	
Family Day Care Sustainability Grant	80,904	143,264	62,360	31,180	
NGALA My Time Program	0	10,560	7,920	7,920	
Operational Subsidy - Aboriginal Resource Worker		31,500	31,500	30,514	
Library Contributions and Donations		166	106	79	
Youth Social Justice Program	0	178,760	134,070	134,071	
Youth Leadership and Development LYRIK	0	20,000	0	0	
Youth Intervention Program	0	20,000	0	0	
Youth Spaces Activation	0	5,000	0	0	
CSIRO - Indigenous STEM awards event recognition		0	0	700	
<b>Community amenities</b>					
PTA Bus Shelter Subsidy		7,000	0	0	
SMCC - KIC Coastcare in the KIA		0	0	0	
SMCC - BP Coastcare		0	0	0	
SMCC - Perth Region NRM Kleenheat Project		0	0	0	
SMCC - Tronox Adopt a Beach		0	0	0	
SMCC - Suez/ProAlliance Adopt a Beach		0	0	0	
Greening fund	62,853	40,000	0	0	\$100k Grant received in 2018. 3year funding to 2021.
Bin Tagging Funding	0	22,000	0	0	
Sustainability Workshops	0	1,455	0	0	
Grow it Local	0	25,000	0	0	
<b>Recreation and culture</b>					
Shared Use Agreements	0	119,963	105,844	104,707	
Stay on Your Feet Program - Injury Matters	3,230	3,230	0	0	
Australia Day branding grant	0	1,000	1,000	1,000	
Good Things Foundation Grant - Get Online Week	0	1,000	1,000	1,000	
KIC Sponsorship 2020 Lollyrun		10,000	10,000	10,000	
Community Development Fund - Kwinana Industries Council		20,000	20,000	20,000	Kwinana Community Chest.
OMG Kwinana Fringe Festival		72,636	72,636	63,636	DLGSC & Fremantle Ports
<b>Transport</b>					
Main Roads Annual Direct Grant		191,745	191,745	191,745	
Main Roads Street Light Subsidy		6,000	0	0	
Main Roads Maintenance Contribution		131,780	65,890	65,890	
<b>TOTALS</b>	<b>146,987</b>	<b>6,467,901</b>	<b>4,369,038</b>	<b>4,383,806</b>	

Non-operating grants, subsidies and contributions Provider	Unspent Funds Liability	Current Budget Revenue	Budget YTD Revenue	Actual YTD Revenue	Comments
	\$	\$		\$	
<b>Law, order, public safety</b>					
Department Premier & Cabinet - Kwinana South extensions	30,000	699,600	0	0	
Department Premier & Cabinet - Mandogalup extensions	0	26,302	13,151	13,151	Final recognition of Grant received FY19. Project complete.
Department Fire and Emergency Services - Mandogalup Ablutions	0	131,718	110,824	110,824	Final 25% claim received October. Project complete.
<b>Recreation and culture</b>					
Medina Oval Extension/Upgrade - Changeroom Development	0	400,000	0	0	
Department of Infrastructure - Thomas Oval Lighting	0	37,500	37,500	37,500	Final claim completed.
Department of Education - McWhirter Promenade and Johnson Road landscaping	55,663	0	0	0	Project complete.
Department of Education - Breccia Parade Footpath	0	2,547	2,547	2,547	
Local Roads and Community Infrastructure Program	0	214,000	130,000	0	
Local Roads and Community Infrastructure Program - Round 2	0	183,500	0	0	
Lotterywest - Kwinana Loop Trail	0	38,000	38,000	38,000	Project complete.
<b>Transport</b>					
Roads to Recovery - Pace Road Re-seal	0	275,000	0	0	
Roads to Recovery - Orton Road Re-seal	0	174,000	0	0	
Roads to Recovery - Milton Place Re-seal	0	48,903	0	0	
State Road Grant - Wellard Road Duplication Project	0	0	0	0	
State Road Grant - Road Reseal - Ocean Street	0	230,133	90,976	181,952	
State Road Grant - Road Reseal - Cockburn Road NB	0	30,933	23,478	30,816	Final claim complete.
State Road Grant - Road Reseal - Leath Road 1	0	66,200	26,480	26,480	
State Road Grant - Road Reseal - Leath Road 2	0	33,133	12,664	12,664	
State Road Grant - Road Reseal - Leath Road 3	0	263,533	104,604	104,604	
State Road Grant - Road Reseal - Mandurah Road SB1	0	125,333	49,460	98,920	
State Road Grant - Road Reseal - Mandurah Road NB 2	0	43,200	16,673	33,346	
State Road Grant - Road Reseal - Mason Rd	0	141,133	55,376	110,752	
Black Spot - Gilmore Avenue Pedestrian Crossing	0	154,940	30,940	30,940	
Black Spot - Meares Road - roundabout deflection treatment	0	73,333	29,333	29,333	
Perth Bicycle Network LG Grant - Gilmore Avenue Shared Path Stage 1 Design	0	40,700	16,280	32,560	
Perth Bicycle Network LG Grant - Parmelia Avenue Shared Path Stage 1 Design	0	29,400	23,520	23,520	
Local Roads and Community Infrastructure Program	141,462	320,720	195,720	125,898	Barwell Rd & Bilya Gdns footpaths and Wellard paving project completed.
Local Roads and Community Infrastructure Program - Round 2	0	1,358,306	0	0	
<b>Economic services</b>					
Medina CCTV Grant	0	10,007	0	10,007	Project complete.
	0				
<b>Community amenities</b>					
DCA 1 - Hard Infrastructure - Bertram	660,537	675,102	0	0	
DCA 2 - Hard Infrastructure - Wellard	1,721,297	2,095,102	0	0	
DCA 3 - Hard Infrastructure - Casuarina	0	3,472,228	0	0	
DCA 4 - Hard Infrastructure - Anketell	1,200,960	22,591	0	0	
DCA 5 - Hard Infrastructure - Wandii	615,994	460,231	0	0	
DCA 6 - Hard Infrastructure - Mandogalup	569,703	22,591	0	0	
DCA 7 - Hard Infrastructure - Mandogalup (west)	49,262	22,954	0	0	
DCA 8 - Soft Infrastructure - Mandogalup	1,303,383	23,902	0	0	Revenue is recognised upon meeting performance obligations (in-line with expenditure on DCA infrastructure).
DCA 9 - Soft Infrastructure - Wandii / Anketell	10,990,358	3,342,105	0	0	
DCA 10 - Soft Infrastructure - Casuarina/Anketell	194,800	24,774	0	0	
DCA 11 - Soft Infrastructure - Wellard East	5,723,005	25,662	0	0	
DCA 12 - Soft Infrastructure - Wellard West	8,262,560	24,362	0	0	
DCA 13 - Soft Infrastructure - Bertram	170,063	24,216	0	0	
DCA 14 - Soft Infrastructure - Wellard / Leda	665,903	24,152	1	0	
DCA 15 - Soft Infrastructure - Townsite	328,843	24,151	0	0	
<b>TOTALS</b>	<b>32,683,792.52</b>	<b>15,436,197</b>	<b>1,007,527</b>	<b>1,053,813</b>	

**NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY  
FOR THE PERIOD ENDED 28 FEBRUARY 2021**

**NOTE 12  
TRUST FUND**

Funds held at balance date over which the City has no control and which are not included in this statement are as follows:

<b>Description</b>	<b>Opening Balance 1 July 2020</b>	<b>Amount Received</b>	<b>Amount Paid</b>	<b>Closing Balance 28 Feb 2021</b>
	\$	\$	\$	\$
APU Security Bonds	17,934	3,000	0	<b>20,934</b>
Contiguous Local Authorities Group (CLAG)	3,594	1,383	0	<b>4,977</b>
Public Open Space Cash In Lieu	283,848	36,765	0	<b>320,613</b>
	<b>305,376</b>	<b>41,148</b>	<b>0</b>	<b>346,524</b>

Amendments to original budget since budget adoption. Surplus/(Deficit)

Date	Description	Increase / (Decrease) to Net Surplus	Amended Budget Running Balance
		\$	\$
<b>24/06/2020</b>	<b>Annual Budget adoption</b>	<b>0</b>	<b>0</b>
<b>12/08/2020</b>	<b>Non-Operating Grants &amp; Contributions - Local Roads and Community Infrastructure Program</b>	534,720	
	Capital projects expenditure- Buildings	(229,000)	
	Capital projects expenditure - Footpaths	(145,720)	
	Capital projects expenditure - Roads	(160,000)	<b>0</b>
<b>26/08/2020</b>	<b>Opening Surplus Adjustment</b>	25,000	
	Capital project expenditure - Plant & Equipment - Recquatic upgrades	(25,000)	
	Opening Surplus Adjustment	3,540	
	Transfer from Reserve - Restricted Grants and Contributions Reserve	7,000	
	Capital project expenditure - Furniture & Equipment - Mayoral Chains	(10,540)	<b>0</b>
<b>9/09/2020</b>	<b>Reallocation of budgets between Capital building projects as did not meet LRCI criteria</b>		
	W13856 - Maydwell Way - Window screens renewal	7,000	
	W13855 - Magenup Equestrian Centre - Stable renovation	7,000	
	W13633 - Sloans Cottage Accessibility Works	(14,000)	<b>0</b>
	<b>Allocation of FESA ESL Grant received</b>		
	FESA ESL Grant - Govt Grant State	6,992	
	FESA ESL - Kwinana South Vehicle Expenses	(1,320)	
	FESA ESL - Mandogalup Vehicle Expenses	(1,321)	
	FESA ESL - Kwinana South Plant & Equipment Purchases	(1,422)	
	FESA ESL - Mandogalup Plant & Equipment Purchases	(2,929)	<b>0</b>
<b>20/09/2020</b>	<b>Transfer of Insurance proceeds from stolen statue to Public Arts Reserve.</b>		
	Insurance Proceeds	29,900	
	Transfer to Public Arts Reserve	(29,900)	<b>0</b>
	<b>Reallocation of capital funding.</b>		
	W13833 - Pool upgrades to align with new chlorine gas standard.	(7,000)	
	W13642 - Backwash tank no longer required	7,000	
	Transfer from Reserve Asset Mgmt Reserve - to W13833 Pool Upgrades	7,000	
	Transfer from Reserve Asset Mgmt Reserve - to W13642 25m backwash tank lid	(7,000)	
	W13630 - Darius Wells Automated Disabled toilet doors - additional expenditure	(5,000)	
	W13628 - John Wellard CC Stormwater system	5,000	
	Transfer from Asset Management Reserve - W13630 Darius Wells Automated Doors	5,000	
	Transfer from Asset Management Reserve - W13628 John Wellard Community Centre Stormwater System	(5,000)	<b>0</b>
<b>14/10/2020</b>	<b>Reallocation of capital funding to Kwinana South VBFB Station.</b>		
	W13624 - Transfer from Asset Management Reserve (savings in Parmelia House roof renovation)	32,700	
	W13620 - Transfer from Asset Management Reserve to Kwinana South VBFB Station upgrade	(32,700)	
	W13624 - Capital Expenditure - savings in Parmelia House roof renovation	(32,700)	
	W13620 - Capital Expenditure- Kwinana South VBFB Station upgrade.	32,700	
	Site works construction costs not allowed for under DFES funding.		<b>0</b>
<b>28/10/2020</b>	<b>Reallocation of fleet vehicle replacement Plant 551.</b>		
	W13692/W13886 - Capital Expense - Plant and equipment	(10,000)	
	W13692/W13886 - Reserve Transfer - Plant and Equipment Replacement Reserve	10,000	<b>0</b>
	<b>Playground remediation work for Gemstone Park - reallocation of Parks for People Strategy funding.</b>		
	Capital Expense - Parks for People Strategy - Gemstone Park	(129,360)	
	Capital Expense - Parks for People Strategy - Ascot Park	143,680	
	Transfer from Asset Management Reserve	(14,320)	<b>0</b>

Amendments to original budget since budget adoption. Surplus/(Deficit)

Date	Description	Increase / (Decrease) to Net Surplus	Amended Budget Running Balance
		\$	\$
<b>28/10/2020</b>	<b>Main Roads Black Spot funding (2/3) to construct pre-deflection treatment at Meares Avenue.</b>		
	Capital Expense - Meares Avenue - Challenger Avenue roundabout pre deflection treatment	(110,000)	
	W13699 - Traffic Management - Traffic Calming	36,667	
	W13699 Transfer from Asset Management Reserve	(36,667)	
	W13887 Transfer from Asset Management Reserve	36,667	
	Capital Revenue - Grant - Black Spot Grant Funding	73,333	0
	<b>In-house delivery of Age Friendly Strategy.</b>		
	Operating expense - City Engagement Admin - Salaries	(75,000)	
	Operating expense - City Executive - Consultancy	75,000	0
	<b>Thomas Oval Lighting carried forward from 2019/2020.</b>		
	Capital Expense - Reserve Development - Sport - Thomas Oval Lighting	134,607	
	Capital Revenue - Grant - Department of Sport and Recreation	(112,500)	
	Reserve transfer - Restricted Grants and Contributions - Thomas Oval Lighting	(54,613)	
	Opening Surplus	32,506	0
	<b>Maintenance of Wellard sub drain for 21months following handover per Development Contribution Plan.</b>		
	Operating expense - Drainage maintenance - Wellard	(22,240)	
	Reserve transfer - DCA 2 Reserve	22,240	0
<b>12/11/2020</b>	<b>Good Things Foundation Grant</b>		
	Operating Grant Revenue	1,000	
	Operating expense - Special Events	(1,000)	0
	<b>Recquatic Café Splash opening</b>		
	Revenue - Café Income	44,058	
	Operating Expenditure (employee expenses, materials and contracts)	(44,058)	0
<b>25/11/2020</b>	<b>Recquatic Swim Club Office relocation</b>		
	W13873 Capital Expense - Swim Club Office relocation	(10,400)	
	Operating expense - Expendable equipment	10,400	0
	<b>Inhouse Printer for Rates department</b>		
	Capital expense - Folding/insertion machine with software	(17,505)	
	Operating expense - stationary and printing	17,505	0
	<b>DFES Mitigation Works Grant</b>		
	Revenue - Operating Grants	15,251	
	Operating expense - Mitigation works	(15,251)	0
<b>16/12/2020</b>	<b>Plant Replacement P227 Molnar 4 post Hoist</b>		
	W13905 Capital Expense - Plant & Equipment - New Molnar 4 post hoist	(18,541)	
	Sales Proceeds - Plant & Equipment	1,500	
	Operating Expense - savings in City Operations Infrastructure Maintenance	17,041	0
<b>27/01/2021</b>	<b>Local Roads and Community Infrastructure Program - Round 2 Funding</b>		
	Capital Revenue - Grant - LRCI	534,720	
	Capital Expense - Buildings - Various	(229,000)	
	Capital Expense - Footpaths - Various	(145,720)	
	Capital Expense - Roads - Various	(160,000)	0
	<b>Family Day Care Vehicle - purchase rather than lease</b>		
	Capital Expense - Bright Futures - Vehicles	(55,000)	
	Operating Expense - Bright Futures - Lease Vehicles	(5,000)	
	Family Day Care Reserve	60,000	0

Amendments to original budget since budget adoption. Surplus/(Deficit)

Date	Description	Increase / (Decrease) to Net Surplus	Amended Budget Running Balance
		\$	\$
<b>27/01/2021</b>	<b>Insurance Replacement P574</b>		
	Capital expense - New fleet vehicle	(39,697)	
	Capital Revenue - Insurance proceeds	39,197	
	Operating Expense - Plant operating - contractors	500	0
	<b>Purchase of Toxic Chemical Storage Cabinet</b>		
	Operating expense - City Operations - Expendable equipment	(13,771)	
	Capital Expense - Plant & Equipment - Toxic chemical storage cabinet	(6,081)	
	Capital Revenue - Plant & Equipment proceeds - Trailers x2	6,602	
	Operating revenue - City Operations - sale of operating equipment	13,250	0
	<b>Deposits Payable to Secure 2021/2022 Vehicles</b>		
	Prepayment - Plant & Equipment - Vehicles Deposit	(3,000)	
	Reserve Transfer - Plant and Equipment Replacement Reserve	3,000	0
<b>10/02/2021</b>	<b>Donation to Wooroloo and Hills Bushfire Appeal</b>		
	Operating Expense - Other Welfare - Sundry Donations	(5,000)	
	Reserve Transfer - Community Services & Emergency Relief Reserve	5,000	0
<b>24/02/2021</b>	<b>Budget Review</b>		
	Increase in Operating Funding Surplus brought forward	1,412,908	
	Operating Activities	1,087,235	
	Depreciation/Other Non Cash	6,420,692	
	Decrease in Non-operating Grants, Subsidies and Contributions	(35,623)	
	Decrease in proceeds from disposal of assets	(56,946)	
	Decrease in Capital Expenditure	150,878	
	Decrease in Financing Activities	(4,451,148)	4,527,996
	<b>Allocation of Surplus</b>		
	Operating expenditure - Online Booking System	(7,500)	
	Operating expenditure - Industrial Relations advice	(10,000)	
	Operating expenditure - Internal Audit	(15,000)	
	Operating expenditure - Consultant for Bank Tender Review	(15,000)	
	Operating expenditure - Regulation 17 Review	(16,000)	
	Operating expenditure - Natural Area Management Plan	(25,000)	
	Operating expenditure - City Leadership Training	(146,000)	
	Capital Expenditure - Admin Building Refurbishment	(150,000)	
	Capital Expenditure - Corporate Business System Project	(619,795)	
	Reserve Transfer - Workers Compensation Reserve	(180,000)	
	Reserve Transfer - Infrastructure Reserve (future funding of PTA grants shared pathways)	(360,000)	
	Reserve Transfer - Restricted Grants & Contributions Reserve - Workforce reclassification project	(400,000)	
	Reserve Transfer - Information Technology Reserve - Corporate Business System 2021/22	(400,000)	
	Reserve Transfer - Employee Leave Provision Reserve	(458,300)	
	Reserve Transfer - Workers Compensation Reserve Asset Management Reserve	(1,725,401)	
		<b>(4,527,996)</b>	<b>0</b>
		<b>(4,527,996)</b>	<b>0</b>

## KEY TERMS AND DESCRIPTIONS

FOR THE PERIOD ENDED 28 FEBRUARY 2021

### REVENUE

#### RATES

All rates levied under the *Local Government Act 1995*. Includes general, differential, specified area rates, minimum rates, interim rates, back rates, ex-gratia rates, less discounts and concessions offered. Exclude administration fees, interest on instalments, interest on arrears, service charges and sewerage rates.

#### OPERATING GRANTS, SUBSIDIES AND CONTRIBUTIONS

Refers to all amounts received as grants, subsidies and contributions that are not non-operating grants.

#### NON-OPERATING GRANTS, SUBSIDIES AND CONTRIBUTIONS

Amounts received specifically for the acquisition, construction of new or the upgrading of identifiable non financial assets paid to a local government, irrespective of whether these amounts are received as capital grants, subsidies, contributions or donations.

#### REVENUE FROM CONTRACTS WITH CUSTOMERS

Revenue from contracts with customers is recognised when the local government satisfies its performance obligations under the contract.

#### FEES AND CHARGES

Revenues (other than service charges) from the use of facilities and charges made for local government services, sewerage rates, rentals, hire charges, fee for service, photocopying charges, licences, sale of goods or information, fines, penalties and administration fees. Local governments may wish to disclose more detail such as rubbish collection fees, rental of property, fines and penalties, other fees and charges.

#### SERVICE CHARGES

Service charges imposed under *Division 6 of Part 6 of the Local Government Act 1995*. *Regulation 54 of the Local Government (Financial Management) Regulations 1996* identifies these as television and radio broadcasting, underground electricity and neighbourhood surveillance services. Exclude rubbish removal charges. Interest and other items of a similar nature received from bank and investment accounts, interest on rate instalments, interest on rate arrears and interest on debtors.

#### INTEREST EARNINGS

Interest and other items of a similar nature received from bank and investment accounts, interest on rate instalments, interest on rate arrears and interest on debtors.

#### OTHER REVENUE / INCOME

Other revenue, which can not be classified under the above headings, includes dividends, discounts, rebates etc.

#### PROFIT ON ASSET DISPOSAL

Excess of assets received over the net book value for assets on their disposal.

## NATURE OR TYPE DESCRIPTIONS

### EXPENSES

#### EMPLOYEE COSTS

All costs associate with the employment of person such as salaries, wages, allowances, benefits such as vehicle and housing, superannuation, employment expenses, removal expenses, relocation expenses, worker's compensation insurance, training costs, conferences, safety expenses, medical examinations, fringe benefit tax, etc.

#### MATERIALS AND CONTRACTS

All expenditures on materials, supplies and contracts not classified under other headings. These include supply of goods and materials, legal expenses, consultancy, maintenance agreements, communication expenses, advertising expenses, membership, periodicals, publications, hire expenses, rental, leases, postage and freight etc. Local governments may wish to disclose more detail such as contract services, consultancy, information technology, rental or lease expenditures.

#### UTILITIES (GAS, ELECTRICITY, WATER, ETC.)

Expenditures made to the respective agencies for the provision of power, gas or water. Exclude expenditures incurred for the reinstatement of roadwork on behalf of these agencies.

#### INSURANCE

All insurance other than worker's compensation and health benefit insurance included as a cost of employment.

#### LOSS ON ASSET DISPOSAL

Shortfall between the value of assets received over the net book value for assets on their disposal.

#### DEPRECIATION ON NON-CURRENT ASSETS

Depreciation expense raised on all classes of assets.

#### INTEREST EXPENSES

Interest and other costs of finance paid, including costs of finance for loan debentures, overdraft accommodation and refinancing expenses.

#### OTHER EXPENDITURE

Statutory fees, taxes, allowance for impairment of assets, member's fees or State taxes. Donations and subsidies made to community groups.



## 18.2 Accounts for payment for the month ended 28 February 2021

### DECLARATION OF INTEREST:

Mayor Carol Adams declared an impartiality interest due to her spouses employer (Kwinana Industries Council) receiving a reimbursement payment.

### SUMMARY:

The purpose of this report is to present to Council a list of accounts paid under delegated authority for the month ended 28 February 2021, as required by the *Local Government (Financial Management) Regulations 1996*.

### OFFICER RECOMMENDATION:

That Council:

1. Accepts the list of accounts, totalling \$5,662,545.44, paid under delegated authority in accordance with Regulation 13(1) of the *Local Government (Financial Management) Regulations 1996* for the period ended 28 February 2021, as contained within Attachment A.
2. Accepts the detailed transaction listing of credit card expenditure paid for the period ended 28 February 2021, as contained within Attachment B.

### DISCUSSION:

Council has delegated, to the Chief Executive Officer, the exercise of its power to make payments from the City's Municipal and Trust funds. In accordance with Regulation 13 of the *Local Government (Financial Management) Regulations 1996* a list of accounts paid is to be provided to Council, where such delegation is made.

The following table summarises the payments for the period by payment type, with full details of the accounts paid contained within Attachment A.

Payment Type	Amount (\$)
Automatic Payment Deductions	\$ 247,816.81
Cheque Payments #201042 to #201043	\$ 567.75
EFT Payments #4149 to #4163	\$ 4,069,894.83
Payroll Payments 07/02/21 and 21/02/21 and Interim Payroll Payments 01/02/21, 05/02/21, 07/02/21 and 10/02/21	\$ 1,344,266.05
<b>Total Attachment A</b>	<b>\$ 5,662,545.44</b>

Contained within Attachment B is a detailed transaction listing of credit card expenditure paid for the period ended 28 February 2021. This amount is included within the total payments, listed above.

## 18.2 ACCOUNTS FOR PAYMENT FOR THE MONTH ENDED 28 FEBRUARY 2021

### **LEGAL/POLICY IMPLICATIONS:**

Regulation 13 of the *Local Government (Financial Management) Regulations 1996* states:

13. *Payments from municipal fund or trust fund by CEO, CEO's duties as to etc.*
- (1) *If the local government has delegated to the CEO the exercise of its power to make payments from the municipal fund or the trust fund, a list of accounts paid by the CEO is to be prepared each month showing for each account paid since the last such list was prepared —*
    - (a) *the payee's name; and*
    - (b) *the amount of the payment; and*
    - (c) *the date of the payment; and*
    - (d) *sufficient information to identify the transaction.*
  - (2) *A list of accounts for approval to be paid is to be prepared each month showing*
    - (a) *for each account which requires council authorisation in that month —*
      - (i) *the payee's name; and*
      - (ii) *the amount of the payment; and*
      - (iii) *sufficient information to identify the transaction;*  
*and*
    - (b) *the date of the meeting of the council to which the list is to be presented.*
  - (3) *A list prepared under subregulation (1) or (2) is to be —*
    - (a) *presented to the council at the next ordinary meeting of the council after the list is prepared; and*
    - (b) *recorded in the minutes of that meeting.*

### **FINANCIAL/BUDGET IMPLICATIONS:**

There are no financial implications that have been identified as a result of this report or recommendation.

### **ASSET MANAGEMENT IMPLICATIONS:**

There are no asset management implications that have been identified as a result of this report or recommendation.

### **ENVIRONMENTAL IMPLICATIONS:**

There are no environmental implications that have been identified as a result of this report or recommendation.

## 18.2 ACCOUNTS FOR PAYMENT FOR THE MONTH ENDED 28 FEBRUARY 2021

**STRATEGIC/SOCIAL IMPLICATIONS:**

This proposal will support the achievement of the following outcome and objective detailed in the Corporate Business Plan.

Plan	Outcome	Objective
Corporate Business Plan	Business Performance	5.4 Ensure the financial sustainability of the City of Kwinana into the future

**COMMUNITY ENGAGEMENT:**

There are no community engagement implications that have been identified as a result of this report or recommendation.

**PUBLIC HEALTH IMPLICATIONS:**

There are no implications on any determinants of health as a result of this report.

**RISK IMPLICATIONS:**

The risk implications in relation to this proposal are as follows:

Risk Event	That Council does not accept the payments
Risk Theme	Failure to fulfil statutory regulations or compliance requirements
Risk Effect/Impact	Compliance
Risk Assessment Context	Operational
Consequence	Minor
Likelihood	Possible
Rating (before treatment)	Low
Risk Treatment in place	Reduce – mitigate risk
Response to risk treatment required/in place	Officers provide a full detailed listing of payments made in a timely manner
Rating (after treatment)	Low

**COUNCIL DECISION**

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**MOVED CR P FEASEY****SECONDED CR M ROWSE****That Council:**

1. **Accepts the list of accounts, totalling \$5,662,545.44, paid under delegated authority in accordance with Regulation 13(1) of the *Local Government (Financial Management) Regulations 1996* for the period ended 28 February 2021, as contained within Attachment A.**

18.2 ACCOUNTS FOR PAYMENT FOR THE MONTH ENDED 28 FEBRUARY 2021

2. **Accepts the detailed transaction listing of credit card expenditure paid for the period ended 28 February 2021, as contained within Attachment B.**

**CARRIED  
6/0**

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
<b>Automatic Deductions</b>			
Go Go On-Hold Pty Ltd	00046233	Messages on hold February 2021	198.00
Commonwealth Bank	040221A	Credit card Functions Officer to 040221	36.60
Commonwealth Bank	040221B	Credit card Economic Development Manager to 040221	114.06
Commonwealth Bank	040221C	Credit card Director City Development to 040221	855.24
Commonwealth Bank	040221D	Credit card Emergency Services Coordinator to 040221	609.50
Commonwealth Bank	040221E	Credit card Director City Business to 040221	1,339.38
Commonwealth Bank	040221F	Credit card A/Manager Customer and Communications to 040221	2,625.28
Commonwealth Bank	040221G	Credit card Manager Governance to 040221	85.90
Commonwealth Bank	040221H	Credit card Rates Coordinator to 040221	196.00
Commonwealth Bank	040221I	Credit card Manager Human Resources to 040221	2,079.15
BP Australia Pty Ltd	11439615	Fleet fuel 010121 to 310121	17,862.88
iinet Technologies Pty Ltd	124746381	Monthly internet charges various locations	589.93
Windcave Pty Ltd	1649977	Monthly service fees January 2021	55.00
Ampol Australia Petroleum Pty Ltd	0301917399	Fleet fuel 010121 to 310121	5,662.73
Fines Enforcement Registry	27139589	Lodgement fee for unpaid infringements	308.00
Fines Enforcement Registry	27184687	Lodgement fee for unpaid infringements	77.00
Fines Enforcement Registry	27190985	Lodgement fee for unpaid infringements	539.00
JCS Online Resources Ltd	27966452	Find My Past World annual subscription	1,990.88
Wright Express Australia Pty Ltd	63	Fleet fuel 010121 to 270121	1,137.74
iinet Technologies Pty Ltd	706372216	Monthly internet charges Bertram Community Centre	79.99
Toyota Fleet Management	858890	Fleet management services February 2021	1,194.07
TPG Internet Pty Ltd	I255362325	Kwinana South Station internet connection	59.99
TPG Internet Pty Ltd	I255775525	Mandogalup Station internet connection	59.99
City of Kwinana	2020/2021	Rates and waste charges City of Kwinana properties	210,060.50
<b>Total Automatic Deductions</b>			<b>-247,816.81</b>
<b>Cheques</b>			
City Of Kwinana - Pay Cash	080221 Depot	Petty cash recoup to 180121 Library	55.60
City Of Kwinana - Pay Cash	190221 - Library	Petty cash recoup to 190221 Library	192.75
	230221 - Admin	Petty cash recoup to 230221 Admin	253.60
	230221 - Library	Petty cash recoup to 230221 Library	65.80
<b>Total Cheques</b>			<b>-567.75</b>
<b>EFT</b>			
EFT TRANSFER: - 02/02/2021			150.00
Sandra Widginton	9.6	Rates Refund	150.00
EFT TRANSFER: - 03/02/2021			63,287.33
Bright Futures Family Day Care - Pa	250121 to 310121	FDC Payroll 250121 to 310121	46,464.53
Bright Futures In Home Care - Payro	250121 to 310121	IHC Payroll 250121 to 310121	16,822.80
EFT TRANSFER: - 04/02/2021			228,901.86

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Pointform Pty Ltd	9.5	Rates Refund	134.96
Shred-X Pty Ltd	01560638	Exchange and destroy secure document bins	40.48
David Wills and Associates	00030778	Engineering services for retaining wall Belvoir Cr	1,684.65
Summers Consulting	INV-904	Mosquito monitoring 210121	1,500.40
	INV-858	Mosquito monitoring 220121	1,126.40
Retech Rubber	00003422	Repair soft fall Orelia Pavilion playground	522.50
Alison Bannister Career Coaching	DWNY8	Career coaching session	495.00
Auscontact Association Limited	17273	Corporate membership 310121 to 200122	450.00
Smiley's Out of School Care	1781021	Refund bond on The Patio	100.00
Leaf Bean Machine Pty Ltd	00084876	Items for Cafe Splash at Recquatic	385.00
Noni Danielle Comben	28January2021	Lyrik Scholarship payment	400.00
WA Carmax Pty Ltd	R111301213	2020 Isuzu auto crew cab ute	23,654.85
Red Oxygen Pty Ltd	C100206490-51314	Licence and message charges 271220 to 260121	45.32
Burson Automotive Pty Ltd	110677866	Workshop consumables for City Operations	587.40
Australian HVAC Services Pty Ltd	59510	Investigate air handling unit at Recquatic Centre	346.50
	59518	Investigate air conditioner at Bright Futures	179.30
	59571	Replace fans in stadium air conditioner Recquatic	2,302.08
	59579	Replace compressor unit at Recquatic Centre	4,253.66
Behrooz Houshangpoor Eslam	003-2021	GST component of invoice 002-2020	15.00
Matthew Ponsford	1	Dragons and Dungeons program for The Zone	170.00
Anna Kelly	210101	Mural painting at Ascot Park for Place Plan Active	480.00
Future Physio	02February21	Refund hire fee on Wellard Pavilion booking 020221	23.40
Perth Sup School	INV-0091	Make a Splash program for The Zone	300.00
Satellite Security Services	IV010329	Replace batteries in controller at City Operations	255.00
	IV010339	Replace alarm sensor at Wellard Pavilion	200.20
	IV010338	Inspect siren tamper at William Bertram Community	165.00
Jordan Peter Kilfoyle	28January21	Reimbursement for Working With Children Check	87.00
Sneha Rapur	1800835	Refund bond on The Patio	100.00
Caffissimo Kwinana	22January2021	Local Commercial Support Grant Funding	1,100.00
Good Times Tattoo	25January2021	Local Commercial Support Grant Funding	880.00
Ruby Pereira	28January2021	Lyrik Scholarship payment	300.00
Jasmin Pereira	28January2021	Lyrik Scholarship payment	800.00
Lee Michael Kennedy	29January2021	Refund of swimming Lessons	115.20
Emily Philippe	28January2021	Lyrik Scholarship payment	800.00
Venessa Figredo	28January2021	Lyrik Scholarship payment	400.00
Emma Cobb	29January2021	Refund of swimming Lessons	119.00
Marian Cacao	1794610	Refund bond on Medina Hall	955.50
Emma Rose Martinez	9.5	Rates Refund	298.83
Nikki Lucre	1753371	Refund bond on The Patio	100.00
Ryan Watret	1774340	Refund bond on The Patio	100.00
Nachippan Nachiappan	1789641	Refund bond on The Patio	100.00
	02February21	Cancelled Patio booking 060221	39.00
Alphonsa Mannara Thadevus	1800930	Refund bond on Medina Hall	1,000.00
Hester Heaysman	1757100	Refund bond on The Patio	100.00
Suresh Kaja	1800606	Refund bond on The Patio	100.00
Tarmira Wilson	1800800	Refund bond on Thomas Kelly Pavilion	1,000.00

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Rebecca Savage	1810974	Refund bond on The Patio	100.00
Bronnyn Tohu-Tumai-Totorewa	1809281	Refund bond on The Patio	100.00
Devinder Kumar Mahendra	1809274	Refund bond on The Patio	100.00
Gurjit Singh Masoun	1803231	Refund bond on Tuart Room	2,000.00
Suman Talukdar	1803144	Refund bond on The Patio	100.00
	02February21	Cancelled Patio hire on 310121	39.00
Anthony Widjaja	1803024	Refund bond on The Patio	100.00
Jo Proudfood	1789057	Refund bond on The Patio	100.00
Benjamin Hillary	1794694	Refund bond on The Patio	100.00
	02February21	Cancelled hire of the Patio 140221	39.00
Wilhelmina Geertruida Johanna Hof	03February21	Seniors security subsidy scheme 2020/2021	250.00
Arthur John Thompson	03February21	Seniors security subsidy scheme 2020/2021	250.00
Teresa Olive Franco	03February21	Seniors security subsidy scheme 2020/2021	250.00
Sportsworld Of WA	139358	Goggles for Proshop at Recquatic	2,192.85
Telstra	0335568200Jan21	Banksia Park Clubhouse to 140121	48.05
	1548725500Jan21	Feilman Building to 020221	19.25
Toolmart Australia Pty Ltd	RH-162459	Diesel storage and dispensing kit for Depot	1,220.00
Total Eden Pty Ltd	411276875	Reticulation stock	5,239.85
Trisley's Hydraulic Services Pty Lt	100203378	Replace chlorine transmitter box in hydro pool	1,320.00
Western Australian Local Government	I3085822	2021 Transport and Road Forum	70.00
Wandi Progress Association Inc.	01February2021	Local Commercial Support Grant Funding	2,200.00
Absolute Painting Services	INV-1758	Repaint ceilings in toilets at Darius Wells	1,925.00
	INV-1759	Banksia V10 complete repaint	1,672.00
Palm Lakes Gardens & Landscape Serv	124587	Callistemon U2 and U17 reticulation repairs	890.00
Beaver Tree Services Aust Pty Ltd	75780	Tree pruning Kwinana Beach	32,957.40
Prestige Catering & Event Hire	INV-2274	Australia Day Citizenship catering	3,340.00
ZircoData Pty Ltd	ZDW0159126	Offsite storage pick up and retrieval 250121	898.29
Bunnings Building Supplies	2163/01612273	Hose equipment for Facilities Department	132.70
	2163/01617899	Pipe for Recquatic Centre	26.61
	2163/01617737	Castor wheel set for Library trolley	26.39
Ixom Operations Pty Ltd	6341391	Chlorine gas supply for Recquatic Jan 20	1,035.89
Sigma Chemicals	145455/01	Chemical order for Recquatic	530.20
Ausco Modular Pty Ltd	7264708	Demountable hire Feb 21	1,471.80
Bolinda Publishing Pty Ltd	218420	Audio books for Library	224.27
Outsource Business Support Solution	00001638	Temp staff 250121 to 310221	3,702.60
	00001632	Temp staff week ending 240121	3,239.77
PFD Food Services Pty Ltd	KW842040	Items for Cafe Splash at Recquatic	1,103.95
AAA Blinds Port Kennedy	7699	Install blinds Recquatic Group Fitness room	1,500.00
BullAnt Security Pty	10206784	Duplicate key for City Operations Team	35.44
	10206714	Supply lockwood twin keys for Recquatic	64.56
HECS Fire	73027	Install washer kits at John Wellard Centre	968.00
	72194	Repair fire alarm sensor at Darius Wells	176.00
TC Precast Pty Ltd	SI-00003379	Manhole cover with insert	275.00
Studiosity Pty Ltd	INV-4358	Studiosity annual support and maintenance	4,180.00
Schweppes Australia Pty Ltd	9006981844	Items for Cafe Splash Recquatic	311.40
MRP General Pest/Termite Division 4	97358	Pest control Challenger Beach toilets	139.70

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Intelife Group	02February21	Refund hire fee on Alf Lydon and Frank Baker Room	23.30
AC Cooling Services	7676	Callistemon U11 inspect air conditioner	143.00
	7718	Banksia V27 service inspection on air conditioner	223.00
ED Property Services	00001686	Callistemon U23 repairs to door locks	66.00
Data #3 Limited	01980995	Annual renewal Adobe and Acrobat 2021	19,668.00
Medina Residents Group	01February21	Local Commercial Support Grant Funding	2,000.00
Trophy Express	13487	Medals for Lap It Up Swim Club	292.50
Complete Office Supplies Pty Ltd	09823924	Stationery items for Recquatic Centre	1,334.53
JB HiFi Commercial Division	BD0370188	Computer equipment for IT	595.85
Master Lock Service	00007570	Callistemon U66 and Banksia V24 keys cut	210.00
Name Badge World	BW16428	Employee name badges	76.85
Fire & Emergency Services, Dept of	01February2021	ESL collections for January 2021 Option A	49,670.77
ALSCO Pty Ltd	CPER2100330	Linen hire for OCM Council dinners	118.65
	CPER2101430	Linen hire for OCM Council dinners	67.45
	CPER2102437	Linen hire for OCM Council dinners	61.83
Plants & Garden Rentals	00016161	Community Resource Centres plant hire for Darius	159.50
Woolworths Group Limited	4024086	Items for The Zone	46.50
	4024076	Items for The Zone	51.70
	4024078	Items for The Zone	98.18
	4024083	Items for The Zone	9.86
	80268103	Items for Admin Building and Council Dinners	106.40
	80086298	Items for Admin Building	96.85
	81516218	Items for City Operations	107.79
	81127359	Items for Cafe Splash Recquatic	319.26
	4024085	Items for Recquatic Centre	139.00
	81245137	Items for Admin Building	372.39
Elexacom	112874	Banksia V44 replace GPO in bedroom	165.09
	112882	Emergency evacuation testing Leda Hall	132.00
	112884	Emergency evacuation testing Darius Wells	308.00
	112886	Emergency evacuation testing Kwinana Tennis Club	132.00
	112909	Reset circuit on BBQ at Boyne Park	154.00
	112918	Disconnect hot water system at Bright Futures	143.61
	112876	Repair BBQ at Wells Park	404.99
	112880	Emergency evacuation testing The Zone	154.00
	112881	Emergency evacuation testing Thomas Netball Centre	132.00
	112879	PAT testing at The Adventure Park	122.10
	112919	Install power point at City Operations	77.00
	112915	Repair emergency light at Margaret Feilman Centre	129.42
	112887	Emergency evacuation testing Civic Administration	385.00
	112885	Emergency evacuation testing Bright Futures	308.00
	112883	Emergency evacuation testing Kwinana Out of School	132.00
	112869	Street lighting repairs Gilmore Ave	5,456.03
Isentia Pty Limited	MN0813969	Media monitoring February 21	935.00
Gregs Glass	10733	Replace window Out of School Day Care Centre	405.00
Heatley Sales Pty Ltd	C966706	Uniforms for City Operations Team	488.40
	C966484	Uniforms for City Operations Team	296.84
	C966061	Uniforms for City Operations Team	577.83
	C965278	Uniforms for City Operations Team	260.54
	C965106	Uniforms for City Operations Team	325.55
Irene Storey	03February21	Seniors security subsidy scheme 2020/2021	250.00
Quantum Building Services	00003719	Install ceiling box at Darius Wells	3,006.58
	00003720	Replace doors at Recquatic	3,237.85



# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
HP Financial Services Pty Ltd	100001272740	Monthly payment contract 5389066248AUS3 for Feb 21	588.85
	100001273266	Monthly payment contract 5389066248AUS3 for Mar 21	588.85
Blackwood & Sons Ltd	PE6888YU	Equipment for City Operations Team	168.41
Kwinana Veterinary Hospital Pty Ltd	117143	Animal Services	50.00
	117206	Microchipping fee	65.00
	116876	Microchipping fee	65.00
	120814	Microchipping fee	65.00
	119123	Microchipping fee	65.00
	117684	Microchipping fee	65.00
Kwinana Heritage Group	21January2021	Heritage management fees 3rd quarter Jan to Mar 2021	2,500.00
Lamp Replacements	IN1033830	Replace panel LED at Recquatic Centre	1,316.70
McLeods	117540	Legal fees matter 45241	566.83
Lo-Go Appointments	00422950	Temp staff week ending 160121	1,657.63
Planning Institute of Australia Pty	ED210127	Advertising for Senior Statutory Planning Officer	330.00
KM Coffee 2 You	26January2021	Local Commercial Support Grant Funding	1,000.00
Xtreme Bounce Party Hire	XT6303	Holiday program at Bertram Community Centre	555.00
EFT TRANSFER: - 04/02/2021		Payment reversal creditor #12831	-100.00
EFT TRANSFER: - 08/02/2021			156,786.00
Shirley Elfrida Bull	08February21	Tenure sum for Villa 27 Bright Rd	156,786.00
EFT TRANSFER: - 10/02/2021			47,301.77
Bright Futures Family Day Care - Pa	010221 to 070221	FDC Payroll 010221 to 070221	33,831.60
Bright Futures In Home Care - Payro	010221 to 070221	IHC Payroll 010221 to 070221	13,470.17
EFT TRANSFER: - 11/02/2021			1,078,627.92

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Web Track	INV-5651	Install GPS tracking unit	165.00
Emerg Solutions Pty Ltd	INV-1353	Purchase of BART subscription for Mandogalup BFB	150.00
Alex Krsnik	SQ21058	Cleanup and verge maintenance various locations	396.00
	SQ21059	Fire mitigation verge slashing various locations	891.00
	SQ21060	Litter pick up in various locations	605.00
	SQ21061	Box out and mulching of gardens on Kemble Lane	423.50
	SQ21062	Slash and litter pick up at Isaac Way	330.00
	SQ21063	Various roundabout garden maintenance	3,621.00
	SQ21064	City Centre litter collection Jan 21	3,190.00
	SQ21065	Streetscape garden maintenance Jan 21	836.00
	SQ21066	Clean up and verge maintenance various locations	17,126.52
Bay Concrete Grinding	00038020	Concrete grinding Runnymede Gate	880.00
	00038019	Concrete grinding Abingdon Park footpath	3,850.00
Rockingham Toyota	JC23024238	Part payment invoice JC23024238	3.00
	JC23023837	Fuel tank repairs for IGJ339	957.62
	JC23024238	Airconditioning repairs to 1CRB723	436.58
Technifire 2000	24243	Bullbar with real ezy winder	176.09
PTE Group	INV-9930	Custom plant trailer 4.5T	20,361.66
Strategic DCP Consulting	030	DCP consulting services for DCA's	750.75
Port Printing Works	INV062520	OMG posters for distribution	60.50
Chorus Australia Ltd	C1087651	Mowing services at Bright Futures Jan 21	266.81
Outback Handyman	2928	Callistemon Court and Banksia Park maintenance	616.00
Sonic Health Plus	2246003	Pre-employment medical assessment 270121	325.60
	2242675	Pre-employment medical assessment 190121	325.60
	2244044	Pre-employment medical assessment 250121	198.00
Ritz Party Hire	1244604882	Hire of items for Sloan Reserve event	232.00
Travis Hayto Photography	00002064	Banksia Park videography	437.50
	00002060	Photography for Australia Day Citizenship Ceremony	400.00
Holcim (Australia) Pty Ltd	9407335342	14mm concrete for Galah Way	387.20
	9407345032	14mm concrete for Peace Park	576.40
	9407348611	14mm concrete for Chilcott and Sawyer Way	416.24
DNR Contracting Pty Ltd	1247	Civil works for Pace Road	117,376.26
Anita Wood	1789596	Refund bond on The Patio	100.00
	08February21	Cancellation of The Patio hire 060221	39.00
WA Carmax Pty Ltd	R111301217	Purchase of Isuzu 1KWN2150	49,211.10
Mackie Plumbing and Gas Pty Ltd	196488	Banksia V17 install hot water system	2,332.00
	195985	Banksia V11 replace plumbing fixtures to bathroom	1,540.51
Premier & Cabinet Department of	1000237	Local Planning Scheme 2	77.25
As Clean As A Whistle	00000539	Banksia Park Clubhouse and office cleaning Jan 21	819.50
Rota Moulding WA Pty Ltd	40103	Alterations to spray unit tank	1,223.20
Churchill Capital Consulting Pty Lt	00003267	Consulting services for Jan 21	17,156.64
Hocking Heritage + Architecture	INV-4112	Concept Development review	2,365.00
	INV-4100	Concept Development review Sloans Reserve	6,300.80
ETS Vegetation Management	150975	Vegetation works January 21	6,886.00
Adam Hammond	14January21	School holiday facilitator for Volleyball	134.50
Anna Kelly	29January21	Painting friendly zones Johnson Road activation	240.00
Nashtec Auto Electrics	58365	Uninstall emergency lighting on vehicle	250.00
BNL Earthworks	INV-1443	Local Commercial Support Grant Funding	1,000.00
Teija Eastwood	1792640	Refund bond on The Patio	100.00

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Tilly Sommerville	1803437	Refund bond on The Patio	100.00
Aboriginal Health Council of WA	04February21	Booking cancellation of Ken Jackman Hall	762.00
Robyn Goodes	03February21	Cancellation of Patio booking on 070221	39.00
	1772157	Refund bond on The Patio	100.00
Carey Michael Winton	Refund	Overpayment of building search fee	38.50
Jemma Frances Schoonenberg	9.7	Rates Refund	300.00
Mosole Nominees Pty Ltd	20January21	Refund of BSL and Commission	68.50
Nelly Klein	1766016	Refund bond on The Patio	100.00
	08February21	Cancellation of The Patio hire 140121	39.00
Kathy Young	1738034	Refund bond on The Patio	100.00
	08February21	Cancellation of The Patio hire 130221	39.00
Cherry Murphy	09February21	Cancellation of The Patio hire 130221	39.00
	1802681	Refund bond on The Patio	100.00
Jyoti Prakash	1793548	Refund bond on The Patio	100.00
	09February21	Cancellation of The Patio hire 130221	39.00
Z-CARD Pocketmedia Solutions	INV-2427	Deposit for Essential Services Z card	985.88
Daimler Trucks Perth	DFCRD278673	Diagnostic repairs for KWN2007	1,176.85
Stewart & Heaton Clothing Co Pty Ltd	SIN-3301171	Purchase of uniforms for WABFB	453.62
	SIN-3301163	Purchase of uniforms for WABFB	2,268.09
	SIN-3301696	Purchase of uniforms for WABFB	3,047.81
Sunny Sign Company Pty Ltd	450006	Street sign for Blackboy Rise	74.25
Toll Transport Pty Ltd	1108453	Transport services food water sampling analysis	79.88
	0411-T221490	Courier charges 180121	13.05
Total Eden Pty Ltd	411327469	Stock for City Operations Team	1,728.95
	411327454	Stock items for City Operations Team	750.50
	411277112	Flexi tube for City Operations Team	65.73
Trisley's Hydraulic Services Pty Ltd	100203385	Install vacuum breaker to hydrotherapy pool	1,094.50
Waste Stream Management Pty Ltd	00428089	Tipping fees on 290121	1,749.00
Water Corporation of Western Austrai	9014096921Feb21	20U Wellard Pavilion	506.20
	9017125687Jan21	2U Malden Park	5.16
	9018600726Jan21	19U Wellard Community Centre	1,554.66
	9021239484Jan21	0U Ryhill Cr drinking fountain	45.07
	9021511329Jan21	4U Boyne Park drink fountain	10.32
	9021554601Jan21	1U Heywood Action Park drink fountain	2.58
Western Irrigation Pty Ltd	J49245	Callistemon repair broken reticulation	1,994.50
Weston Road Systems	pr86	Wandi Drive resurfacing	495.00
	PR81	Marking for Mason Road	1,760.00
	PR83	Leath Road rehabilitation	880.00
	PR85	Ocean Street rehabilitation	880.00
	PR82	Mandurah Road rehabilitation	1,760.00
	PR84	Marking for Gilmore Ave pedestrian crossing	1,204.50
Public Transport Authority of Weste	1692	Bus shelter contribution various sites	25,148.75
	1693	Contribution towards new footpath various sites	6,502.65
Kyocera Document Solutions Australi	90148771	Copy costs Dec John Wellard staff	27.19
	90148769	Copy costs Jan William Bertram staff	36.61
	90148768	Copy costs Jan John Wellard public	20.65
	90148767	Copy costs Jan William Bertram Community	15.98
	90148766	Copy costs Jan Darius Wells downstairs	16.97
	90148770	Copy costs Jan Banksia Park Village	41.90
	90148765	Copy costs Jan Admin CSO	65.67
	90148763	Copy costs Jan Depot demountable	92.72

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
	90148756	Copy costs Jan Admin Planning	322.78
	90148757	Copy costs Jan Admin Governance	196.41
	90148758	Copy costs Jan Admin Finance	357.70
	90148764	Copy costs Jan Admin Records	266.66
	90148772	Copy costs Jan Recquatic Front Counter	19.42
	90148753	Copy costs Jan The Zone staff	39.89
	90148759	Copy costs Jan Family Day Care	116.61
	90148762	Copy costs Jan Depot Admin	79.70
	90148761	Copy costs Jan Recquatic	288.30
	90148760	Copy costs Jan Library Public	54.55
	90148752	Copy costs Jan IT	62.35
	90148755	Copy costs Jan Library Staff	147.15
	90148750	Copy costs Jan City Leadership Team	96.55
	90148754	Copy costs Jan Zone Multimedia Room	11.90
	90148751	Copy costs Jan Darius Community Centre	44.20
Taylor Tyres Pty Ltd	22853	Puncture repair	66.00
	22852	New tyre for vehicle	209.00
	22850	Two new tyres	455.40
	22986	Two tyres for KWN005	941.60
	22955	Puncture repair for 1KWN624	198.00
Construction Training Fund	03February21	CTF levy for Jan 21	7,917.20
Cornerstone Legal	17634	Legal fees matter No 005285	1,553.20
	17895	Legal fees matter No 005330	1,097.80
Palm Lakes Gardens & Landscape Serv	20010	Callistemon U52 and U53 repairs to sprinklers	110.00
Beaver Tree Services Aust Pty Ltd	76210	General vegetation clearance various sites	71,127.49
	76298	Root barrier install Brougham Crescent	471.70
	76299	Tree pruning Maddox Place	755.85
	76300	General vegetation clearance various sites	21,226.59
	76251	General vegetation clearance various sites	2,470.82
	76292	Supply and apply fertilisers to various sites	3,432.26
	76252	Tree pruning Leasham Court	555.94
	76253	Tree renewal of English Oak tree	2,420.00
	76254	Tree pruning Pace Road	445.87
	76311	City wide watering 220121	4,015.86
	76255	City wide watering 150121	4,015.86
	76256	City wide watering 080121	4,015.86
Asbestos Masters WA	3445	Asbestos removal Bombay Blvd and Orton Road	880.00
Suez	41254285	Greenwaste bin and tipping fees to Jan 21	735.15
Synergy	129764890Feb21	6385U Lambeth Park	1,334.98
	693987550Feb21	0U Challenger Beach toilets	119.91
	792417950Feb21	Street lighting	118,804.20
	856518550Feb21	Decorative lighting	2,741.26
	107029100Feb21	5532U Wellard Community Centre	1,189.87
	179469390Feb21	2422U Bertram Community Centre	614.17
Bob Jane Corporation Pty Ltd	0096702990	Puncture repair for KWN1942	23.10
	0096683222	Tyres and wheel alignment for 1GIW953	1,068.99
	0096689888	Tyre fitted to KWN1934	236.01
Prestige Catering & Event Hire	INV-2279	OCM Dinners 270121	360.00
Allcom Communications	30538	Re-installation of radio from KWN2026 to KWN2150	314.05
	30537	Re route antenna cable in KWN2073	237.05
Bunnings Building Supplies	2163/01111020	Purchase of hoses and parts for dog exercise area	176.25
	2163/01049988	Garden sprayer for City Operations Team	36.00
Ixom Operations Pty Ltd	6344254	Chlorine gas supply	126.85

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Technology One Limited	195917	OneCouncil implementation fees	3,850.00
	196858	OneCouncil implementation fees	5,445.00
Natural Area Holdings P/L t/as Natu	00014653	Peel sub and drain landscaping progress Jan 21	2,475.00
Wilson Security Pty Ltd	W00272465	Callistemon Court security patrols Jan 21	889.19
Cannon Hygiene Australia Pty Ltd	96961996	Hygiene services 060221 to 050321	1,519.46
Challenge Chemicals Australia	106968	2 x 5kg clean fry	51.59
	106801	Spicesan for animal services	87.78
Outsource Business Support Solution	00001640	Temp staff week ending 040221	2,815.52
Marketforce Pty Ltd	36602	Advertising 231220	350.55
	36969	Advertising 060121	352.75
	36972	Advertising 270121	175.66
	36967	Advertising 130121 and 270121	2,101.00
	36977	Advertising 220121	247.50
	36975	Advertising 200121	247.50
	36973	Advertising 200121	247.50
	36971	Advertising 140121	247.50
	36970	Advertising 140121	247.50
	36968	Advertising 130121	220.00
	36976	Advertising 200121	247.50
	36966	Displays for December and January for Recquatic	1,133.00
	Daniels Printing Craftsmen	70519	Bin tagging tags
AAA Blinds Port Kennedy	7700	Callistemon U15 repair broken wand carrier	108.00
City of Rockingham	115142	General waste Jan 21	985.32
HECS Fire	73425	Banksia Clubhouse investigate fire panel alarm	484.00
Coastline Mowers	26719#7	Set of mower blades x 2	339.50
KLMedia Pty Ltd	1148171	DVD for Library requests	217.95
Foreshore Rehabilitation & Fencing	INV-4841	Extension to fencing at Peace Park	4,111.47
Cockburn Party Hire	00007320	Australia Day Citizenship chair and parasol hire	1,192.25
	00006480	Equipment for Christmas function	1,392.50
Advanced Traffic Management (WA) Pt	00142787	Traffic management for Thomas Road retic works	7,134.50
	00142738	Traffic management for Sunrise Boulevard	854.60
	00142741	Traffic management for Treeby Road	16,667.16
	00142910	14mm concrete for Calista Ave	3,327.26
	00142737	Traffic management Pace Road	12,095.03
	00142912	Traffic management Pace Road	9,994.51
	00143089	Traffic management Pace Road	6,756.07
	00143090	Traffic management Pace Road	5,701.40
LD Total	111114	Landscape maintenance various locations Jan 21	33,035.81
	111099	Landscape maintenance various locations Jan 21	8,872.47
	111097	Landscape maintenance various locations Jan 21	5,078.39
	111101	Landscape maintenance various locations Jan 21	15,981.61
	111108	Landscape maintenance various locations Jan 21	9,370.17
	111116	Landscape maintenance various locations Jan 21	1,986.75
	111103	Landscape maintenance various locations Jan 21	11,447.15
	111112	Landscape maintenance various locations Jan 21	2,382.64
	111106	Landscape maintenance various locations Jan 21	824.44
	111110	Landscape maintenance various locations Jan 21	6,378.28
	111104	Landscape maintenance various locations Jan 21	2,755.48
	110919	Additional maintenance at various locations Jan 21	2,695.00
	110920	Additional maintenance at various locations Jan 21	2,524.50
	110907	Landscape maintenance various locations Jan 21	8,111.24
	110932	Additional maintenance at various locations Jan 21	288.39

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
	110934	Additional maintenance at various locations Jan 21	863.90
	110906	Landscape maintenance various locations Jan 21	2,927.09
	110905	Additional maintenance at various locations Jan 21	1,658.49
	110930	Additional maintenance at various locations Jan 21	180.89
	110933	Additional maintenance at various locations Jan 21	824.34
	110960	Additional maintenance at various locations Jan 21	279.61
	111109	Landscape maintenance various locations Jan 21	1,886.81
	111105	Landscape maintenance various locations Jan 21	234.65
	111111	Landscape maintenance various locations Jan 21	664.20
	111102	Landscape maintenance various locations Jan 21	3,601.76
	111115	Landscape maintenance various locations Jan 21	352.17
	111107	Landscape maintenance various locations Jan 21	2,813.79
	111100	Landscape maintenance various locations Jan 21	4,745.31
	111096	Landscape maintenance various locations Jan 21	1,131.09
	111098	Landscape maintenance various locations Jan 21	2,503.95
	111113	Landscape maintenance various locations Jan 21	9,374.15
ED Property Services	00001698	Banksia V34 and V56 gutter repairs	1,100.00
	00001699	Callistemon Court U14 install downpipe	330.00
	00001700	Banksia V21 repairs to burst pipe in ceiling	572.00
	00001695	Callistemon U44 replace tiles on roof	165.00
	00001689	Callistemon U11 repairs to cracks in walls	550.00
	00001690	Banksia Clubhouse V38 39 40 install base of fence	770.00
	00001694	Callistemon U31 install exhaust fan	165.00
Rockingham Skylights	12589	Banksia V38 reseal skylight	75.00
	12570	Banksia V18 remove window tint	150.00
T J Depiazzi & Sons	112853	Supply and deliver pinebark mulch to Depot	3,134.45
Statewide Cleaning Supplies	S1409474	Fit wipes for Recquatic Gym and Group Fitness	787.60
Elliotts Irrigation Pty Ltd	B21312	Iron filter servicing for Jan 21	3,071.20
Complete Office Supplies Pty Ltd	09840852	Stationery for City Operations Team	324.41
Big W	176118	Animal Services	124.75
	176116	New release books for Library	109.00
	176121	Food for Animal Services	165.60
Debra Evelyn Newell	09February21	Payment of principal Christmas Saver	300.00
Goodchild Enterprises	591001	Batteries for City Operations Team	523.60
Accord Security Pty Ltd	00025494	Security services for Covid Lockdown	198.00
	00025487	Alarm attendance for various facilities Jan 21	1,122.00
	00025486	Cash handling services for Jan 21	643.50
ALSCO Pty Ltd	CPER2106415	Linen hire for OCM Council dinners	59.63
	CPER2103975	Linen hire for OCM Council dinners	59.63
Castrol Lubricants	25110919	Supply of Magnatec oil and diesel oil	3,597.18
Woolworths Group Limited	4024098	Items for Mooditj Kulungars and Admin	60.60
	82000305	Items for Admin and OCM Dinners	100.36
	81873246	Items for City Operations Team	116.30
	81874217	Items for City Operations Team	71.70
Elexacom	112951	Street lighting repairs Gilmore Ave	30,000.00
	112994	Banksia V31 repairs to rangehood in kitchen	269.50
	112990	Callistemon U76 replace powerpoint in laundry	98.38
Home Group WA Pty Ltd	Refund	Invoice 63845 incorrectly paid	2,693.42
Total Green Recycling	INV9219	Collection of batteries and globes	159.72
Gregs Glass	10748	Banksia V27 adjust pivot on shower door	180.00
Sprayking WA Pty Ltd	00001959	Treatment of weeds Daintree	451.96
	00001957	Hardstand weed control various locations Dec 20	9,028.03

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
	00001958	Weed control in streetscapes various locations Jan 21	3,606.03
	00001961	Weed control in streetscapes various locations Jan 21	3,227.37
	00001960	Hardstand weed control various locations Jan 21	13,097.97
Australia Post	1010209155	Postage for period ending Dec 20	1,593.09
Harmony Software	3-762	Harmony Web Educator subscription Dec 20	821.70
	3-776	In Home Care Educator subscription Dec 20	254.10
Heatley Sales Pty Ltd	C967104	Uniforms for City Operations Team	345.58
	C968300	Uniforms for City Operations Team	915.59
The Village at Wellard Residents	03February21	2020 Covid relief contribution	1,000.00
Cubic Promote	INV-4812	Purchase of face masks	2,734.60
Pickles Auctions	DI000213230	Vehicle impound fee	121.00
Lisa Ann Crouch	09February21	Payment of principal and interest Christmas Saver	150.00
Blackwood & Sons Ltd	KW7839YV	Banksia Park no parking signs	71.80
	KW7836YV	Banksia Park no parking signs for verge	502.58
	KW7511YW	Gloves for City Operations Team	56.43
	KW5990YW	Face masks for City Operations Team	178.20
	PE5471YX	Repellent spray for City Operations Team	282.22
	PE6402YW	Do Not Enter danger tape	374.00
	PE6403YW	Face masks for City Operations Team	8.80
Flex Industries Pty Ltd	1013292	Airconditioner repairs KWN2040	991.21
Jason Signmakers	215733	Spanners x 2	26.73
Frontline Fire and Rescue Equipment	70053	DFES removable panel sets	43.74
GreenLite Electrical Contractors Pt	00013473	Investigate pump at Chipperton Lake	760.60
	00013474	Repair pump at Lyon Road Reserve	277.20
	00013460	Install new pump at Honeywood Oval	4,989.33
	00013475	Replace level probes at Town Park	292.84
K Mart	269937	Items for Bright Futures Day Care	94.00
Kwinana Bowling Club Inc	156897	Catering for Community Engagement Team Building	700.00
Poolwerx Spearwood	INVSPE2985	Splash pad water quality test Adventure Park Jan 21	1,981.54
Envirosweep	83683	Road sweeping for Pace Road Jan 21	363.00
	83796	Road sweeping for Anketell Jan 21	128.00
	83185	Carpark sweeping for Jan 21	2,502.50
	83205	Road sweeping various locations for Jan 21	4,097.50
	83206	Footpath sweeping various locations Jan 21	3,652.00
Purearth	INV-1117	Road sweeping and tip fees	2,435.07
Landscape and Maintenance Solutions	INV-1952	Mowing of various dryland reserves for Nov 20	9,778.69
	INV-1982	Passive and streetscape mowing various Jan 21	8,502.21
	INV-1983	Mowing maintenance Rockingham/Patterson Rd Jan 21	1,980.53
	INV-1984	Broadacre mowing of Sportsgrounds Jan 21	15,507.88
Vocus Communications	P674197	Monthly internet services Feb 21	1,694.00
McLeods	117573	Legal fees matter 46940	1,496.17
	117574	Legal fees matter 46997	1,445.68
Department of Mines, Industry	03February21	Building Services Levy January 21	21,202.33
Kearns Garden Supplies	24January21	Hardware for City Operations Team	99.56
Air Liquide Australia	XE4826	Hire of medical oxygen Recquatic	50.56
Lo-Go Appointments	00423010	Temp staff week ending 300121	1,240.15
Natsales Advertising Pty Ltd	00345106	Replacement of OMG panels	192.50
Charles Service Company	00033399	Cleaning services at various locations Jan 21	45,010.72
	00033509	Cleaning consumables for Jan 21	5,215.01

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Micro Focus Australia Pty Ltd	IAUSA44507	CM9 licence and support renewal to 310122	25,308.72
EFT TRANSFER: - 11/02/2021			234,466.63
Australian Services Union	PY01-17-Aust Ser	Payroll Deduction	275.98
	PY01-17-Aust Ser	Payroll Deduction	71.70
Australian Taxation Office			192,753.00
	PY01-17-Australi	PAYG tax withheld	7,299.00
	PY99-17-Australi	PAYG tax withheld	12,912.00
Maxxia Pty Ltd			136.19
	100329020210131	Employee net ITC for 010121 to 310121	1,110.47
	PY01-17-Maxxia P	Payroll Deduction	1,292.58
	PY01-17-Maxxia P	Payroll Deduction	1,004.55
Health Insurance Fund of WA (HIF)			7,710.00
	PY01-17-Health I	Payroll Deduction	1,099.19
City of Kwinana - Xmas fund			348.78
	PY01-17-TOK Chri	Payroll Deduction	5,826.86
Child Support Agency			2,256.55
	PY01-17-Child Su	Payroll Deduction	359.52
Easifleet			10.26
	142534	Novated lease charges 100221	5,000.00
	142168	Novated lease charges 010221	5,000.00
	141914	Novated lease charges 270121	252,591.10
LGRCEU			
	PY01-17-LGREC U	Payroll Deduction	
	PY01-17-LGREC U	Payroll Deduction	
EFT TRANSFER: - 11/02/2021			
Lord Mayor's Distress Relief Fund	11February21	2021 Bushfire donation	
EFT TRANSFER: - 12/02/2021			



# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount	
SuperChoice	January2021-01	Superannuation-January2021-01	174,491.54	
	January2021-03	Superannuation-January2021-03	4,617.90	
	January2021-06	Superannuation-January2021-06	1,143.16	
	January2021-07	Superannuation-January2021-07	3,369.47	
	January2021-13	Superannuation-January2021-13	230.74	
	January2021-14	Superannuation-January2021-14	9,041.70	
	January2021-16	Superannuation-January2021-16	590.90	
	January2021-17	Superannuation-January2021-17	19,738.60	
	January2021-18	Superannuation-January2021-18	1,361.49	
	January2021-21	Superannuation-January2021-21	2,301.35	
	January2021-22	Superannuation-January2021-22	274.54	
	January2021-24	Superannuation-January2021-24	696.15	
	January2021-28	Superannuation-January2021-28	64.52	
	January2021-30	Superannuation-January2021-30	1,124.06	
	January2021-32	Superannuation-January2021-32	2,185.31	
	January2021-48	Superannuation-January2021-48	481.27	
	January2021-49	Superannuation-January2021-49	1,010.95	
	January2021-50	Superannuation-January2021-50	2,031.59	
	January2021-53	Superannuation-January2021-53	294.48	
	January2021-54	Superannuation-January2021-54	1,812.62	
	January2021-55	Superannuation-January2021-55	3,048.22	
	January2021-56	Superannuation-January2021-56	1,193.86	
	January2021-58	Superannuation-January2021-58	994.30	
	January2021-59	Superannuation-January2021-59	1,113.77	
	January2021-60	Superannuation-January2021-60	1,184.76	
	January2021-61	Superannuation-January2021-61	522.86	
	January2021-63	Superannuation-January2021-63	263.93	
	January2021-64	Superannuation-January2021-64	793.80	
	January2021-66	Superannuation-January2021-66	461.26	
	January2021-70	Superannuation-January2021-70	600.40	
	January2021-72	Superannuation-January2021-72	1,003.36	
	January2021-73	Superannuation-January2021-73	649.45	
	January2021-75	Superannuation-January2021-75	308.43	
	January2021-79	Superannuation-January2021-79	1,175.41	
	January2021-81	Superannuation-January2021-81	809.98	
	January2021-83	Superannuation-January2021-83	1,709.64	
	January2021-84	Superannuation-January2021-84	121.15	
	January2021-85	Superannuation-January2021-85	804.13	
	January2021-89	Superannuation-January2021-89	1,586.35	
	January2021-90	Superannuation-January2021-90	956.87	
	January2021-91	Superannuation-January2021-91	905.49	
	January2021-92	Superannuation-January2021-92	1,696.92	
	January2021-94	Superannuation-January2021-94	2,195.64	
	January2021-95	Superannuation-January2021-95	320.83	
	January2021-96	Superannuation-January2021-96	303.21	
	January2021-97	Superannuation-January2021-97	762.54	
	January2021-99	Superannuation-January2021-99	242.20	
	EFT TRANSFER: - 17/02/2021			40,569.70
	Bright Futures Family Day Care - Pa	080221 to 140221	FDC Payroll 080221 to 140221	40,569.70
	EFT TRANSFER: - 18/02/2021			785,167.54

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Canon Production Printing Australia	1515538	Colorwave 3500 scanner charges Jan 21	288.75
Summers Consulting	INV-905	Mosquito monitoring 040221	1,500.40
Wheellie Clean	7836	Rubbish bin clean for Admin Building	267.52
	7837	Rubbish bin clean for Darius Wells	369.38
Rockingham Toyota	PI22015333	Radiator complete with hose	449.66
	RI21201060	2020 Toyota HiAce Van auto	52,784.25
McGees Property	28340	Market valuation for new lease at Koorliny Arts	1,375.00
Outback Handyman	2935	Callistemon and Banksia Park maintenance	616.00
Kleenheat	4052235	Gas charges various locations January 2021	4,447.95
Sonic Health Plus	2254640	Pre-employment medical assessment 080221	325.60
One 20 Productions and Phase 1 Audi	13776	Hire of two trucks for The Lolly Run	29,370.66
Amy Louise Dixon	10February21	Refund bond on The Patio	39.00
	1735804	Refund bond on The Patio	100.00
Red Sand Supplies Pty Ltd	00013329	Concrete tipping Pace Road	662.20
WA Carmax Pty Ltd	RI11301228	2020 Isuzu D-Max 4x4 space cab ute	46,069.45
Jennifer Lee Marslen	08February21	Items for morning tea for staff farewell	200.40
Rockingham Holden	GMCSR273621	Fuel water sensor for KWN2129	273.45
Burson Automotive Pty Ltd	111226992	12 volt battery for City Operations Team	246.40
	111232758	Air filter pumps	42.90
Mackie Plumbing and Gas Pty Ltd	196614	Unblock drain at Dog Pound	162.97
	196615	Investigated alarm fault at Thomas Kelly Pavilion	221.10
	196619	Investigate cistern in hydropool female toilet	73.70
	196620	Repair drains at Kwinana Dog Pound	248.81
	196621	Repair toilets at Wandi Resource Centre	371.60
	196625	Repairs to Wandi Hall toilets	162.47
	196703	Repairs to water tank filler Wandi Hall	653.94
	196705	Repairs to stormwater pumps at Wandi Pavilion	110.55
	196527	Inspect fire system at John Wellard Community	73.70
	196607	Repair water fountain Honeywood Park	78.08
	196613	Repair leaking cistern at Darius Wells	418.21
	196606	Banksia V21 repair pipes in roof	242.06
	196612	Banksia V24 repair leaking tap	176.81
	196610	Callistemon U66 ignite hot water system	159.49
	196701	Callistemon U36 repair leaking garden tap	107.23
	196706	Banksia V46 replace laundry taps	217.78
196616	Banksia V17 install new laundry taps	180.84	
196458	Callistemon U11 replace laundry taps	423.76	
Lewis Winter Fire & Emergency Engag	2101	Emergency Recovery Plans Development review	8,200.00
Friends of the Community Inc.	432	Catering for Staying on Your Feet event	340.00
Australian HVAC Services Pty Ltd	59614	Repairs to airconditioner at Business Incubator	115.50
One Degree Advisory Pty Ltd	2021-245	Consultancy fees Koorliny Centre	1,512.50
NORDA Architects Pty Ltd	2102-03	Kwinana South BFB tender preparation stage 1	8,277.00
Anna Kelly	11February21	Bertram Place activities	400.00
Future Physio	11February21	Cancellation of hire Wellard Pavilion	234.00
	1800562	Refund on bond	300.00
Satellite Security Services	IV009662	Repairs to automatic doors The Zone	505.80
Payreq Australia Pty Ltd	1004486	Pre purchased credits for rates notices	1,320.00
Element Advisory Pty Ltd	52746	Mandogalup Precinct Concept review	22,902.00
Dennis Panel & Paint	12	Recquatic Gym painting equipment	957.00

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Wellard Management Pty Ltd	1783443	Refund crossover bond	2,437.50
Kristy Watson	1792534	Refund bond on The Patio	100.00
Karthik Subramanian	08February2021	Lyrik Scholarship Payment	375.00
Evelyn Lanting Paz	07February21	Refund of stadium hire on 070221	245.00
Gayathiri Schultz	16February21	Swimming lesson refund for Term 1	104.60
St John Ambulance Australia (WA) In	FAINV00731760	First aid training 080220	128.00
Telstra	9385375010Feb21	Internet and data to 240221	2,491.62
	1170168000Jan21	Usage to 160121 corporate phone account	37,551.35
	1355246271Jan21	Mobile device whole organisation Jan 21	9,316.60
T-Quip	98594#12	Various items for City Operations Team	615.60
Trisley's Hydraulic Services Pty Lt	100203396	Air release valve for Recquatic pools	627.00
WA Limestone Co	BY5124/01	Supply 395t road base to City Operations Team	4,355.23
Water Corporation of Western Austra	9022594998Feb21	51U Honeywood Oval drinking fountain	131.63
Rubek Automatic Doors	00026801	Inspect fault at Parmelia House doors	242.00
Westbooks	319098	Adult Library books	11.09
	319099	Adult Library books	88.00
	319100	Junior Library books	11.09
	319103	Adult and junior Library books	81.78
	319104	Library adult books	41.02
	319105	Library adult books	24.41
	319106	Adult and junior damaged books	24.57
	319107	Adult Library books	14.79
	319101	Junior Library books	24.95
	319108	Adult Library books	14.79
	319110	Adult Library books	111.30
	319111	Adult Library books	83.61
	319102	Junior Library books	13.99
	319109	Adult Library books	60.47
	319335	Adult Library books	74.77
	319334	Adult Library books	14.79
	319338	Junior Library books	11.09
	319337	Adult Library books	36.99
	319336	Adult Library books	29.59
	319332	Adult and junior Library books	224.05
	319333	Adult Library books	476.17
Catalyse Pty Ltd	INV-1143	Community Wellbeing and Services score cards	33,000.00
Sandra Elizabeth Lee	May - Nov 2020	Reimbursement of travel expenses May to Nov 2020	266.57
	Nov - Jan 2021	Reimbursement of travel expenses Nov to Jan 2021	110.88
Officeworks BusinessDirect	15233745	Paper towel and hand wash for Mandogalup	123.21
Palm Lakes Gardens & Landscape Serv	88441	Callistemon Court general vegetation pruning	425.00
	124587	Repairs to splash down at Kwinana Adventure Park	715.00
Synergy	830669340Feb21	16U Wandii fire pump	126.35
	294827290Feb21	12U Art sculpture Darling Park	125.19
	294428370Feb21	663U Mornington Park	312.07
	290833680Feb21	23U Honeywood Park BBQ/Lights	128.35
	264244690Feb21	2566U Gecko Park bore	858.36
	259587970Feb21	2731U Honeywood Ave retic	905.73
	214467920Feb21	63U Honeywood/Bruny Meander bore/BBQ/lights	139.83
	198511040Feb21	2744U Bruny Meander bore/BBQ/lights	909.46
	422268910Feb21	5795U Depot	1,499.64
	258360080Feb21	6028U Kwinana Adventure Park	1,212.19
	177581220Feb21	97393U Recquatic	22,783.00

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
	149872970Feb21	5473U Incubator	1,411.84
	141057240Feb21	14972U The Zone	4,035.81
	135567600Feb21	46840U Darius Library/Resource Centre	11,961.42
	118367820Feb21	5953U New Thomas Oval Pavilion	1,316.05
	201813230Feb21	4894U Mandogalup Station/reserve bore	1,528.49
	938812910Feb21	8284U Wandi Reserve	1,826.42
	657514270Feb21	0U Wandi Oval	118.06
	114826720Feb21	228U Industrial s/scapes Naval Base	183.51
	295922660Feb21	1210U Willandra Park	465.41
	314938770Feb21	0U Woko Park bore	118.06
	129764890Feb21	Lambeth Park invoice adjustment	171.66
	129764890Feb21	6331U Lambeth Park new charges	1,272.79
	198694990Feb21	46512U Admin/Arts/Parmelia	12,058.52
	566370150Feb21	8856U Chipperton Park	1,223.98
	144372270Feb21	8948U Thomas Oval retic	1,143.23
	521814530Feb21	8270U Wellard Pavilion	1,320.73
Prestige Catering & Event Hire	INV-2296	Catering 100221	405.80
ABCO Products	660325	Toilet roll dispensers for Recquatic Centre	262.77
	662765	Covid supplies for Recquatic Centre	298.65
Bunnings Building Supplies	2163/01518726	Equipment for Recquatic and Adventure Park	153.58
	2163/01051184	Site contractors lock box	82.65
	2163/01516424	Equipment for Facilities and City Operations Teams	136.24
	2163/01067495	Cleaning supplies for public toilets	70.22
	2163/01611531	Various items for Recquatic Centre	103.25
	2163/99835233	Cabinets for bathroom	877.80
Technology One Limited	196934	OneCouncil implementation fees	144,614.80
Sigma Chemicals	145635/01	Backstroke flags for Recquatic pools	613.80
Natural Area Holdings P/L t/as Natu	00014397	Peel sub and drain landscaping completion	32,734.85
Downer EDI Works Pty Ltd	6010298	Supply road stabilisation services Pace Road	134,276.56
	6010300	Supply deliver and apply asphalt Pace Road	39,605.73
Connect CCS	00105061	After hours monitoring overcalls Jan 21	1,733.88
Bolinda Publishing Pty Ltd	220239	Audio mixed format books for Library	79.16
	220241	Audio mixed format books for Library	74.76
	220240	Large print books for Library	54.00
	219908	Audio mixed format books for Library	113.81
Outsource Business Support Solution	00001644	Temp staff week ending 140221	5,785.31
Marketforce Pty Ltd	36974	Advertising 270121	352.75
Signaltech	3851	Callistemon U22 install antenna and cabling	715.00
HECS Fire	73855	Repairs to fire alarm in Recquatic stadium	484.00
Coastline Mowers	26831#5	3 x mower batteries for City Operations Team	167.25
Eclipse Soils Pty Ltd	KWIN01R044227	General waste tipping Pace Road	5,208.50
Kwinana Industries Council	09February21	Cancellation of Ken Jackman Hall due to Covid	112.80
Winc Australia Pty Ltd	9035180676	Stationery for Admin Building	6.07
	9035179474	Stationery for Admin building	863.31
	9035179474	Stationery for Admin Building GST	86.33
	9035180676	Stationery for Admin Building GST	0.61
Advanced Traffic Management (WA) Pt	00142911	Traffic Management Ocean Street	1,278.66
David Mario Boccuzzi	15February21	Parks and Leisure subscription	203.00
ED Property Services	00001703	Banksia V40 maintenance of roof tiles	44.00
	00001704	Banksia V18 and V42 roof repairs progress payment	550.00
Rockingham Skylights	12592	Banksia V42 replace ceiling fan and skylight	150.00

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Eyeline Australia Pty Ltd	183413	Lifeguard bags Recquatic	108.90
JB HiFi Commercial Division	BD0386610	5 x iphones with 3 cases	3,622.76
West Oz Wildlife	2472	Animal Encounters for The Zone	423.50
Centrecare	24378	EAP annual registration and management fee	1,026.66
Master Lock Service	00007594	Callistemon U30 adjust security door strike	125.00
In Safe Hands Educators In Safety P	10642	Safe Sleep course for Educators	1,760.00
Zenien Pty Ltd T/as ATFT Astuta Tru	18355	CCTV maintenance February 21	1,890.02
ALSCO Pty Ltd	CPER2109051	Linen hire for OCM Council dinners	67.69
Woolworths Group Limited	4024093	Items for City Operations Team	56.00
	82517991	Items for Admin Building and staff meeting	103.44
	4200055	Items for The Zone	49.75
	4024095	Items for The Zone	120.70
	82328480	Supplies for City Operations Team	102.31
	4200058	Items for Mooditj and Family Day Care	124.13
	4024100	Items for Recquatic	40.35
Elexacom	112996	Repair light and replace fan Thomas Oval Pavilion	581.49
	112998	Supply and program remote to suite sliding gate	187.50
	112820	Inspect electricals at Thomas Netball changerooms	132.00
	112824	Investigate electricity to City Operations Team	226.75
	112983	Inspect ceiling lights at John Wellard Community	281.66
	112992	Repair pit lid at Wells Park	438.49
	113031	Callistemon Court repairs to reticulation pipe	154.00
	113002	Install wall mounted fans Mandogalup Fire Station	3,136.55
	113020	Repair air con circuit at Koorliny Art Centre	541.04
Daniel Wakeford Turrell	10February21	Reimbursement of HR Licence	44.05
Advance Scanning Services	20165573	Removal of pothole markers	418.00
	20165741	HP gas supervision Challenger Ave	2,838.00
Heatley Sales Pty Ltd	C969029	Uniforms for City Operations Team	149.60
Quantum Building Services	00003752	Roof inspections on various buildings	5,148.00
HP Financial Services Pty Ltd	100001278668	Monthly payment contract 5389066248AUS1 Apr 21	2,216.50
	100001277939	Monthly payment contract 5389066248AUS2 Apr 21	15,829.24
	100001277940	Monthly payment contract 5389066248AUS3 Apr 21	588.85
Morris Jacobs	23 04/02/21	Art classes at Darius Wells	200.00
Blackwood & Sons Ltd	KW7579YX	Items for Recquatic Centre	25.63
	PE9714YZ	Repellent for Admin Building	176.39
Jason Signmakers	214964	Parking signs for Wellard Primary School	1,214.75
Walter J Pratt Pty Ltd	612965	Ethernet status check on Hydrotherapy pool	200.00
Preview Industries Pty Ltd	A00059773	Stationery for Library	90.00
Skateboarding WA	INV-0961	Skateboarding coaching clinic for The Zone	550.00
Irrigation West	IV00000001093	Supply of SDS data coils	1,237.50
	IV00000001090	Supply of SDS data coils	1,237.50
Les Mills Australia	1114470	Les Mills licence fee Feb 21	982.37
Fridgair Industries Pty Ltd	37302	Repairs to fridge at Thomas Kelly Pavilion	247.50
McLeods	117572	Legal fees matter 46058	1,670.20
Kearns Garden Supplies	04January21	Hardware for Banksia Park	110.92
	03January21	Hardware for Kwinana Village	131.79
Lo-Go Appointments	00423041	Temp staff week ending 060221	834.96
Whimsical Wonders Hight Tea & Party	COK-DW010	Backdrop for photo displays	110.00
Taylorod to Suit	00001005	Christmas activation backdrop 2020	984.50

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Espresso Essential WA	SI-00215450	Items for Admin Building	127.90
Shari Maughan	1793548	Refund on bond The Patio	100.00
Copyright Agency Limited	INV00013013	City of Kwinana copyright access licence 20 to 21	7,535.68
EFT TRANSFER: - 18/02/2021			15,270.79
Bright Futures In Home Care - Payro	080221 to 140221	IHC Payroll 080221 to 140221	15,270.79
EFT TRANSFER: - 24/02/2021			48,900.77
Bright Futures Family Day Care - Pa	150221 to 210221	FDC Payroll 150221 to 210221	36,125.64
Bright Futures In Home Care - Payro	150221 to 210221	IHC Payroll 150221 to 210221	12,775.13
EFT TRANSFER: - 25/02/2021			890,232.47

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Pointform Pty Ltd	9.9	Rates Refund	6,347.21
Nilfisk Pty Ltd	PRI0003681	Rental of battery floor scrubber Recquatic Feb 21	660.00
Drive Safe Australia (WA)	00016629	Driver training 130221 x 8 staff	2,000.00
Workpower Incorporated	PS15439	Plants delivered to City Operations Team	1,839.20
Netstar Australia Pty Ltd	110088	EZ subscription advanced website fee	2,364.12
Outback Handyman	2942	Callistemon U48 repairs to tiles in kitchen	396.00
	2941	Callistemon Court and Banksia Park maintenance	616.00
William Buck Audit (WA) Pty Ltd	3838	Audit fee for 300619 and 300620	7,700.00
Kissane & Co	INV-200185	Skip trace and service of minor case claim	160.00
Nightlife Music Pty Ltd	564721	Monthly payment for music at Recquatic Jan 21	517.00
Mackie Plumbing and Gas Pty Ltd	196740	Banksia V11 replace taps to laundry	336.80
	196753	Banksia V31 clear blocked basin in bathroom	267.70
	196748	Callistemon U33 service of toilet	154.81
	196754	Callistemon U22 repair leaking spouts	604.59
Premier & Cabinet Department of	1000348	Local Planning Scheme 2	263.28
	1000326	Local Planning Scheme 2	226.20
Royal Life Saving Society	132694	Home pool barrier inspections 010121 to 310121	4,048.00
Robertson Hayles Lawyers Pty Ltd	013379	Legal fees matter 202009378	7,353.50
Inside Art Space	1021	Art classes at John Wellard Centre	203.50
Grow It Local Pty Ltd	00000020	Grow It Local subscription	5,500.00
Klaybles	0108	Cutting and printing of shapes for Library	50.00
Richard Pascal	1780543	Refund Bond on The Patio	100.00
Duane Joseph Louis	9.8	Rates Refund	330.00
Darren Phillip Lee	983370	Refund 360L recycle bin upgrade fee	50.00
Suvarna Mithun	1800606	Refund bond on The Patio	100.00
	18February21	Cancellation of The Patio hire on 270221	39.00
Motive Building Group	18February21	Reimbursement of Planning application	576.00
Dananjani Wickramasingha Mudiyansele	A/N3230	Crossover subsidy rebate	730.80
Rohit Ramling Tekale	A/N18225	Crossover subsidy rebate	669.90
Natasha Joy Veurink	A/N23923	Crossover subsidy rebate	365.40
Harjit Singh Virk & Daljit Kaur Vir	A/N24358	Crossover subsidy rebate	548.10
Xavier Red Ragas	A/N22597	Crossover subsidy rebate	548.10
Jessica Bachman	23February21	Neighbour Day Everyday 2021	150.00
Ronald William Wilkins	23February21	Seniors security subsidy scheme 2020/2021	250.00
Philip Leslie Joseph	23February21	Seniors security subsidy scheme 2020/2021	250.00
Janice Mary Donnelly	23February21	Seniors security subsidy scheme 2020/2021	250.00
Robyn Kathleen Bartlett	23February21	Seniors security subsidy scheme 2020/2021	250.00
Ryan Charles Masters	23February21	Neighbour Day Everyday 2021	150.00
Patricia Lee Clarke	23February21	Seniors security subsidy scheme 2020/2021	250.00
Janet Anne Menhennett	23February21	Seniors security subsidy scheme 2020/2021	250.00
Mary Grace Romeo Bugarin	1800564	Refund bond on Ken Jackman Hall	1,000.00
Yasmin Sadat Eghtesadi	1826045	Refund bond on Hovea room	200.00
Tamara Baran	1809310	Refund bond on The Patio	100.00
Danielle Jones	1800826	Refund bond on The Patio	100.00
Cody Evans	1772157	Refund bond on The Patio	100.00
Nikki Whalley	1782621	Refund bond on The Patio	100.00

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Michelle Player	1809274	Refund bond on The Patio	100.00
Michelle Blevins	1746988	Refund bond on The Patio	100.00
Di Zhang	1803025	Refund bond on The Patio	100.00
Stacey Lee Norrish	9.9	Rates Refund	893.92
Land Investors Alliance No1 Pty Ltd	9.9	Rates Refund	809.42
Australia Post	1010276056	Agency commission fees to 310121	371.27
St John Ambulance Australia (WA) In	FAINV00736378	First aid training 110221	128.00
Trisley's Hydraulic Services Pty Lt	100203394	Bi monthly service to pools at Recquatic	12,905.39
	100203398	Chlorine gas lead washers for Recquatic	137.50
	100203399	Repairs to Hydro Pool gas booster pump	275.00
	100203401	Replace booster pool pump Recquatic	110.00
WA Electoral Commission	18February21	Duplicate payment for Ken Jackman Hall	84.60
Water Corporation of Western Austra	9000358855Feb21	47U Kwinana Senior Citizens Centre	484.58
	9000345691Feb21	21U Waste Stream	54.20
	9000343688Feb21	6U Little Rascals	288.24
	9000356366Feb21	31U Bright Futures	261.84
	9000354133Feb21	66U New Thomas Oval Pavilion/Tucker Street Reserve	1,624.99
	9000348470Feb21	615U Depot	2,041.51
	9000343469Feb21	0U Old St John Ambulance	90.92
	9000358839Feb21	8U Medina Hall	565.76
	9000358310Feb21	42U Medina Oval	880.79
	9000358337Feb21	5U Boy Scouts Hall	12.91
	9000354002Feb21	93U BMX Track amenities	330.95
	9000357641Feb21	15U Orelia Oval	947.48
Dennis Cleve Wood	ICTALLOW20/21	ICT allowance	291.67
	MEETINGFEES20/21	Meeting fees	2,639.83
Sherilyn Wood	ICTALLOW20/21	ICT allowance	291.67
	MEETINGFEES20/21	Meeting fees	2,639.83
Sandra Elizabeth Lee	ICTALLOW20/21	ICT allowance	291.67
	MEETINGFEES20/21	Meeting fees	2,639.83
Absolute Painting Services	INV-1787	Banksia V42 repaint sliding doors	88.00
	INV-1788	Banksia V35-39 repaint exterior	8,000.00
Coles Group & Myer	CGC928141	Corporate gift cards for staff awards	1,364.85
Palm Lakes Gardens & Landscape Serv	2552	Callistemon U2 11 and 41 removal of shrubs	715.00
Suez	165464	Commercial recycle and waste for Jan 21	149,847.65
	41246007	Waste and recycling services Jan 21	174,816.20
	41280002	Waste and recycling services Jan 21	1,713.11
Synergy	151027420Feb21	208U Brandon Mews Reserve	177.77
	786264020Feb21	1671U Sutherland Pde/Chisham Ave bore	425.59
	809178030Feb21	334U Warner Road sump	213.94
	846031050Feb21	246U Street lights The Strand	188.68
	488212530Feb21	1526U Chisham Oval	401.19
	166216150Feb21	1024U Sloan Reserve bore pump 3	412.02
	479742820Feb21	248U Parmelia verge bore	189.26
	821437900Feb21	2U POS lighting near Wellard Village	118.64
	882174540Feb21	2U POS lighting The Strand	118.64
	144348440Feb21	304U Sutherland Pde bore	205.33
	958335710Feb21	9U Orelia Oval	1,946.08
	958312220Feb21	2016U Djilba Vew	478.18
Ixom Operations Pty Ltd	6348465	Chlorine gas supply	1,035.89
Sigma Chemicals	146194/01	Chemical order for Recquatic	748.00



# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Bolinda Publishing Pty Ltd	221238	Audio mixed format books for Library	418.59
Peter Edward Feasey	DEPMAYFEE20/21	Deputy Mayoral allowance	1,869.83
	ICTALLOW20/21	ICT allowance	291.67
	MEETINGFEES20/21	Meeting fees	2,639.83
Carol Elizabeth Adams	ICTALLOW20/21	ICT allowance	291.67
	MAYALLOW20/21	Mayoral allowance	7,479.42
	MEETING20/21	Meeting fees	3,959.67
WA Boiler Spares & Service	15783	Replace failed pump at Recquatic	3,619.17
Outsource Business Support Solution	00001649	Temp staff week ending 210221	5,245.35
Aegis Aged Care Group Pty Ltd	Refund	Duplicate payment invoice 64173	219.00
Daniels Printing Craftsmen	70673	Stickers for Admin Building	176.00
Carroll & Richardson Flagworld Pty	96487	Australian Flag	95.00
Civica Pty Ltd	M/LG018081	Licence support and maintenance to 300421	9,005.49
	M/LG016895	Licence support and maintenance to 301121	14,566.76
Western Maze Pty Ltd	00001210	Green waste collection area 1	17,941.75
MRP General Pest/Termite Division 4	97737	Pest control Darius Wells Library	139.70
Infiniti Group	514671	Consumables for Recquatic	1,015.96
Redink Homes Pty Ltd	Refund	Refund fees BP2021/146	760.92
Winc Australia Pty Ltd	9035225289	Stationery items for HR	81.18
	9034991354	Items for Business Incubator	11.70
	9034991491	Items for Business Incubator	10.91
	9035052260	Items for Business Incubator	10.25
	9035020500	Items for Business Incubator	2.93
Advanced Traffic Management (WA) Pt	00143292	Traffic Management Ocean Street	1,894.31
AC Cooling Services	8084	Banksia V42 service air conditioner	143.00
ED Property Services	00001710	Banksia Park roof repairs	770.00
JB Hi-Fi Rockingham	503180673-100	Adapter and headset for Recquatic Gym	61.00
Aveling Homes Pty Ltd	Refund	DA9903 application not required	706.46
Statewide Cleaning Supplies	S1412728	Fit wipes for Recquatic Gym and Group Fitness	787.60
Web In A Box	58803	Domain registration	55.00
	59531	Domain registration	270.00
Landgate	362585-10001098	GRV chargeable schedule No G2021/3	1,850.75
	362358-10001098	GRV chargeable schedule No G2021/2	1,605.06
	1074236	Landgate title search fees Jan 21	26.70
Big W	176132	Earbud headphones for Library	60.00
Compact Mobile Entertainment	2672	Medina Hall local organisation Christmas Party	850.00
Quell Clean	00053473	Banksia V42 vacate clean	360.00
Burdens Australia Pty Ltd	00059201	Park bench seat for Parkline Street	2,876.15
Wendy Gaye Cooper	ICTALLOW20/21	ICT allowance	291.67
	MEETINGFEES20/21	Meeting fees	2,639.83
Name Badge World	BW17416	Employee name badges	88.01
Programmed Property Services Pty Lt	SINV598444	BP and retirement gardening and lawn mowing Feb21	10,257.86
Schneider Electric	58658169	Maintenance agreement for Business Incubator UPS	6,326.93
West Coast Profilers	24095	Profiling works on Leath Rd	2,853.00
Zenien Pty Ltd T/as ATFT Astuta Tru	18361	CCTV for Darius Car Park	586.74
	18360	CCTV for The Zone	796.40
Woolworths Group Limited	80158943	Items for Cafe Splash Recquatic	65.50
	80621363	Items for Cafe Splash Recquatic	556.55

# Payment Listing

Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
	80997358	Items for Cafe Splash Recquatic	256.51
	81597557	Items for Cafe Splash Recquatic	253.20
	82324110	Items for Cafe Splash Recquatic	133.81
	79877154	Items for Cafe Splash Recquatic	404.45
	82982237	Items for Executive Staff meetings	100.10
	4200067	Items for Library	58.50
	4200060	Items for The Zone	10.03
Zip Heaters	3574190	Service water fountain at Recquatic Centre	108.90
Wellard Residential Pty Ltd	9.9	Rates Refund	3,626.98
Alyka Pty Ltd	INV-29006	Website design and development phase 1	25,042.05
	INV-29516	Website design and development phase 1	990.00
Morris Jacobs	23January21	Art Classes at Darius Wells 230221	200.00
Morgan James Scarfe	INV-285	OMG Fringe Festival stage 3 fees	12,952.50
Challenger Veterinary Hospital	99666	Animal services February 21	400.00
Strata Specialists	INV009803	Strata levies 010121 to 310321 U24 Meares Ave	2,375.00
	INV009802	Strata levies 010121 to 310321 U23 Meares Ave	2,850.00
Mervyn Thomas Kearney	ICTALLOW20/21	ICT allowance	291.67
	MEETINGFEES20/21	Meeting fees	2,639.83
Bushfire Prone Planning	BPP-20214	Bushfire plan review Bonney Rd	1,386.00
Kwinana in Transition	23February21	Neighbour Day 2021 funding program	2,000.00
Sienna Properties Pty Ltd	1735409	Refund early clearance bond	284,669.27
Planning Institute of Australia Pty	ED210227	Advertising for Senior Strategic Planner	330.00
Pitney Bowes Australia Pty Ltd	I6406931	Software for Rates folding machine	10,714.00
Dept of Local Government, Sport and	RI027439	Medina activation projects	3,353.90
Matthew James Rowse	ICTALLOW20/21	ICT allowance	291.67
	MEETINGFEES20/21	Meeting fees	2,639.83
Mad Dog Promotional Products Pty Lt	INV-2663	Uniforms for Fringe Festival	860.00
EFT TRANSFER: - 25/02/2021			222,740.95
Australian Services Union	PY01-18-Aust Ser	Payroll Deduction	275.98
	PY01-18-Aust Ser	Payroll Deduction	71.70
Australian Taxation Office	PY01-18-Australi	PAYG tax withheld	200,458.00
	PY99-20-Australi	PAYG tax withheld	2,139.00
	PY99-19-Australi	PAYG tax withheld	6,332.00
	PY99-18-Australi	PAYG tax withheld	528.00
Maxxia Pty Ltd	PY01-18-Maxxia P	Payroll Deduction	1,110.47
	PY01-18-Maxxia P	Payroll Deduction	1,292.58
Health Insurance Fund of WA (HIF)	PY01-18-Health I	Payroll Deduction	1,004.55
City of Kwinana - Xmas fund	PY01-18-TOK Chri	Payroll Deduction	7,710.00
Child Support Agency	PY01-18-Child Su	Payroll Deduction	1,100.11
Easifleet	142754	Novated lease charges 240221	348.78
LGRCEU	PY01-18-LGREC U	Payroll Deduction	359.52
	PY01-18-LGREC U	Payroll Deduction	10.26
<b>Total EFT</b>			<b>-4,069,894.83</b>

## Payroll

Payroll - Interim	23,497.09
Payroll - Interim	15,205.14
Payroll	625,715.94
Payroll - Interim	1,332.49

# Payment Listing



Payments made between 01/02/2021 and 28/02/2021

Payee	Invoice	Description	Amount
Payroll - Interim			15,830.25
Payroll - Interim			10,440.73
Payroll			652,244.41
<b>Total Payroll</b>			<b>-1,344,266.05</b>
<b>Grand Total</b>			<b>-5,662,545.44</b>

# Credit Card Transactions

1/02/2021 to 28/02/2021

Transaction No	Tran Type	Tran Reference	Invoice Date	Actual	Transaction Description
<b>Credit card Functions Officer to 040221</b>				<b>\$36.60</b>	
5060298	Invoice	040221A	04/02/2021	\$16.36	Flags for Australia Day Citizenship ceremony
5060298	Invoice	040221A	04/02/2021	\$18.60	Milk for staff room
5060298	GST	040221A	04/02/2021	\$1.64	GST
<b>Credit card Economic Development Manager to 040221</b>				<b>\$114.06</b>	
5060300	Invoice	040221B	04/02/2021	\$20.45	Coffee for Home Based Business Group
5060300	Invoice	040221B	04/02/2021	\$87.15	Morning tea items for staff farewell
5060300	GST	040221B	04/02/2021	\$6.46	GST
<b>Credit card Emergency Services Coordinator to 040221</b>				<b>\$609.50</b>	
5060302	Invoice	040221D	04/02/2021	\$554.09	Meals for firefighters
5060302	GST	040221D	04/02/2021	\$55.41	GST
<b>Credit card Director City Business to 040221</b>				<b>\$1,339.38</b>	
5060304	Invoice	040221E	04/02/2021	\$32.67	International transaction fee
5060304	Invoice	040221E	04/02/2021	\$1,306.71	Matterport yearly professional space processing
<b>Credit card Rates Coordinator to 040221</b>				<b>\$196.00</b>	
5060306	Invoice	040221H	04/02/2021	\$196.00	Court fees for document lodgements
<b>Credit card Director City Development and Sustainability to 040221</b>				<b>\$855.24</b>	
5060308	Invoice	040221C	04/02/2021	\$131.00	Corporate licensing requirements
5060308	Invoice	040221C	04/02/2021	\$158.49	Conference High Density Development
5060308	Invoice	040221C	04/02/2021	\$499.91	Team Building milestone event
5060308	GST	040221C	04/02/2021	\$65.84	GST
<b>Credit card Manager Governance to 040221</b>				<b>\$85.90</b>	
5060311	Invoice	040221G	04/02/2021	\$78.09	Flowers for staff member
5060311	GST	040221G	04/02/2021	\$7.81	GST
<b>Credit card Manager Human Resources to 040221</b>				<b>\$2,079.15</b>	
5060314	Invoice	040221I	04/02/2021	\$58.14	Hamper for staff member
5060314	Invoice	040221I	04/02/2021	\$70.00	Gift card for staff member
5060314	Invoice	040221I	04/02/2021	\$450.00	Training Ignite your Facebook

# Credit Card Transactions

1/02/2021 to 28/02/2021

Transaction No	Tran Type	Tran Reference	Invoice Date	Actual	Transaction Description
5060314	Invoice	040221I	04/02/2021	\$653.57	LinkedIn advertising
5060314	Invoice	040221I	04/02/2021	\$664.79	LinkedIn advertising
5060314	GST	040221I	04/02/2021	\$182.65	GST
<b>Credit card A/Manager Customer and Communications to 040221</b>				<b>\$2,625.28</b>	
5060317	Invoice	040221F	04/02/2021	\$1.14	International transaction fee
5060317	Invoice	040221F	04/02/2021	\$2.50	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$2.50	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$2.50	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$19.08	Google Ads
5060317	Invoice	040221F	04/02/2021	\$45.73	Typeform subscription
5060317	Invoice	040221F	04/02/2021	\$50.00	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$100.00	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$100.01	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$195.87	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$263.60	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$313.11	Mailchimp email marketing software
5060317	Invoice	040221F	04/02/2021	\$313.23	Facebook advertising
5060317	Invoice	040221F	04/02/2021	\$351.00	istock image pack
5060317	Invoice	040221F	04/02/2021	\$828.00	Survey Monkey
5060317	GST	040221F	04/02/2021	\$37.01	GST
<b>Grand Total:</b>				<b>\$7,941.11</b>	

### 18.3 City of Kwinana Local Emergency Management and Recovery Plans

#### **DECLARATION OF INTEREST:**

There were no declarations of interest declared.

#### **SUMMARY:**

The Local Emergency Management Committee (LEMC) at its 9 March 2021 meeting endorsed the City of Kwinana's Local Emergency Management and Recovery Plans 2021, including the respective Appendices. These documents are presented to Council for consideration and endorsement.

Council endorsed an interim Local Emergency Management Plan at its 14 February 2018 meeting, and this interim approval was endorsed pending a comprehensive risk review being undertaken by the City.

This review has now been undertaken and the revised and completed documents are now being presented to Council for consideration and adoption.

#### **OFFICER RECOMMENDATION:**

That Council endorse the Local Emergency Management and Recovery Plans and the associated Appendices as detailed in Confidential Attachment A and B.

#### **DISCUSSION:**

The LEMP and Recovery Plan and the associated appendices, are required to ensure that there are suitable plans in place to deal with identified emergencies should they arise and to ensure that there is an understanding between agencies and stakeholders in managing emergencies and recovery.

As endorsed by Council at its 14 February 2018 meeting, a full risk review of the interim plans has been undertaken and are attached as Confidential Attachment A and B. The LEMP and Recovery Plan now reflect these reviewed changes made and are set to replace the interim plans.

The key changes arising from the review include:

- Removal of unnecessary detail ( nice to have but not directly needed information at an emergency event )
- Inclusion of key detail needed for when the plans are to be implemented.
- Identified key action points or guides within both plans and highlighted throughout both documents in making it easier for staff to identified what they need to do at certain times with the emergency response or recovery process
- Overall revised format to ensure easy systematic guidance is provided to officers providing clarity around certain actions and responsibilities.
- Categorise areas within the document themselves for ease of access and reference points to key templates and guides
- Separation of the Emergency Management and Recovery Plans and their associated appendices to remove confusion between the two requirements

### 18.3 CITY OF KWINANNA LOCAL EMERGENCY MANAGEMENT AND RECOVERY PLANS

The Local Emergency Management Committee (LEMC) is established as a legislative requirement of the *Emergency Management Act 2005*.

The purpose of the LEMC is to:

- Provide advice to Council on all matters relating to emergency management within the City of Kwinana.
- Recommend appropriate action to Council on all matters relating to emergency management.
- Develop and maintain effective emergency management arrangements for the local area.
- Liaise with participating agencies in the development, review and testing of emergency management arrangements.
- Assist with the preparation of emergency management operating procedures for application in the local area.
- Prepare an annual report on Committee activities for submission to the State Emergency Management Committee.
- Participate in the emergency risk management process.
- Carry out other emergency management functions as directed by the State Emergency Management Committee.

The LEMC at its 9 March 2021 meeting endorsed the City of Kwinana's Local Emergency Management and Recovery Plans 2021, including the respective Appendices.

The *Emergency Management Act 2005*, section 41 states that a function of the local government is to ensure that effective local emergency management arrangements are prepared and maintained for its district; and to manage recovery following an emergency affecting the community in its district.

These documents are presented to Council for consideration and endorsement, following which the District and State Emergency Management Committees (DEMC and SEMC) will be provided with copies of these plans for noting.

#### LEGAL/POLICY IMPLICATIONS:

##### ***Emergency Management Act 2005***

##### *41. Emergency management arrangements in local government district*

- (1) *A local government is to ensure that arrangements (**local emergency management arrangements**) for emergency management in the local government's district are prepared.*
- (2) *The local emergency management arrangements are to set out —*
  - a) *the local government's policies for emergency management; and*
  - b) *the roles and responsibilities of public authorities and other persons involved in emergency management in the local government district; and*
  - c) *provisions about the coordination of emergency operations and activities relating to emergency management performed by the persons mentioned in paragraph (b); and*

### 18.3 CITY OF KWINANA LOCAL EMERGENCY MANAGEMENT AND RECOVERY PLANS

- d) *a description of emergencies that are likely to occur in the local government district; and*
  - e) *strategies and priorities for emergency management in the local government district; and*
  - f) *other matters about emergency management in the local government district prescribed by the regulations; and*
  - g) *other matters about emergency management in the local government district the local government considers appropriate.*
- (3) *Local emergency management arrangements are to be consistent with the State emergency management policies and State emergency management plans.*
- (4) *Local emergency management arrangements are to include a recovery plan and the nomination of a local recovery coordinator.*
- (5) *A local government is to deliver a copy of its local emergency management arrangements, and any amendment to the arrangements, to the SEMC as soon as is practicable after they are prepared.*

#### 42. *Reviewing and renewing local emergency management arrangements*

- (1) *A local government is to ensure that its local emergency management arrangements are reviewed in accordance with the procedures established by the SEMC.*
- (2) *Local emergency management arrangements may be amended or replaced whenever the local government considers it appropriate.*

#### 43. *Local emergency management arrangements to be available for inspection*

- (1) *A local government is to keep a copy of its local emergency management arrangements at the offices of the local government.*
- (2) *The arrangements are to be available for inspection, free of charge, by members of the public during office hours.*
- (3) *The arrangements may be made available in written or electronic form.*

#### **FINANCIAL/BUDGET IMPLICATIONS:**

There is no direct financial impact for the approval and or endorsement process; however, there are cost incurred in the printing and distribution of the completed document, to those identified key stakeholders linked to these plans and appendices

#### **ASSET MANAGEMENT IMPLICATIONS:**

There will be no asset management implications arising from the preparation of the LEMP and Recovery Plan. It should be noted that the LEMP and Recovery Plans have regard to the impact of any emergency on the City's assets and seeks to manage those impacts to ensure financial risks and risks of public injury are mitigated



## 18.3 CITY OF KWINANA LOCAL EMERGENCY MANAGEMENT AND RECOVERY PLANS

**ENVIRONMENTAL IMPLICATIONS:**

There are no known environmental implications arising from the preparation of the LEMP and Recovery Plan. It should be noted that the LEMP and Recovery Plans have regard to increasing propensity for climate emergencies that may impact the City.

**STRATEGIC/SOCIAL IMPLICATIONS:**

The City of Kwinana Strategic Community Plan 2019 – 2029 provides that Council will;

Plan	Outcome	Objective
Strategic Community Plan 2019 – 2029	A City adapted to climate change	3.5 Understand the impacts of climate change and take a risk management approach to addressing these effects in future planning
	A well serviced City	4.3 Ensure the Kwinana community is well serviced by government and nongovernment services

**COMMUNITY ENGAGEMENT:**

No community engagement has been undertaken as part of the preparation of this report. Advice of the endorsement of the plans and associated appendices will only be provided to the key listed parties directly involved in the City's Emergency Management response and recovery processes.

A public viewing document of the City Emergency Management and Recovery Plans and the associated appendices will be made available for public viewing on the City electronic mediums and public library after these plans are endorsed minus the personal detail of key contacts within all these documents.

**RISK IMPLICATIONS:**

The risk implications in relation to this proposal are as follows:

Risk Event	Without Local Emergency Management Arrangements in place there is a risk that the response to emergency Incidents within the community will not be coordinated.
Risk Theme	Failure to fulfil statutory regulations or compliance Inadequate safety and security practices
Risk Effect/Impact	People/Health Financial Reputation Compliance Property

## 18.3 CITY OF KWINANA LOCAL EMERGENCY MANAGEMENT AND RECOVERY PLANS

Risk Assessment Context	Operational
Consequence	Moderate
Likelihood	Possible
Rating (before treatment)	Moderate
Risk Treatment in place	Reduce - mitigate risk
Response to risk treatment required/in place	Ensure regular and ongoing updates to the Emergency Management and Recovery plans and associated appendices in place and endorsed by the City as per legislative requirement
Rating (after treatment)	Low

**COUNCIL DECISION**

390

**MOVED CR M ROWSE****SECONDED CR W COOPER**

**That Council endorse the Local Emergency Management and Recovery Plans and the associated Appendices as detailed in Confidential Attachment A and B.**

**CARRIED  
6/0**

*The Coordinator Strategic Planning exited the Council Chambers at 5:53pm.*

## **19 Notices of motions of which previous notice has been given**

Nil

## **20 Notices of motions for consideration at the following meeting if given during the meeting**

Nil

## **21 Late and urgent Business**

Nil

## **22 Reports of Elected Members**

### **22.1 Councillor Wendy Cooper**

Councillor Wendy Cooper reported that she had attended the Southern Metropolitan Regional Council (SMRC) Meeting.

Councillor Cooper advised that she had attended the City of Kwinana Social Strategy Focus Group - Community Organisations Meeting.

### **22.2 Councillor Sandra Lee**

Councillor Sandra Lee reported that she had attended the City of Kwinana Social Strategy Focus Group - Community Organisations Meeting, held in the Ken Jackman Hall. Councillor Lee explained that there were approximately 45 people attended from 35 different Community Organisations, many people contributed in a positive way and hopefully a rounded action plan can be developed.

## **23 Answers to questions which were taken on notice**

Nil

## **24 Mayoral Announcements**

Mayor Carol Adams reported that she had participated in the Filming of the fortnightly 'Council Wrap' in relation to outcomes from the Ordinary Council Meeting held on 10 March 2021.

The Mayor advised that she had attended and spoke on behalf of the City at the official opening of atWork Australia. The Mayor explained that atWork are a job active service whom assist people living with disabilities and disadvantage gain work. The organisation deliver Jobactive, Indigenous Employment Services, ParentsNext and NDIS in Perth as well as Disability Employment Services on a national scale.

## 24 MAYORAL ANNOUNCEMENTS

The Mayor mentioned that she had attended a Creating Community for Kids (CCK) Joint Advisory Committee Strategic Workshop.

The Mayor reported that, in company with the Chief Executive Officer (CEO) and Economic Development and Advocacy Manager, they met with Mr Frank Ha, President of Tinqu Lithium and Peter Bradford, Managing Director of IGO. The Mayor advised that they had a tour of the facility and an open discussion about the progress of the plant's commissioning, which is anticipated early next year.

The Mayor advised that she had attended Bright Futures Children's Service in Orelia for its Harmony Day Event.

The Mayor mentioned that, in company with the CEO, they met with Mr Chris Ellison from Mineral Resources to discuss its operations in Kwinana.

The Mayor reported that she had attended the Rockingham Kwinana Chamber of Commerce (RKCC) Business Breakfast where she provided a presentation on "all thing Kwinana" ahead of the guest speaker, Mike Edmonds, a marketing specialist and author whom has been instrumental in helping small and major brands succeed in business. The Mayor added that the event was hosted by the City of Kwinana and was well attended with over 100 attendees.

The Mayor advised that she had participated in a special board meeting of Kwinana Early Years' Service Inc.

The Mayor mentioned that the City had hosted a luncheon forum for the WA European Business Association with a delegation of Trade Ambassadors representing Finland, Netherlands, Austria and Germany. The Mayor explained that discussions were focused on the opportunities for Kwinana to play a pivotal role in facilitating business growth in the mineral energy supply chain especially with the growing interest in Hydrogen, Green Energy, Lithium and Battery business globally.

The Mayor reported that she had participated in the inaugural Public Art Review (PARP) Meeting.

## 25 Confidential items

Nil

## 26 Close of meeting

The Mayor declared the meeting closed at 6:06pm.